06/17/2020

Attn: Mr. Nick Eagleson
Planner II, Adams County
4430 S. Adams County Parkway, 1st Floor, Suite W2000A
Brighton, CO 80601

Project Info:
Project Name: Rocky Mountain Rail Park
Project Number: PRC2018-00006
Review Number: 3

Documents Submitted:
Doc1-Comment Response Document (this document)
Doc2-Exhibit A: Lot and Tract Area
Doc3-Exhibit B: Active Recreation Exhibit
Doc4-Final Development Plan (Revision Dated 03/14/2020)
Doc5-Final Plat (Dated 06/15/20)
Doc6-Roadway Construction Plans (Dated 05/15/2020)
Master Development Agreement (Dated 06/17/2020) (in Word format for editing)

Dear Mr. Eagleson,

Thank you for your 3rd round review comments regarding the Final Development Plan and Final Plat for Rocky Mountain Rail Park. We have prepared the following responses which address those comments and have included the necessary resubmittals in PDF format.

Like our prior responses, this document is organized in the same order as the comments were provided with separate sections for each reviewer. These comment responses include:

- Adams County ROW
- Adams County Planning
- Adams County Engineering
- City of Aurora
- Transport Colorado: Adjacent Landowner / Objecting Competitor
- Colorado Department of Transportation

Again, we appreciate you and your team's efforts and look forward to continuing to work with the County on this significant project.

Comments from Adams County ROW:

Comment ROW1: *Show the ROW dedication to Aurora in a Tract, then you can include utility easement outside the Tract, which will be dedicated in the future. This will be easier for Aurora to reference that in their legal description when they get the deed. The dedicated utility easement would already be outside of it and it won't have to be relocated in the future.*
Response ROW1: The applicant would like to proceed by keeping the ROW to be dedicated outside of the boundaries of the plat. ROW will be dedicated at the appropriate time by metes and bounds.

Comment ROW2: *Please add a note to the Tract table showing Tract L is owned by RMRP and will be dedicated to the City of Aurora for Petterson Road.*
Response ROW2: Please see response to ROW 1

Comment ROW3*: Along 48th Ave, there should also be a Tract that also states the ROW will be dedicated to Aurora by separate instrument. This area should address the dedication of ROW in some fashion. Right now it appears the southern half of 48th is not being dedicated.*
Response ROW3: Please see response to ROW 1. The southern half of the ROW will be dedicated to the City of Aurora from Rail Park Drive to Petterson Road, 40' from the section line, by separate instrument. From Rail Park Drive to CASP, the applicant proposes a restricted-access road as part of a through-the-fence agreement so that secure access to CASP can be assured. This is beneficial to the applicant, Adams County, and CASP.

Comment ROW4: *Why does the railroad widths on Sheet 3 not line up? Would be cleaner to line everything up.*
Response ROW4: The railroads widths do not line up because there is an access road adjacent to the rail tracks that switches sides at this location. Please refer to the site plan to see how this appears as a surface feature.

Comment ROW5: *Sheet 5 has a label showing Tract C, that should be labeled Tract F.*
Response ROW5: The label has been modified as requested.

Comments from Adams County Planning (Nick Eagleson):
Comment PLN01: *As referenced in the letter dated May 11, 2020, the total cash-in-lieu of the PLD requirement for this project is $1,539,800. The Industrial Use classification was used for this calculation, which differs from our Residential calculations. The Standards do not allow for a waiver of the land dedication requirements. The fee can be split into four*

payments in accordance with ACDS&R 5-05-05-03 (3), with the first payment being due prior to the recording of the plat.
Response PLN01: After consultation with the County, the applicant is proposing to split the fee into four payments. At the time the payments are due, if there is a way under the County code that they can be reduced, the applicant will have the opportunity at that time to seek such a reduction. Further details are provided in the proposed Master Agreement.

Comment PLN02: I do not see any reference in the staff report or the plat for the PDP, that there was a request to reduce the open space Standards. We still need to see the breakdown shown for overall open space, which should be 30% of the entire site, with 25% of the amount dedicated for active recreation. An appropriate place for this summary and calculation might be under Open Area, on page two of the FDP document.
Response PLN02: Page 2 of the Preliminary Development Plan ("PDP") approved the open space ratio at 26 percent, with active recreation space comprising 23 percent of the open space. The applicant is not requesting a reduction in the previously approved minimum required open space. In fact, the applicant is proposing a reduction in the amount of saleable property from 507.23 acres (as approved in the PDP) down to 485.80 acres (as shown in the proposed FDP).

The changes are predominantly in response to the need to make minor change to lot sizes and tracts to accommodate the various uses that are typical as one transitions from a preliminary design to a final design. We have included a spreadsheet as Exhibit A that compares the PDP numbers with the FDP numbers after dedication of the ROW (which will be done by separate instrument). The inclusion of Tract D in the upper section of the table is to accommodate the Adams County request that the applicant place the manifest rail lines in their own tract rather than include them as part of Lot 1.

Comment PLN03: *Also per the letter dated May 11, 2020, the rail spur agreement will need to be finalized either prior to, or concurrently with the FDP and Final Plat hearing*
Response PLN03: The applicant understands this condition and has submitted documents to the County under separate cover with a proposal to purchase, which we understand is the County's preferred alternative. If you have any further questions, please let us know if the agreement is not acceptable.

Comment PLN04: *On your previous resubmittal, you reference that there is no direct access to CASP along the western side of the property because there is the north-south runway that cuts off CASP from the RMR property. There is actually a large piece of CASP property adjacent to 30th and RMR property that is on the east side of the north-south runway. This area of the CASP will be developed at some point in the future, which is why there has always been the discussion of having a "through the fence" agreement worked out. Since it's currently unknown where and what type of development would occur on either side of CASP and RMR, it would make sense to have this future "through the fence" agreement as a*

condition of approval. Also, 48th Ave is intended to have a future private access point to CASP, as development occurs. The north-south runway does not prohibit this.
Response PLN04: After consultation with County staff, the applicant and the County agreed that a through-the-fence agreement is in the interest of both parties, and does not have to be a condition of approval. The applicant intends to submit a proposed through-the-fence agreement prior to the hearing on the FDP. The processing of the FDP and through-the-fence agreement will proceed on separate tracks.

Comment PLN05: *You stated there would be a separate exhibit showing the active recreation areas that went around the perimeter of the south property perimeter and where it was relocated to on the north property. I did not see this exhibit on the FDP or the Final Plat documents.*
Response PLN 05: The applicant has provided the exhibit with this submittal and has shown the tracts that are also identified as Active Recreation Tracts.

Comment PLN06: *I did not see a separate landscape page within the FDP documents, which shows berming along the western edge of RMR?*
Response PLN06: The applicant has been holding off on preparing a revised landscape plan pending resolution with the County on other site boundary issues (so that the landscaping is placed in the correct locations). During our meeting on 06/15/20 we advised Adams County staff that a revised plan is forthcoming during the week of 06/22/20 since our landscape architect had a family emergency that resulted in us not being able to send it with the submittal, which is required by 06/17/20.

We discussed landscaping issues with staff on 06/15/20. The western edge of the property is not bermed as it did not make sense to attempt to visually screen the runways from the development. Also: (1) the applicant's property is lower than the adjacent property to the west and it would be highly impractical to build a berm high enough to screen the property from the west, and (2) there is an existing major gas line against the west property line and installing berms (which wouldn't accomplish the desired function) would result in burying a major gas line under 6+ feet of fill.

Comment PLN07: *A subdivision improvements agreement (SIA) is required to be submitted and approved prior to noticing for the public hearing.*
Response PLN07: The applicant has submitted a Subdivision Improvement Agreement for the County's review and is currently negotiating this agreement with the County Attorney and staff.

<u>Eng. Review Comments: Matt Emmens (only comments that are not closed are shown)</u>

Comment ENG3: *The construction plans have been submitted and are currently under review. The public hearings cannot be scheduled until the plans have received preliminary approval. This comment will remain open until the construction documents have been preliminarily approved.*
Response ENG3: Based on our prior discussions, we have set a tentative hearing date of August 25th. We understand that the construction plans are still under review. However, if significant items are identified, we request those comments be returned quickly so they could be addressed prior to our hearing date.

Comment ENG12: Regarding Manilla Road: (most recent comment shown)*: The County is aware of the condition of Manilla Rd. Information on recent work (3 years ago) was sent to the applicant on 5/8/2020. The applicant was made aware that the County possesses information including asphalt thickness information, as well as bore test data. County staff does not believe this existing asphalt section can support the heavy truck traffic that will be generated by this development. The applicant can either agree to improve the asphalt section of Manilla Rd to handle the truck traffic based on the information the County has or with data the applicant collects or, use that data to prove that the asphalt section is adequate. Colfax Ave is under the jurisdiction of the Colorado Department of Transportation (CDOT). The applicant is required to work with CDOT to obtain all necessary permits for work within CDOT roadways. This will be a requirement of construction plan approval by Adams County.*
Response ENG12: The applicant has reviewed the documents associated with the construction of Manilla road by DJ South (oil and gas operator) including the geotechnical report for the ½ mile on the south side of Manilla Road. During that review, and as a result of physical inspection of Manilla Road, we have found nothing that would support an assertion that Manilla road is not capable of carrying heavy truck traffic as noted in the comment above. The transverse cracking that is visible on approximate 70' intervals is not traditionally caused by load induced failures but rather by shrinkage. The fix for this is to crack seal the road to prevent further damage caused by shrink/swell of the subgrade rather than providing an expanded asphalt depth (such as the 2" mill/overlay performed by DJ South).

During our meeting on 06/15/20, the applicant and the county agreed that we have the same goal of making sure that Manilla road is capable of carrying truck traffic during the short-term horizon (less than 10 years). While we do not agree that we are responsible for improving roads in locations where our traffic study does not find that improvements are warranted by our proposed development, we have volunteered to perform some testing of the road so that the applicant and the County are both comfortable with its condition.

In the event that the road is deemed capable of conveying the short-term truck traffic, this issue will be resolved by the testing. Even if it is not capable of conveying the truck traffic, we do not believe it is appropriate to perform road work in the short-term (such as placing a 2" overlay) since this road will be dramatically modified as future development occurs adjacent to Manilla road, and the planned ultimate section would result in any improvements that are made being torn out.

If the County desires the applicant to perform certain improvements based on the analysis, the County has suggested that one alternative is to have the applicant pay for certain improvements up front and then then the county would reimburse the applicant for those expenditures from future traffic impact fees collected by the county as a result of development on the property. This is a possibility the applicant is willing to consider. It will be further evaluated based on the results of the road analysis.

City of Aurora Comments: Steve Rodruguez:

Comment COA1: *Thank you for the referral of the RMRP's recent resubmittal (Final Plat & Final development Plan). As discussed with the applicant and Adams County on a conference call April 8th, the City requires the Pre-Application meeting, submittal of an ISP, and Civil plans. After review of the ISP, the City will be able to provide notice to Adams County that the application can advance, provided all major items have been addressed. Feel free to reiterate the aforementioned to RMRP representative Rob Singer and team as was discussed in the above referenced April 8th telephone conference call. Take care.*
Response COA1: In an attempt to work with Aurora on this issue, the applicant has informally submitted engineering drawings to the City of Aurora. The drawings follow the Aurora design standards. The applicant submits that the City is not in a legal position to "require" any particular procedure at this stage as a condition of the County's further processing of the FDP and Final Plat applications. That said, in the interest of facilitating the process, the applicant intends to submit documentation for a formal pre-application meeting right after the County's approval hearing.

To date, we have not submitted our drawings through a "formal" submittal process (which includes the pre-application meeting) since we do not want Aurora reviewing and approving plans that may still be subject to change until we have FDP and plat approval through Adams County. We have met with Aurora multiple times (both at the site and during meetings with the entire staff) and even discussed whether the meetings we had constituted a "pre-application meeting". Originally, Aurora directed us to route all information through Adams County since they were a referral agency, and it is only just recently that Aurora has changed its mind and is now requesting that we go through a formal submittal process at the last minute.

We have committed to both Adams County and Aurora that once we are ready for formal roadway submittal in the City of Aurora ROW we will do so (and meet their design standards). We acknowledge that we are legally required to do so. The applicant understands that it will be necessary to go through this process with Aurora before we perform any construction activities in the City of Aurora ROW- and we will do so at the appropriate time.

In an effort to address this comment effectively, we will schedule a pre-application meeting with Aurora to occur immediately after our public hearing with Adams County.

Transport Colorado Comments via Consultant FHU

Comment 1: *Page 9 under Trip Generation Estimates*
Response 1: This development covers a large area with an expected low employee count. Therefore, the most appropriate way to calculate the true project trip generation is to use the anticipated number of employees. It is the employees that are anticipated to be driving to and from the project site, not the land or building area. Therefore, using this employee estimate is believed to be the best and most appropriate method for estimating project traffic. The applicant stands by its estimates regarding the type of use and number of employees. Additional analysis based on opinions from competing development is not appropriate.

Comment 2: *Page 10 Under Background Traffic Growth*
Response 2: No Response required.

Comment 3: *Page 10 under Operational Analysis*
Response 3: No Response required.

Comment 4: *Page 11 Under Infrastructure Requirements*
Response 4: A northbound right turn lane was not found to be needed on Manilla at SH-36.

Transport Colorado Comments via CVL
The responses provided below to Transport Colorado Comments via CVL

Comment on PLN 13: *We note that Adams County and the Mile High Flood District have specific comments that have been presented to the RMRP regarding the need to convey and appropriately detain the stormwater flows stated by MHFD's consultant and are satisfied with the RMRP implementing the MHFD's criteria, which supercedes other drainage reports in the area in some instances and is recognized by the City of Aurora within Adams County.*
Response PLN 13: No response required.

Comment on ENG 12: *We are in agreement with Adams County's comment. FHU's Memorandum to Nick Eagleson and the Adams County Planning Commission Members dated May 15, 2020 outlines specific understated trip generation estimates for the RMRP. Understating the trips generated would result in the RMRP contributing less funds to the surrounding roadway infrastructure than appropriate.*
Response Eng12: The applicant has responded to FHU under prior comment responses. The trip generation identified for this project is believed to be appropriate and accurate.

Comment on ENG 13: *The RMRP representatives acknowledge Adams County as their lead in the coordination with the City of Aurora and TransPort Colorado. The TransPort Colorado team is not aware of any Adams County mandate or official improvement agreement for the RMRP project to participate in appropriate and proportionate offsite improvements to Colfax Avenue, Manila Road and the Manila and I-70 interchange improvements. We respectfully request Adams County to work with the City of Aurora in reviewing FHU's May 15, 2020 Memorandum to Mr. Nick Eagleson and the Adams County Planning Commission to ensure that appropriate employee counts and trip generation values are assigned to the RMRP and that agreements are executed to obligate RMRP to roadway infrastructure cost participation reflecting that design criteria.*
Response ENG 13: The applicant has responded to FHU comments regarding traffic under a separate comment.

Comment 4: *We request that FHU's May 15, 2020 Memorandum previously referenced be considered and studied by Adams County to ensure appropriate infrastructure cost participation by the RMRP.*
Response Comment 4: The applicant has addressed discussions associated with cost sharing in prior comment responses.

Comment 5: *The TransPort Colorado team is in agreement with Adams County. The RMRP representatives' response to this comment is not acceptable and does not address Adams County's requirement to assess the structural adequacy of the referenced existing Manila Road pavement sections to handle the increased truck traffic that will be generated by the RMRP in accordance with FHU's May 15, 2020 Memorandum.*
Response Comment 5: The applicant is addressing discussions regarding Manilla Road in prior comment responses and directly with Adams County. The applicant does not concede that it is responsible for the evaluation or improvement of Manilla road. However, the applicant has volunteered to provide an analysis of the structural adequacy of Manila, and may pre-pay traffic impact fees if necess
ary to address structural inadequacy.

Colorado Department of Transportation

CDOT Environmental:

Comment CDOTE1: *Environmental will need to see environmental tech memos for the area of CDOT ROW that will become an access point(s) , will have work or staging. (02/24/20 and 03/9/20 and 05/13/20)*

Response CDOTE1: The applicant will submit and complete the required environmental technical memos upon submitting detailed design document for CDOT's review.

CDOT Traffic (Jason Igo)

Comment CDOTT1: *NEATS model is not adopted by CDOT. The DRCOG model is what CDOT has adopted. We will need a sensitivity analysis performed between the two models to see what the difference in volumes on our roadways.*

Response CDOTT1: We understand. We agree on use of the DRCOG model as the initial traffic study prepared was based on those projections. However, the City of Aurora commented to update the traffic study to their NEATS model. An updated study with adjusted background volumes based on the NEATS model was submitted in January 2020. The NEATS model traffic volumes were slightly higher than the DRCOG model; therefore, the modification provides a conservative analysis. Of note, the change resulted in only one difference with the proposed recommendations, which was signalization of the I-70 WB Ramps intersection with Manilla Road by 2045.

Comment CDOTT2: *Figure 6 shows two ¾ movements for the south section. It looks like there are just extra arrows on the figure.*

Response CDOTT2: Figure 6 shows the trip distribution for the North Section. There is no traffic anticipated to be entering either driveway for the South Section. The figure is correct. Those arrows indicate no volume as being assigned at either driveway.

Comment CDOTT3: *Proper clearance timings should be used in analysis. Red time and yellow time are the Synchro defaults. It would be good to have proper clearance interval. From rough calculations it will be roughly 5 seconds for yellow and 2 seconds for red.*

Response CDOTT3: Only one traffic signal is proposed in the study area at the I-70 Westbound Ramp and Manilla Road intersection in 2045. Based on calculations for this intersection with a 45 mph speed limit results in a yellow time of 4.5 seconds and red of 1.5 seconds. This clearance interval is not anticipated to change the results or conclusions of this intersection in the 2045 horizon.

Comment CDOTT4: *(Jason Igo) 3-18-2020 Reviewed comments. Response to comments that SH -36 is 2% percent trucks is false. SH-36 listed in OTIS has at least 6% trucks and is probably closer to 7%. This is what you should be using.*

Response CDOTT4: Those are the off-peak truck percentages that are being referenced, at 5.9 percent for this section of the highway from the CDOT OTIS. The peak hour percentage is identified as 0.47 percent. Therefore, the 2 percent peak hour truck volume, as used in this analysis, is conservative.

Comment CDOTT5: *The Ramp Terminals are considered part of CDOT System and SHAC does still apply at the intersection. The North and South Movements are both over 100 DHV and are opposing traffic in this scenario. The left would need to wait for those movements to proceed thru the intersection before it could go. Has not addressed previous comments in this submittal. Only see comment and responses. Jason Igo 5/18/2020*
Response CDOTT5: We believe that we have interpreted the State Highway Access Code correctly for warrants for turn lanes when considering adjacent and opposing traffic volumes. Furthermore, the operational level of service analysis did not identify the need for additional turn lanes.

<u>CDOT Resident Engineer Comments (kdd)</u>
Comment CDOTRE01: *The residency will need to review the plans for any modifications made to SH36. The TIS lists several modifications that will need to be made to the state highway, and CDOT will need to review these plans to ensure they were designed to our standards and specifications. --kdd 11/13*
Response CDOTRE01: Understood

Comment CDOTRE02: *No comments on Revision #3. --kdd 12/14/18*
Response CDOTRE02: No response required on most recent revision

<u>CDOT Permits Comments (rs 03/04/20)</u>
Comment CDOTP01: *On all sheet maps, please add the label SH 36 on Colfax, which would also imply that different standards and specifications may apply to that Right-of-way.*
Response CDOTP01: The label SH36 has been added.

Comment CDOTP02: *On sheet 2 of 28, RE: Site General note # 17, it is recommended that any work on SH 36 be in accordance to the approved plans and specifications tied to the respective CDOT permit.*
Response CDOTP02: Any and all work within the CDOT ROW will be approved with the appropriate CDOT permit.

Comment CDOTP03: *Note quite clear why "City of Aurora Roadway notes" are added to the Roadway construction plans, sheet 3 of 28. For SH 36, the construction standards and specifications should adhere to CDOT's rules, outlined by a permit.*
Response CDOTP03: The plans shown are City of Aurora and are not intended for improvements to SH7

Comment CDOTP04: *On sheet 28 of 28 add the appropriate cross section for SH 36/Colfax. It is not likely that the RR is going to dedicate any additional RoW for SH 36 for improvements in the westbound direction. There needs to be a note in the Column of Site/General notes on sheet 2 of how-when the appropriate RoW will be dedicated. The response to our comment (#5) presumes that CDOT and Adams county will accept a 120-ft of RoW. The RoW for SH 36 is also intended to accommodate utilities, roadside drainage, and multi-modal facilities.*
Response CDOTP04: This set of plans is intended for City of Aurora roadway improvements. A plan set for the improvements for Colfax specifically will be prepared as part of acquiring an access permit and the requested cross section will be added to this plan set.

Comment CDOTP05: *There needs to be a note in the column of Site/General notes on sheet 2 of 28 that "Any improvements on SH 36/Colfax, will require permitting through CDOT. Access, Utility and Special Use Permits may be warranted for any work in CDOT RoW."*
Response CDOTP05: The requested note has been added

Comment CDOTP06: *Unclear if the applicant understands the context of their response to "Additional Comments - Engineering (AC-ENG) #6": An access permit application (to CDOT) will be submitted when specific project phases are moving forward, and improvements are identified. If a permit is warranted for an existing RoW, that permit must come from the local agency who owns the RoW. We note a response to the RoW comment #6 that an unidentified piece of right-of way will be dedicated to the City of Aurora. This remark has a greater affect than who will maintain such road, but also the authority & responsibility for permitting.*
Response CDOTP06: The applicant will only be dedicating property that is part of our property boundary. The roadway maintenance of Petterson Road will be the responsibility of the city of Aurora once Petterson is built, however, the applicant anticipates entering into an agreement to provide snowplowing during the winter months.

Comment CDOTP07: *Additional Comments received 12-13-19 (Matt Emmons) (AC-ME): "The developer/applicant will be required to enter into a Subdivision Improvements Agreement (SIA) with the County and bond for all public and drainage infrastructure to ensure the public improvements are completed. No building permits will be issued until all public improvements are completed and preliminarily accepted by the County". Also, in the response #2 to CDOT Permit remarks "Per the SIA requirements, the applicant will provide detailed design information at such time as the public improvements to SH36 are warranted" CDOT was not provided opportunity to examine or comment on the SIA. We are unclear who-when westbound improvements on SH 36 will be made or accommodated in the limited RoW provided.*
Response CDOTP07: Understood

Comment CDOTP08: *Another concern is the off-site Manila/I-70 interchange which has a 14'- 6" clearance and no auxiliary turn lanes. The numbers shown in the TIS at the east & westbound interchange ramps (figures 9 & 10) would suggest that auxiliary turn lanes be provided, and of a design that is adequate to handle the size-scale of the trucks this industrial park is to attract and accommodate. This TIS suggest that within 20 years, the westbound ramps would need to be signalized. The process by which FHWA and CDOT examines, determines and approves the proposed improvements at the interstate requires extensive lead time. The State's 1601 process, even temporary or minor improvements (MIMR), must be initiated by the County, not the developer. We presume this is covered in the SIA, but without a review, we are unable to attest.*
Response CDOTP08: It is believed that study of the Manilla Road and I-70 interchange will need to be performed as a specific County/City and CDOT project to the FHWA requirements at some time in the future but not as part of this project.

Comment CDOTP09: *Note that this property has a long build out horizon. CDOT has also observed and recognizes that vesting of site plans has become a more common practice in property entitlement. Please note that access permits if/when issued by CDOT for connections to a state highway, has a 1-year time limit, extendable for 2 additional years before construction plans must be submitted for acceptance and approval of a Notice to proceed. Otherwise the Permit is void and withdrawn. We would recommend caution in approving vested site plans and plats, especially those with multiple phases and which dedicate right of way and/or access easements for a future connection to SH 36. Plans which illustrate future access to the state highway that are not legitimized within the time frame of a CDOT permit, could be a future issue. We would suggest some type of language be added to the column of Site/General notes sheet 2 of 28 relative to illustrating & vesting (future) access from a State limited access highway. In essence: the County cannot vest an expired CDOT Permit.*
Response CDOTP09: Understood

Comment CDOTP10: *Last word of caution is that improvements to the Petterson Rd RR crossing can be complex and also require extensive lead time. CDOT RR coordination is currently handled through HQ, not at the region level. However, permits associated with signalized crossings are handled at the region level.*
Response CDOTP10: Understood. The applicant has been working extensively with the RR and has met with the PUC regarding the future rail crossing.

Comment CDOTP11: *BTS-2-24-2020- I'll stand by for construction plans to be submitted, BTS-3/30/2020- Steve, we have been requiring a continuous accell/decel lane for all projects along Colfax, It does not appear on their site plans. Could you please request that the lane be provided and shown on the site plan.*

Response CDOTP11: The approximate locations of proposed acceleration and deceleration lanes are presented in the SIA and will be included in detailed construction plans. We understand those will require CDOT approval before any work could occur.

CDOT Other Comments: (Steve Loeffler 11/14/19)

Comment CDOTOC1: *State Highway Access Permits are required for the three accesses to State Highway 36. One will be needed for Petterson Rd. on the north side. The Permittee for this permit should be Adams County since this is a County Road connection to the Highway. For the development south of Highway 36, two State Highway access permits are required. The west, full-movement access should be on the property line. A provision of the permit will be that the full-movement access would become a shared access with the adjacent property to the west at the time it develops. The east access will be limited to Right-in, Right-out only. And the spacing of the accesses must accommodate any required auxiliary lanes. No design waivers will be given for substandard required lanes. Point of contact for the access permits is Steve Loeffler who can be reached at 303-757-9891 or steven.loeffler@state.co.us*
Response CDOTOC1: Understood

Comment CDOTOC2: *We need to review a drainage study for both the north and south developments to confirm that there will be no negative impact ot State Highway 36.*
Response CDOTOC2: The drainage report will be provided with the construction plans when we permit the work on SH36.

Comment CDOTOC3: *Any signing on these developments that will be visible to the Sate Highway must be on-premises and cannot be either wholly or partly in the State Highway Right-of-Way. All signing must comply with any other applicable rules governing outdoor advertising in Colorado per 2 CCR 601-3*
Response CDOTOC3: Understood

Thank you for all the work you have put into this project. If you have questions about the responses we have provided, please do not hesitate to contact me.

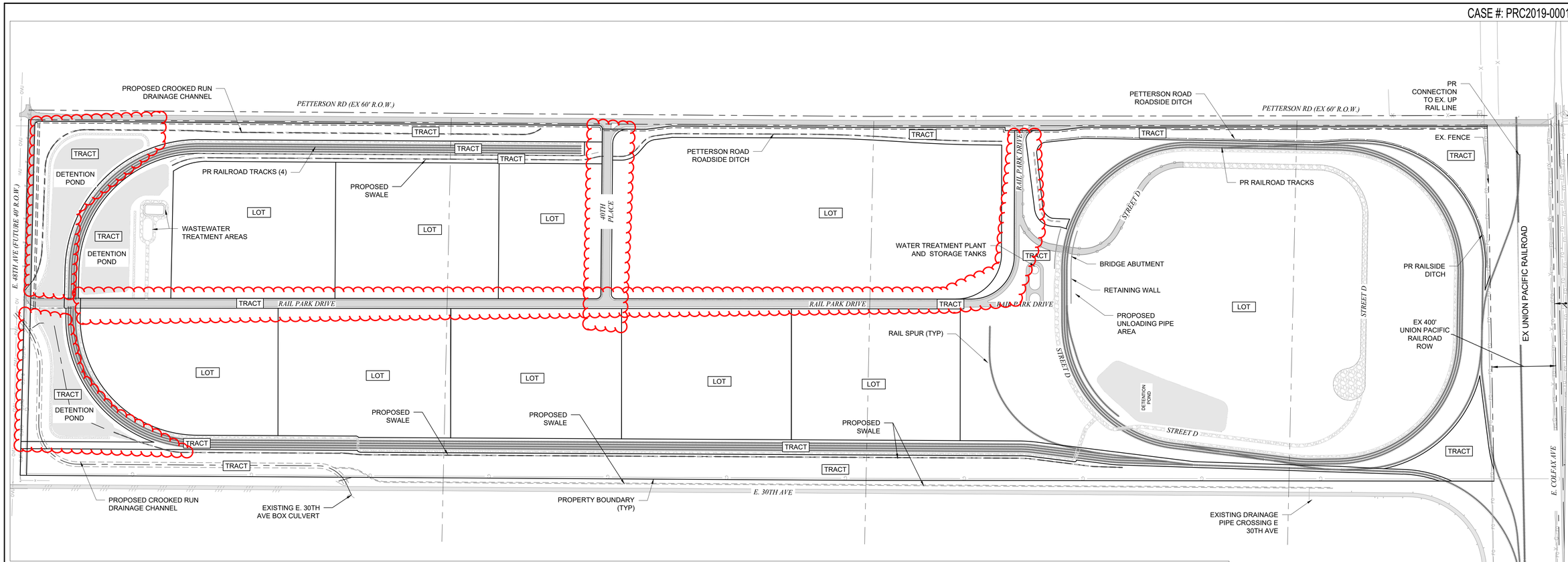
Sincerely,

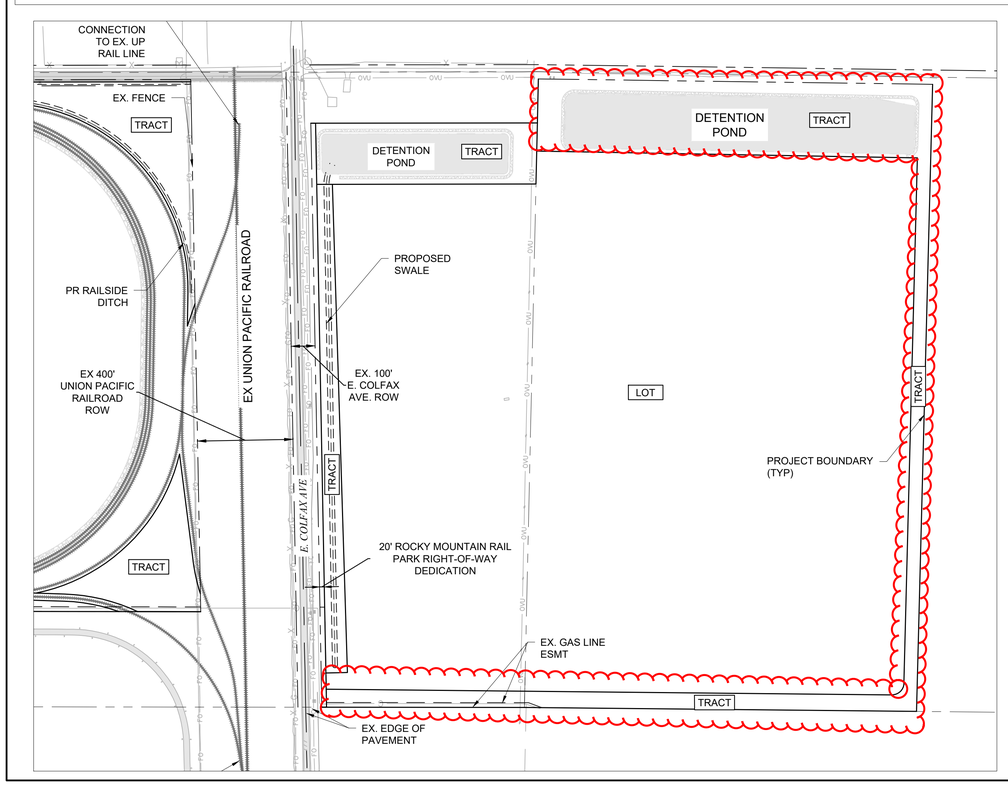
Rob Singer, P.E. CFM
Senior Vice President-Development
Rocky Mountain Industrials
4601 DTC Blvd, Suite 130
Denver, CO 80237

Exhibit A: Parcel Table Comparison

Location	Preliminary Development Plan	Final Development Plan
Lot 1	104.78	131.55
Lot 2	29.86	19.99
Lot 3	20.18	19.99
Lot 4	16.84	19.99
Lot 5	18.59	19.99
Lot 6	20.51	19.99
Lot 7	25.41	19.99
Lot 8	37.96	19.99
Lot 9	51.08	12.49
Lot 10	38.73	54.86
Lot 11	140.91	121.59
Tract D (MANIFEST TRACKS)	2.38	25.38
Sub-Total Lots	**507.23**	**485.80**
Tract A (RAIL PARK DRIVE)	21.53	15.34
Tract B (DRN OPEN SPACE WEST PL)	3.79	28.01
Tract C (NORTH PONDS)	3.41	24.94
Tract E (NORTH POND WWTP)	2.12	12.82
Tract F (DRN OPEN SPACE EAST MID)	5.24	6.90
Tract G (DRN OPEN SPACE EAST SOUTH)	2.49	9.78
Tract H (DRN OPEN SPACE SOUTH WEST	6.95	3.62
Tract I (DRN OPEN SPACE SOUTH PARCEL NORTH ROW	18.1	3.66
Tract J (SOUTH PARCEL POND B2)	22.93	5.44
Tract K (SOUTH PARCEL POND B3 LKQ BERMS)	13.68	18.46
Tract L (NOT USED)	9.37	-
Sub-Total Tracts		**128.97**
ROW Dedication (48th)	0.51	1.08
ROW Dedication (Petterson)	2.09	2.47
ROW Dedication (Colfax)		1.12
Sub-Total ROW Dedication		**4.68**
Sub-Total	**112.21**	**133.65**
Total	619.44	619.45

CASE #: PRC2019-00012





OWNERSHIP AND MAINTENANCE TABLE					
LOTS	AREA	OWNER	DEDICATIONS	MAINTENANCE	USE
Lot 1		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot2		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 3		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 4		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 5		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 6		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 7		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 8		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 9		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 10		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 11		RAIL LAND COMPANY, LLC		OWNER	PUD
SUB TOTAL LOT AREA (AC)	460.42				
TRACTS					
TRACT A		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	ACCESS, DRN, LA, OS, U AR
TRACT B		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U AR
TRACT C		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT D		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT E		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT F		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT G		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT H		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT I		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT J		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT K		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U AR
SUB TOTAL TRACT AREA (AC)	154.35				
Total	**614.77**				

**RMRPMD = ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT
AR = ACTIVE RECREATION
DRN = DRAINAGE
LA = LANDSCAPING
OS = OPEN SPACE
U = UTILITIES

LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE & CHANNEL CENTERLINE
- SWALE & CHANNEL TOP AND TOE
- GRAVEL/ACCESS ROAD

0' 150' 300' 600'
SCALE: 1"=300'

FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

ROCKY MOUNTAIN RAIL PARK FINAL DEVELOPMENT PLAN

ADAMS COUNTY, COLORADO

OVERALL SITE PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	03/14/2020
FILE	
DWG	SIT-4.0
SHEET	4 OF 9

CASE #: PRC2019-00012

ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN

PARCEL 1: LOCATED IN THE EAST HALF OF SECTIONS 26 AND 23, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN
PARCEL 2: LOCATED IN THE SOUTHEAST QUARTER OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN
COUNTY OF ADAMS, STATE OF COLORADO

CERTIFICATE OF OWNERSHIP

RAIL LINE COMPANY LLC
4601 DTC BLVD. SUITE 130
DENVER, COLORADO 80237

ATTEST:

_____
GREGORY DANGLER, VICE PRESIDENT

STATE OF COLORADO

CITY AND COUNTY OF _____
THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BY ME
THIS _____ DAY OF _____, 2020,

BY: _____

MY COMMISSION EXPIRES _____, 20___

LEGAL DESCRIPTION

A PARCEL OF LAND LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO, DESCRIBED AS FOLLOWS:

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 TO BEAR NORTH 89°24'56" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON BEING RELATIVE THERETO.

COMMENCING AT SAID CENTER QUARTER CORNER OF SECTION 35; THENCE ALONG THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, NORTH 00°25'10" WEST, A DISTANCE OF 986.62 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING ALONG SAID WEST LINE, NORTH 00°25'10" WEST, A DISTANCE OF 1644.15 FEET TO THE SOUTH QUARTER CORNER OF SECTION 26; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°55'54" WEST, A DISTANCE OF 829.46 FEET TO A POINT ON A LINE TWENTY FEET (20') SOUTH OF AND PARALLEL WITH THE SOUTH RIGHT-OF-WAY LINE OF EAST COLFAX AVENUE, SAID POINT BEING "POINT A"; THENCE ALONG SAID PARALLEL LINE, NORTH 87°55'30" EAST, A DISTANCE OF 2449.56 FEET; THENCE SOUTH 01°04'32" EAST, A DISTANCE OF 929.60 FEET TO THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SECTION 26; THENCE ALONG SAID ALIQUOT LINE, SOUTH 89°44'09" EAST, A DISTANCE OF 184.56 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF PETTERSON ROAD; THENCE ALONG SAID WEST RIGHT-OF-WAY LINE, SOUTH 00°12'55" EAST, A DISTANCE OF 1653.32 FEET, MORE OR LESS; THENCE DEPARTING SAID RIGHT-OF-WAY LINE, NORTH 89°31'58" WEST, A DISTANCE OF 2630.74 FEET TO A POINT ON THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, SAID POINT ALSO BEING THE POINT OF BEGINNING.

ALSO COMMENCING AT SAID POINT A; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°55'54" WEST, A DISTANCE OF 520.07 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID NORTH RIGHT-OF-WAY LINE, NORTH 87°55'30" EAST, A DISTANCE OF 400.10 FEET TO A POINT, SAID POINT BEING "POINT B", A SECOND POINT OF BEGINNING; THENCE NORTH 00°55'54" WEST, A DISTANCE OF 1292.35 FEET; THENCE NORTH 00°55'37" WEST, A DISTANCE OF 2646.15 FEET; THENCE NORTH 00°39'08" WEST, A DISTANCE OF 2647.33 FEET; THENCE NORTH 00°39'07" WEST, A DISTANCE OF 2603.27 FEET TO A POINT ON A LINE FORTY FEET (40') SOUTH OF AND PARALLEL WITH THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 23; THENCE ALONG SAID PARALLEL LINE, SOUTH 89°38'34" EAST, A DISTANCE OF 2196.82 FEET TO A POINT OF CURVATURE; THENCE 35.76 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 23.00 FEET AND AN INCLUDED ANGLE OF 89°04'27", SUBTENDED BY A CHORD BEARING SOUTH 45°06'20" EAST, A DISTANCE OF 32.26 FEET; THENCE SOUTH 00°34'06" EAST, A DISTANCE OF 2584.22 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 923.12 FEET TO A POINT OF CURVATURE; THENCE 35.13 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 23.00 FEET AND AN INCLUDED ANGLE OF 87°30'29", SUBTENDED BY A CHORD BEARING SOUTH 43°11'07" WEST, A DISTANCE OF 31.81 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 268.06 FEET TO A POINT OF CURVATURE; THENCE 15.99 FEET ALONG THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 117.00 FEET AND AN INCLUDED ANGLE OF 07°49'45", SUBTENDED BY A CHORD BEARING, SOUTH 04°29'00" EAST, A DISTANCE OF 15.97 FEET; THENCE SOUTH 08°23'52" EAST, A DISTANCE OF 147.82 FEET TO A POINT OF CURVATURE; THENCE 11.34' FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 83.00 FEET AND AN INCLUDED ANGLE OF 07°49'45", SUBTENDED BY A CHORD BEARING SOUTH 04°29'00" EAST, A DISTANCE OF 11.33 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 1258.36 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 821.89 FEET; THENCE SOUTH 89°12'16" WEST, A DISTANCE OF 20.00 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 324.35 FEET TO A POINT OF CURVATURE; THENCE 14.55 FEET ALONG THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 117.00 FEET AND AN INCLUDED ANGLE OF 07°07'30", SUBTENDED BY A CHORD BEARING SOUTH 04°39'01" EAST, A DISTANCE OF 14.55' FEET; THENCE SOUTH 08°12'46" EAST, A DISTANCE OF 148.79 FEET TO A POINT OF CURVATURE; THENCE 10.32 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 83.00 FEET AND AN INCLUDED ANGLE OF 07°07'30", SUBTENDED BY A CHORD BEARING SOUTH 04°39'01" EAST, A DISTANCE OF 10.31 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 1331.48 FEET; THENCE SOUTH 01°05'20" EAST, A DISTANCE OF 1191.75 FEET TO A POINT ON SAID NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID RIGHT-OF-WAY LINE, SOUTH 87°55'30" WEST, A DISTANCE OF 2222.37 FEET, MORE OR LESS, TO POINT B, THE SECOND POINT OF BEGINNING.

CONTAINING 26,737,891 SQ. FT. OR 613.81 ACRES MORE OR LESS.

BENCHMARK

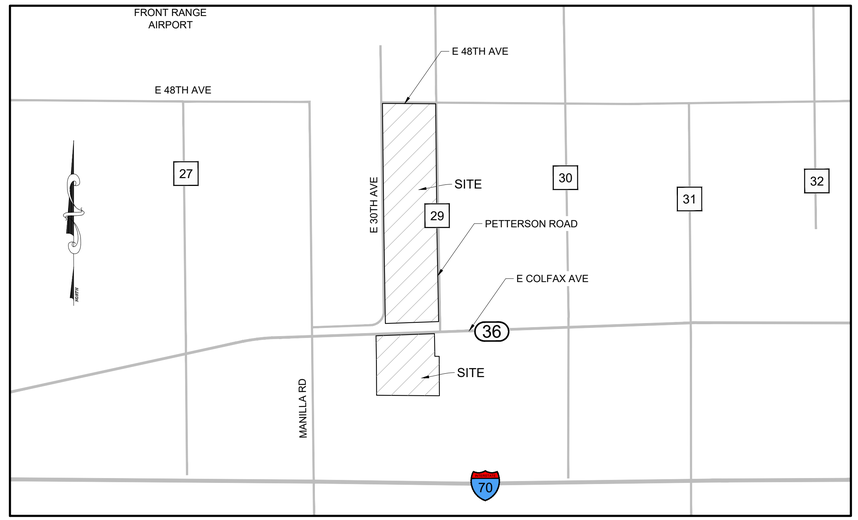
A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

BASIS OF BEARINGS

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 TO BEAR NORTH 89°24'01" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.

SUPPLEMENTAL REPORTS

DRAINAGE, UTILITY, AND TRAFFIC STUDIES HAVE BEEN PREPARED FOR THE ROCKY MOUNTAIN RAIL PARK. THESE DOCUMENTS ARE INCLUDED FOR SUBMISSION TO ADAMS COUNTY AND SHALL BE INCORPORATED HEREIN BY THIS REFERENCE.



VICINITY MAP

SCALE 1" = 3000'

SHEET NO.	DESCRIPTION
1	COVER SHEET
2	GENERAL NOTES
3	EXISTING CONDITIONS PLAN
4	OVERALL SITE PLAN
5	SITE PLAN - 1
6	SITE PLAN - 2
7	SITE PLAN - 3
8	TYPICAL STREET SECTIONS & DETAILS - 1
9	OVERALL GRADING PLAN

PROJECT TEAM CONTACTS

DEVELOPER
ROCKY MOUTAIN RESOURCES
4601 DTC BOULEVARD, SUITE 120 DENVER, COLORADO 80237
CONTACT: GREGORY DANGLER
(720) 459-8675

SURVEYOR
FLATIRONS, INC.
3825 IRIS AVE, SUITE 395
BOULDER, CO 80301
CONTACT: BOB LUND
(303) 443-7001 EXT. 183

ENGINEER
ENERTIA CONSULTING GROUP, LLC
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202
CONTACT: CLIFFORD NETUSCHIL, PE
(303) 525-7768

LANDSCAPER
OUTSIDE LA, LLC
BOULDER/ STEAMBOAT SPRINGS
CONTACT: SANDI GIBSON
(303) 517-9256

TRAFFIC ENGINEER
KIMLEY-HORN
4582 SOUTH ULSTER ST, SUITE 1500
DENVER, COLORADO 80237
CONTACT: CURTIS D ROWE, PE
(303) 228-2300

BOARD OF COUNTY COMMISSIONERS APPROVAL

APPROVED BY THE ADAMS COUNTY BOARD OF COMMISSIONERS ON THIS _____DAY OF _____, 2020.

_____
CHAIR

_____
COUNTY ATTORNEY APPROVED AS TO FORM

CERTIFICATE OF THE CLERK AND RECORDER

THIS FINAL DEVELOPMENT PLAN WAS FILED FOR RECORD IN THE OFFICE OF THE ADAMS COUNTY CLERK AND

RECORDER IN THE STATE OF COLORADO AT _____M. ON THE _____DAY OF _____, 2020.

_____
COUNTY CLERK AND RECORDER

BY DEPUTY: _____

ADDITIONS AND DELETIONS

THE FOLLOWING ADDITIONS AND DELETIONS IN THE PUD WERE MADE BY THE BOARD OF COUNTY COMMISSIONERS AT THE TIME OF APPROVAL.

_____

_____

_____

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

COVER SHEET

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	CVR-1.0
SHEET	1 OF 9

PERMITTED LAND USE

Rocky Mountain Rail Park Permitted Land Use Table (Lots 1-11)		
Zoning	Land Use Category	Permit Restriction
PUD	Abrasive Manufacturing	P
PUD	Aggregate products, storage and sales	P
PUD	Alcoholic beverage manufacturing	P
PUD	Antenna arrays (satellite dishes)	P
PUD	Apparel and other finished products made from fabrics and similar materials	P
PUD	Arrangement of transportation of freight and cargo	P
PUD	Asbestos products manufacturing	SR
PUD	Ashpalt and concrete production facilities	PA
PUD	Auto towing and storage yards	P
PUD	Auto/truck rental, leasing	PA
PUD	Automobile dealers	P
PUD	Automobile manufacturing	PA
PUD	Automotive repair except top, body, upholstery, reapair, paint, and tire retreading shops	PA
PUD	Automotive repair, including top, body, upholstery repair, paint, and tire re-treading shops	P
PUD	Automotive services except wrecking or towing storage yards	P
PUD	Aviation related uses	PA
PUD	Bakeries	P
PUD	Beverage manufacturing	P
PUD	Boiler or tank manufacturing	P
PUD	Bone reduction	SR
PUD	Book binding	P
PUD	Bus repair	P
PUD	Cabinet manufacturing with sales	P
PUD	Can manufacturing	PA
PUD	Candy product manufacturing	P
PUD	Canvas products manufacturing	P
PUD	Caustic soda manufacturing	SR
PUD	Celluloid manufacturing	P
PUD	Cement, cinder block, concrete, lime or plaster manufacturing	PA
PUD	Chemical manufacturing	PA
PUD	Chemicals and allied products manufacturing except drugs	PA
PUD	Clothing or cloth manufacturing	P
PUD	Coal, coke yards, or coal classifications	SR
PUD	Cold storage	P
PUD	Communications	PA
PUD	Cosmetic and perfume manufacturing	PA
PUD	Creosote manufacturing or treatment plant	P
PUD	Dairy and food processing and manufacturing facilities	P
PUD	Detergent, soap, and by-products manufacturing using animal fat	SR
PUD	Development and testing services	P
PUD	Disinfectant, insecticide, or poision manufacturing	SR
PUD	Distillation of bone, refuse, grain, and wood	SR
PUD	Drug manufacturing	P
PUD	Dry cleaning plants	PA
PUD	Dye manufacturing	SR
PUD	Electric and electronic equipment including electronic distribution and electrial industrial	PA
PUD	Eletronics manufacturing	PA
PUD	Fabricated metal, sheet metal shops, metal products manufacturing	P
PUD	Farm machinery sales and services	P
PUD	Farm supply sales	P
PUD	Fat rendering production	P
PUD	Felt manufacturing	P
PUD	Fertilizer manufacturing and processing	P
PUD	Flour mill	P
PUD	Forging plant and foundry	P
PUD	Fossil fuel manufacturing	SR
PUD	Fuel and ice dealers	P
PUD	Fuel, oil, gasoline, and petroleum products (bulk storage and/or sale)	P
PUD	Furnace installation, repair, and cleaning	P
PUD	Furniture and fixtures	P
PUD	Gas station	P
PUD	General building contractors	P
PUD	Glass or glass product manufacturing	PA
PUD	Grain elevators	P
PUD	Greenhouses	P
PUD	Hazardous waste treatment facility	SR
PUD	Heavy construction contractors	P
PUD	Hosiery manufacturing	P
PUD	Instruments and related products	P
PUD	Landscape storage yards	PA
PUD	Leather and leather products, including tanning and finishing	P
PUD	Linseed oil, shellac, and turpentine manufacturing and refinery	SR
PUD	Lubrcation and grease manufacturing	SR
PUD	Lumber mills, planin mills, and storage of logs	P
PUD	Lumber, building materials, and wood products	PA
PUD	Machine shops	P
PUD	Machine tool manufacturing	P
PUD	Machinery sales	P
PUD	Manufactured homes	P
PUD	Measuring, analyzing, and controlling instrument manufacturing	PA
PUD	Meat processing, packing, packaging and slaughterhouses	P
PUD	Medical and optical goods	PA
PUD	Metal ingots, casting sheets, or bearings, forging or rolling mills	PA
PUD	Millinery manufacturing	P
PUD	Miscellaneous manufacturing industries	PA
PUD	Mobile homes manufacturing and storage	PA
PUD	Moving companies	P
PUD	Musical Instruments and sporting/athletic goods manufacturing	P
PUD	Office and computing machines	PA
PUD	Oil compounding	SR
PUD	Paint and enamel manufacturing	SR
PUD	Paper, pulp, or paperboard mills	PA
PUD	Parking lot	P
PUD	Petroleum products or manufacturing	P
PUD	Photographic	PA
PUD	Pickle manufacturing	P
PUD	Printing and publishing	PA
PUD	Public utility storage, yards, and service installments	PA
PUD	Radio or TV broadcasting towers	PA
PUD	Railroad transportation	P
PUD	Railroad yards	PA
PUD	Recreational vehicle storage	PA
PUD	Recycling facilities	PA
PUD	Research	P
PUD	Restaurant/Café	P
PUD	Rubber and miscellaneous plastics manufacturing	PA
PUD	Salvage yards	PA
PUD	Sawmills	P
PUD	Scrap processing or shredding yard	PA
PUD	Shoe manufacturing	P
PUD	Sign manufacturing, repair, and maintenance	P
PUD	Smelting or refining of metal	SR
PUD	Space research and technology	PA
PUD	Special trade contractors	P
PUD	Special warehousing and storage	PA
PUD	Stone and clay products	P
PUD	Storage and disassembly of vehicles and the re-assembly of various parts	PA
PUD	Sugar and beet refining	P
PUD	Tar and waterproofing (materials manufacturing, treatment, and bulk storage)	SR
PUD	Telecommunications towers	PA
PUD	Textiles and apparel	PA
PUD	Trade schools	P
PUD	Trailer and truck manufacturing	P
PUD	Transporation equipment	P
PUD	Transporation services	PA
PUD	Trucking and general warehousing, including mini-storage	PA
PUD	Utility production and processing facilities	P
PUD	Vacation camper manufacturing	PA
PUD	Water storage and sales	P
PUD	Welding repair	P
PUD	Wholesale trade (durable and non-durable)	P
P = Permitted By-Right		
PA = Permitted By-Right by Adams County and are subject to regulations by CDPHE and/or FAA		
SR = Special Review by Adams County and are subject to regulations by CDPHE and/or FAA		
All uses may be subject to State Regulations as required by CDPHE		

PROJECT NARRATIVE

A. EXPLANATION OF THE CHARACTERISTICS OF THE PUD

THE ROCKY MOUNTAIN RAIL PARK FINAL DEVELOPMENT PLAN (FDP) IS COMPRISED OF 619.45 ACRES CONSISTING OF LOTS DESIGNATED FOR INDUSTRIAL/COMMERCIAL USE.

TRACTS DESIGNATED FOR OPEN SPACE STREETSCAPES, DRAINAGE, ACCESS, RAIL LINE, AND ACTIVE RECREATION AREAS INCLUDING SIDEWALKS.

THIS FDP IS LOCATED ON THE EAST SIDE OF E30TH AVENUE, NORTH AND SOUTH OF E COLFAX ST, SOUTH OF E 48TH AVENUE AND WEST OF PETTERSON ROAD. PRIMARY ACCESS TO THIS FDP IS FROM PETTERSON ROAD.

B. NUMBER, TYPE AND SIZE OF BUILDINGS

INDUSTRIAL/COMMERCIAL LOTS ARE PROPOSED IN THE FDP AND CORRESPONDING FINAL PLAT. THE NUMBER, TYPE AND SIZE OF THE PROPOSED BUILDINGS MUST ADHERE TO THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS, LATEST REVISION.

C. OWNERSHIP AND MAINTENANCE OF COMMON AREAS

ALL COMMON AREAS WITHIN TRACTS WILL BE MAINTAINED BY THE ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT (RMRPMD). RMRPMD IS RESPONSIBLE FOR THE ENFORCEMENT OF LANDSCAPE MAINTENANCE REQUIREMENTS. THROUGH THE RMRPMD, A DRC (DESIGN REVIEW COMMITTEE), WILL REVIEW PRIVATE OPEN SPACE DESIGN FOR COMPATIBILITY. PRIVATE LOT OWNERS SHALL BE RESPONSIBLE FOR THE MAINTENANCE OF THEIR PROPERTY. RMRPMD WILL ALSO BE RESPONSIBLE FOR MAINTAINING ALL LOCAL STREETS AND COLLECTOR STREETS.

D. LOCATION AND TYPES OF LANDSCAPING AND MAINTENANCE PROVISIONS

THE APPROVED ROCKY MOUNTAIN RAIL PARK PUD INCLUDES A VARIETY OF OPEN SPACES STRATEGICALLY LOCATED THROUGHOUT THE DEVELOPMENT. THIS FDP INCLUDES STREETSCAPE LANDSCAPING AND VARIOUS LANDSCAPE TRACTS.

E. UTILITY SERVICES

RMRPMD WILL HAVE ADEQUATE CAPABILITIES TO SERVE THIS PROPERTY WITH BOTH WATER AND SANITARY SEWER.

TRACTS & LOTS

THIS FDP REPRESENTS LOTS AND TRACTS. FOR THE ROCKY MOUNTAIN RAIL PARK, THE TERM TRACT SHALL BE DEFINED AS A GENERALLY DESCRIBED AREA OF LAND FOR THE PURPOSES OF ZONING, LOCATED WITHIN THE PROJECT BOUNDARY THAT HAS NOT YET BEEN FINAL PLATTED OR SUBDIVIDED INTO SPECIFIC BLOCKS AND LOTS. TRACTS & LOTS SHOWN MAY BE MODIFIED DURING SUBSEQUENT SITE SPECIFIC FDP.

LOT DEVELOPMENT STANDARDS

FLOOR AREA RATIO

BUILDING FLOOR AREA OR FLOOR AREA RATIO (FAR) - FAR IS THE TOTAL BUILDING FLOOR AREA IN SQUARE FEET DIVIDED BY THE BUILDING SITE AREA IN SQUARE FEET EXPRESSED AS A RATIO OF FLOOR AREA OF BUILDING TO SITE AREA. OVERHANGS, SOFFITS, AND COVERED OPEN ENTRIES/PORCHES ARE EXCLUDED. ALL STRUCTURES AND AUTHORIZED IMPROVEMENTS SHALL BE LOCATED ENTIRELY WITHIN THE BUILDING ENVELOPE, EXCLUDING BUILDING ROOF OVERHANGS.

PARKING

MINIMUM PARKING REQUIREMENTS WILL BE IN ACCORDANCE WITH ADAMS COUNTY DEVELOPMENT STANDARDS AND REGULATIONS TITLE REQUIRED PARKING FOR EACH INTENDED USE.

OPEN AREA

OPEN AREA MAY INCLUDE WALKS AND LANDSCAPING. THIS AREA DOES NOT INCLUDE BUILDING FOOTPRINTS OR THE HARD SURFACE OF PARKING LOTS OR DRIVEWAYS. MINIMUM OPEN AREA REQUIRED FOR INDIVIDUAL LOT DEVELOPMENT IS 10% OF TOTAL LOT AREA.

AN OPEN AREA MAINTENANCE PLAN SHALL BE SUBMITTED WITH EACH LOT AS PART OF THE FDP PROCESS. THE MAINTENANCE PLAN SHALL MEET THE LANDSCAPE AND OPEN SPACE MAINTENANCE REQUIREMENTS CONTAINED IN CHAPTER 4 OF THE ADAMS COUNTY DEVELOPMENT STANDARDS AND REGULATIONS.

OF THE 10-PERCENT REQUIRED OPEN SPACE FOR INDIVIDUAL LOTS, 10-PERCENT SHOULD BE ALLOCATED FOR REQUIRED INTERIOR LANDSCAPING. AT LEAST 75-PERCENT OF THE REQUIRED LANDSCAPE AREA SHALL INCLUDE LIVING PLANT MATERIAL. ALL INTERIOR LANDSCAPING DESIGN FROM A PLANT SPECIES STANDPOINT IS FURTHER OUTLINED IN THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS.

THE REQUIRED 10-FOOT LANDSCAPE BUFFER FOR EACH INDIVIDUAL LOT ALONG RAIL PARK DRIVE MAY BE COUNTED TOWARDS THE REQUIRED 10-PERCENT OPEN SPACE. PLANTING REQUIREMENTS FOR THE 10-FOOT LANDSCAPE BUFFER ARE FURTHER OUTLINED IN THE DESIGN STANDARDS AND ARE CALCULATED SEPARATELY FROM INTERIOR LOT LANDSCAPING.

LOT DEVELOPMENT STANDARDS CONTINUED

SETBACKS

ALL SETBACKS ARE MEASURED FROM THE PROPERTY LINE. ALL STRUCTURES AND AUTHORIZED IMPROVEMENTS SHALL BE LOCATED ENTIRELY WITHIN THE BUILDING FOOTPRINT WITH THE FOLLOWING EXCEPTIONS:

- EAVES, ROOF PROJECTIONS, AWNINGS, WINDOW WELLS, CHIMNEYS, PROJECTING SIGNS, AND OTHER FEATURES MAY PROJECT INTO REQUIRED SETBACKS A MAXIMUM OF 3-FEET, PROVIDED SUCH APPENDAGES ARE SUPPORTED AT, OR BEHIND, THE BUILDING SETBACK OR OFFSET LINE.
- ENCROACHMENTS AND PROJECTIONS INTO EASEMENTS MUST BE APPROVED BY THE EASEMENT HOLDER AND ADAMS COUNTY.
- IN THE CASE OF MULTIPLE ADJACENT LOTS THAT ARE OWNED BY THE SAME OWNER, THE SETBACKS AND BUILDING ENVELOPES SHALL APPLY TO THE OUTERMOST BOUNDARY OF THE CONTIGUOUS LOTS. IF CONSTRUCTED STRUCTURES CROSS A CONTIGUOUS BOUNDARY, THE OWNER SHALL EITHER COMBINE THE LOTS THROUGH A LOT CONSOLIDATION PLAT OR RECTIFY THE STRUCTURE SUCH THAT SETBACK REQUIREMENTS ARE MET.

LOT DEVELOPMENT STANDARDS CONTINUED

SETBACKS

INDIVIDUAL LOT MINIMUM SETBACK REQUIREMENTS ARE AS FOLLOWS:

BUILDING SIDE SETBACK: 20-FEET
BUILDING REAR SETBACK (ADJACENT TO RAIL LINE): 20-FEET
BUILDING FRONT SETBACK (ALONG RAIL PARK DRIVE): 40-FEET

PARKING FRONT SIDE AND REAR SETBACK: 10-FEET

PLEASE REFER TO SITE PLAN FOR SETBACK ORIENTATION.

**SETBACKS DO NOT APPLY TO LOTS WHERE THERE ARE ADJACENT TRACTS THAT PROVIDE BUFFERING FROM EITHER PUBLIC RIGHT OF WAY OR ADJACENT PROPERTIES.

OUTDOOR STORAGE

LOCATION: NO OUTDOOR STORAGE, LOADING, OR GARBAGE COLLECTION OR COMPACTION AREAS SHALL BE LOCATED WITHIN 20- FEET OF ANY PUBLIC ROAD.

INCORPORATION INTO OVERALL DESIGN: LOADING DOCKS, TRUCK PARKING, OUTDOOR STORAGE, UTILITY METERS, HVAC AND OTHER MECHANICAL EQUIPMENT, GARBAGE COLLECTION, GARBAGE COMPACTION, AND OTHER SERVICE FUNCTIONS SHALL BE INCORPORATED INTO THE OVERALL DESIGN OF THE SITE AND THE LANDSCAPE. IF AREAS ARE TO BE COVERED, THEN THE COVERING SHALL CONFORM TO THOSE USED AS PREDOMINANT MATERIALS AND COLORS OF THE PRINCIPAL STRUCTURE.

OUTDOOR STORAGE AREA RESTRICTIONS: ON AN INDIVIDUAL LOT BASIS, OUTDOOR STORAGE IS LIMITED TO A MAXIMUM OF EIGHTY 80-PERCENT OF THE OVERALL LOT AREA.

NO ITEMS MEANT FOR DISPOSAL SHALL BE STORED OUTDOORS EXCEPT AS OTHERWISE AUTHORIZED IN THIS PLANNED UNIT DEVELOPMENT. HEIGHT RESTRICTIONS ARE OUTLINED IN THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS.

ARCHITECTURAL DESIGN

ALL ARCHITECTURAL DESIGN SHALL ADHERE TO THE REQUIREMENTS SET FORTH IN THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS.

TRACT DEVELOPMENT GUIDELINES

ALL TRACTS SHALL BE OWNED AND MAINTAINED BY THE ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT (RMRPMD)

RAIL PARK DRIVE SHALL BE DEVELOPED PER THE CROSS SECTION IDENTIFIED IN THIS FDP FOR THE PORTION OF THE STREET IN THE NORTH SOUTH DIRECTION AND NORTH OF RAIL PARK DRIVE. FOR THE PORTION OF ROADWAY IN RAIL PARK DRIVE THAT IS WEST OF THE BRIDGE AND EAST OF TRACT C THE ROAD SHALL BE ONE-WAY (WESTBOUND) TO ELIMINATE INTERSECTION CONFLICTS BETWEEN TRUCKS COMING OUT OF LOT 1 AND VEHICULAR TRAFFIC ON RAIL PARK DRIVE.

THE REMAINING TRACTS SHALL BE USED BY THE RMRPMD IN GENERAL CONFORMANCE TO WHAT IS PRESENTED IN THIS FDP AND INCLUDE DRAINAGE CHANNELS, UTILITY INFRASTRUCTURE, ROADS, RAIL LINE, LANDSCAPING, VISUAL SCREENING, AND OTHER SERVICES DEEMED NECESSARY BY THE RMRPMD. VISUAL SCREENING FEATURES SHALL INCLUDE BERMS AND FENCING THAT IS INTENDED TO MINIMIZE (THOUGH NOT ELIMINATE) VIEWS ONTO THE PROJECT FROM ADJACENT USES. THIS DOES NOT INCLUDE SCREENING WEST OF THE PROJECT AS THE ONLY ADJACENT USE TO THE WEST IS THE COLORADO AIR AND SPACE PORT FOR WHICH VISUAL SCREENING WOULD BE INEFFECTIVE.

AT LEAST TEN 10- PERCENT OF THE TRACT AREA SHALL BE LANDSCAPED EXCLUDING THE AREA REQUIRED FOR DETENTION BASINS. AT LEAST 50-PERCENT OF THE LANDSCAPING SHALL BE PLACED SO IT ABUTS ADJACENT PUBLIC RIGHT-OF-WAY OR ACCESS EASEMENTS. THE REMAINDER OF THE TRACTS SHALL BE NATIVE GRASSES THAT SHALL BE MAINTAINED AS NOT TO EXCEED TWO 2- FEET IN HEIGHT. PERIMETER LANDSCAPING SHALL INCLUDE TWO (2) TREES PER 60-LINEAL FEET AND BE IN GENERAL CONFORMANCE TO THE LANDSCAPING PLAN PROVIDED IN THE DEVELOPMENT DESIGN DOCUMENTS.

THIS FDP AMENDS THE PDP TO ELIMINATE THE SOFT SURFACE TRAIL THAT WAS PRESENTED IN THE PDP. THIS WAS DETERMINED TO NOT BE FEASIBLE AS IT WOULD PUT THE PUBLIC IN THE POSITION OF CROSSING THE RAIL TRACKS WHICH IS NOT PERMITTED BY THE PUC. IN LIEU OF THE SOFT SURFACE TRAIL, RAIL LAND COMPANY IS PROVIDING HARD SURFACE SIDEWALK ALONG PUBLIC RIGHT-OR-WAY STREETS OF PETTERSON ROAD AND EAST 48TH AVENUE AND ALONG INTERNAL STREETS, RAIL PARK DRIVE AND 40TH PLACE.
LANDSCAPE, LIGHTING, AND SIGNAGE PLANS SHALL BE SUBMITTED FOR INDIVIDUAL LOTS WHEN APPLYING FOR BUILDING PERMITS.

FIRE PROTECTION

- SINGLE ACCESS POINT TO INTERIOR LOTS WILL REQUIRE THE BUILDING TO BE SPRINKLED. DUAL ACCESS TO INTERIOR LOTS MAY ELIMINATE THE SPRINKLED BUILDING REQUIREMENT SUBJECT OF IBC AND IFC REQUIREMENTS.
- ALL FIRE HYDRANTS WILL BE THE MUELLER SUPER CENTURION 250 HYDRANT MODEL AND WILL BE ORDERED IN YELLOW. THE CAP AND TOP COLORS ARE PAINTED AFTER INSTALL PER THE NFPA 291.
- ALL ACCESS POINTS TO INTERIOR LOTS SHALL BE DESIGNED PER THE 2012 IFC ROADWAY DIMENSIONS - APPENDIX D.
- INTERIOR LOT DEVELOPMENT REQUIRES AT LEAST ONE (1) FIRE HYDRANT WITHIN 150-FEET OF RAIL LINE ASSOCIATED WITH SPECIFIC LOT.
- ALL FIRE PROTECTION DESIGN SHALL BE PER THE BENNETT-WATKINS FIRE RESCUE AND IFC STANDARDS AND REGULATIONS.

LANDSCAPE CONCEPT

THE LANDSCAPE CONCEPT FOR ROCKY MOUNTAIN RAIL PARK REFLECTS THE NATURAL LANDSCAPES OF BOTH THE SITE AND ITS CONTEXT WITHIN THE ADJACENT AGRICULTURAL PROPERTIES. THESE CHARACTERISTICS WILL BE EMPHASIZED THROUGHOUT THE DEVELOPMENT'S OPEN LAND AND LANDSCAPE AREAS. STREET SCAPES WILL GENERALLY CONSIST OF PLANTINGS TO COMPLEMENT THE CHARACTER OF EACH DEVELOPMENT TYPE . NATURAL CORRIDORS AND PERIMETER LANDSCAPING WILL REFLECT A MORE INFORMAL AND NATURAL CHARACTER TO BLEND IN WITH THE SITE'S NATIVE LANDSCAPE SETTING AND TO PROVIDE ADEQUATE SCREENING TO ADJACENT PROPERTY OWNERS. PLEASE REFER TO THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS SECTION 5.0 (LANDSCAPE STANDARDS) FOR FURTHER DETAILED LANDSCAPING REQUIREMENTS FOR THE ROCKY MOUNTAIN RAIL PARK DEVELOPMENT.

OPERATIONAL STANDARDS

ALL PROPOSED DEVELOPMENTS WITHIN THE ROCKY MOUNTAIN RAIL PARK SHALL ADHERE TO THE ADAMS COUNTY DEVELOPMENT STANDARDS AND REGULATIONS.

ALL LIGHTING FIXTURE REQUIREMENTS ARE FURTHER OUTLINED IN THE ROCKY MOUNTAIN RAIL PARK DESIGN STANDARDS.

ADDITIONAL NOTES

1. THE PROVISIONS OF THE PUD AND FDP SHALL PREVAIL AND GOVERN THE DEVELOPMENT OF ROCKY MOUNTAIN RAIL PARK; PROVIDED, HOWEVER, THAT WHERE THE PROVISIONS OF THE PUD AND FDP DO NOT ADDRESS A PARTICULAR SUBJECT, THE RELEVANT PROVISIONS OF THE ADAMS COUNTY DEVELOPMENT STANDARDS AND REGULATIONS SHALL APPLY.

2. DIFFERING USES OF INTERNAL LOTS SHALL BE BUFFERED BY WAY OF FENCES OR PLANTINGS.

COLORADO AIR AND SPACE PORT RESTRICTIONS

ALL PROPOSED DEVELOPMENTS WITHIN THE ROCKY MOUNTAIN RAIL PARK SHALL ADHERE TO SECTIONS 3-34 (COLORADO SPACE PORT INFLUENCE ZONE) AND 3-35 (COLORADO SPACE PORT NOISE OVERLAY) OF THE ADAMS COUNTY DEVELOPMENT STANDARDS AND REGULATIONS.

OVERVIEW

IN ACCORDANCE WITH THE PLANNED UNIT DEVELOPMENT ACT OF 1972, THE OBJECTIVE OF A PLANNED UNIT DEVELOPMENT IS TO ESTABLISH AN AREA OF LAND, CONTROLLED BY ONE OR MORE LANDOWNERS, TO BE DEVELOPED UNDER UNIFIED CONTROL OR UNIFIED PLAN OF DEVELOPMENT FOR A NUMBER OF DWELLING UNITS, COMMERCIAL, EDUCATIONAL, RECREATIONAL, OR INDUSTRIAL USES, OR ANY COMBINATION FOREGOING, THE PLAN WHICH DOES NOT CORRESPOND IN LOT SIZE, BULK, OR TYPE OF USE, DENSITY, LOT COVERAGE, OPEN SPACE, OR OTHER RESTRICTIONS TO THE EXISTING LAND USE REGULATIONS.

PROJECT CONCEPT

ROCKY MOUNTAIN RAIL PARK IS DESIGNED AS A SELF SUSTAINING, MIXED USE DEVELOPMENT THAT WILL ENRICH THE ECONOMY OF THE GREATER ADAMS COUNTY AND TOWN OF BENNETT COMMUNITY. THE PLAN FOR ROCKY MOUNTAIN RAIL PARK FORMS THE BASIS OF A MIXED USE DEVELOPMENT THAT INTEGRATES JOBS, RAIL ACCESS, AND A SUPPORTING COMMUNITY FOR THE COLORADO AIR AND SPACE PORT. ROCKY MOUNTAIN RAIL PARK WILL INCORPORATE THE FOLLOWING IN ORDER TO ENHANCE THE LOCAL COMMUNITY AND ADJACENT COLORADO AIR AND SPACE PORT:

1. OFFERING A VARIETY OF EMPLOYMENT OPPORTUNITIES IN THE COMMERCIAL AND INDUSTRIAL SECTOR AT VARYING SCALES TO MEET GROWING DEMANDS IN ADAMS COUNTY AND TO SERVE A BROAD RANGE OF USERS.

2. CREATING A DIVERSITY OF INDUSTRIAL, COMMERCIAL, AND AGRICULTURAL OFFERINGS TO ENHANCE COMMUNITY VARIETY AND INTEREST.

3. MAINTAINING ACCESSIBILITY TO MULTI-MODAL FORMS OF TRANSPORTATION.

4. TO PROVIDE AN ANCHOR FOR ADAMS COUNTY AND THE TOWN OF BENNETT TO FACILITATE THE GROWTH OF INDUSTRIAL AND COMMERCIAL USE IN THE REGION.

5. SERVING AS A MAGNET TO ATTRACT A DIVERSE GROUP OF USERS FROM A WIDE SPECTRUM OF SOCIAL AND ECONOMIC BACKGROUNDS.

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

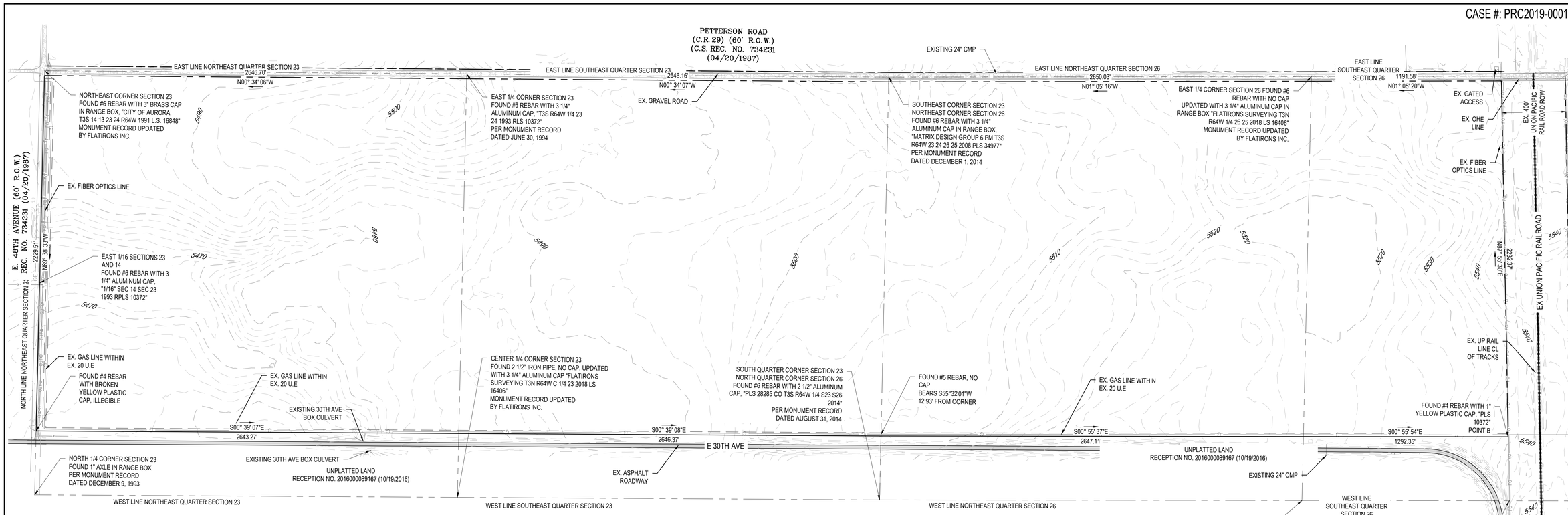
ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

GENERAL NOTES

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	GEN-2.0
SHEET	2 OF 9

CASE #: PRC2019-00012



LEGEND

- PROPERTY BOUNDARY
- EXISTING ASPHALT ROADWAY
- SECTION LINE
- RIGHT OF WAY
- GRAVEL/ACCESS ROAD
- EXISTING 10' CONTOUR
- EXISTING 2' CONTOUR
- EXISTING FIBER OPTIC LINE
- EXISTING RAILROAD TRACKS
- EXISTING GAS LINE
- EX FENCE

0' 150 300 600

SCALE: 1"=300'

NO.	DATE	DSGN	DR	CHK	APVD	BY	APVD
			REVISION				

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DENVER, COLORADO 80202

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ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

ROCKY MOUNTAIN RAIL PARK FINAL DEVELOPMENT PLAN

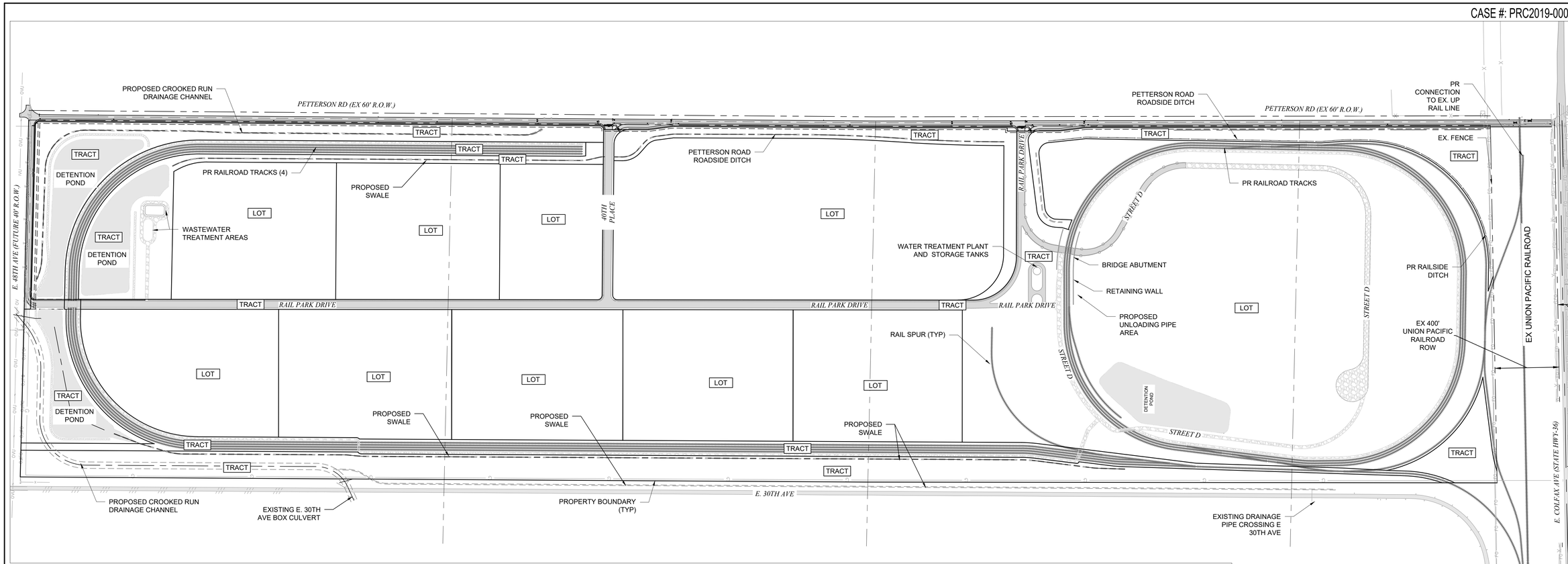
ADAMS COUNTY, COLORADO

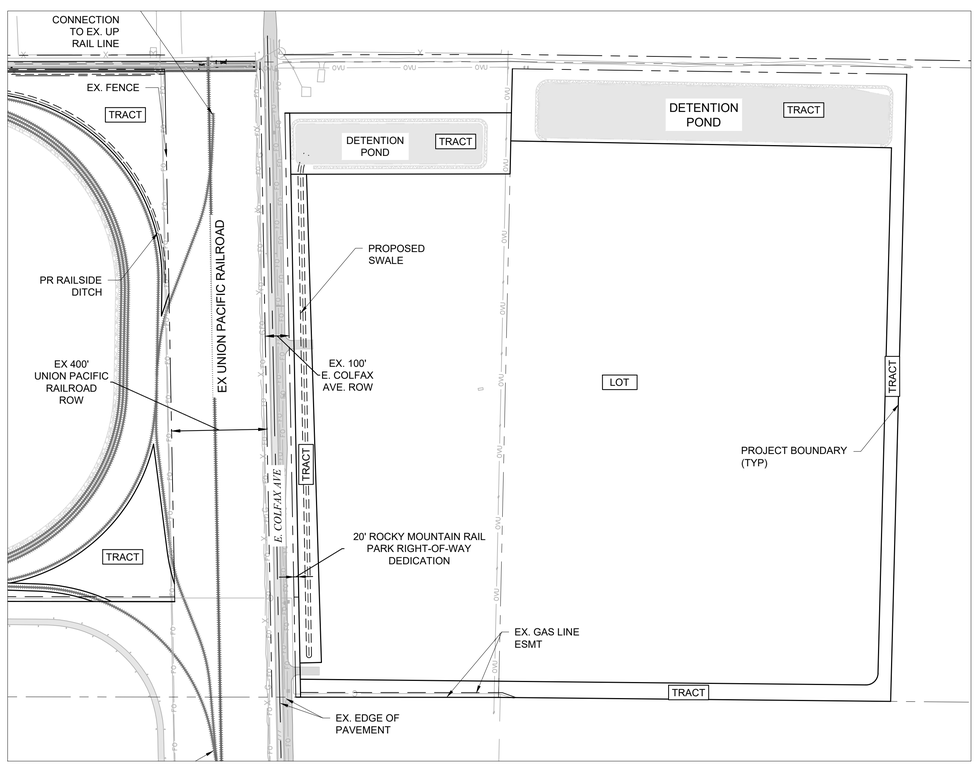
EXISTING CONDITIONS PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	EXC-3.0
SHEET	3 OF 9

CASE #: PRC2019-00012





OWNERSHIP AND MAINTENANCE TABLE

LOTS	AREA	OWNER	DEDICATIONS	MAINTENANCE	USE
Lot 1		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot2		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 3		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 4		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 5		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 6		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 7		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 8		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 9		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 10		RAIL LAND COMPANY, LLC		OWNER	PUD
Lot 11		RAIL LAND COMPANY, LLC		OWNER	PUD
SUB TOTAL LOT AREA (AC)	460.42				
TRACTS					
TRACT A		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	ACCESS, DRN, LA, OS, U
TRACT B		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT C		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT D		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT E		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT F		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT G		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT H		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT I		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT J		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT K		RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
SUB TOTAL TRACT AREA (AC)	154.35				
Total	**614.77**				

**RMRPMD = ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT
AR = ACTIVE RECREATION
DRN = DRAINAGE
LA = LANDSCAPING
OS = OPEN SPACE
U = UTILITIES

LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE & CHANNEL CENTERLINE
- SWALE & CHANNEL TOP AND TOE
- GRAVEL/ACCESS ROAD

0' 150' 300' 600'
SCALE: 1"=300'

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

ROCKY MOUNTAIN RAIL PARK FINAL DEVELOPMENT PLAN

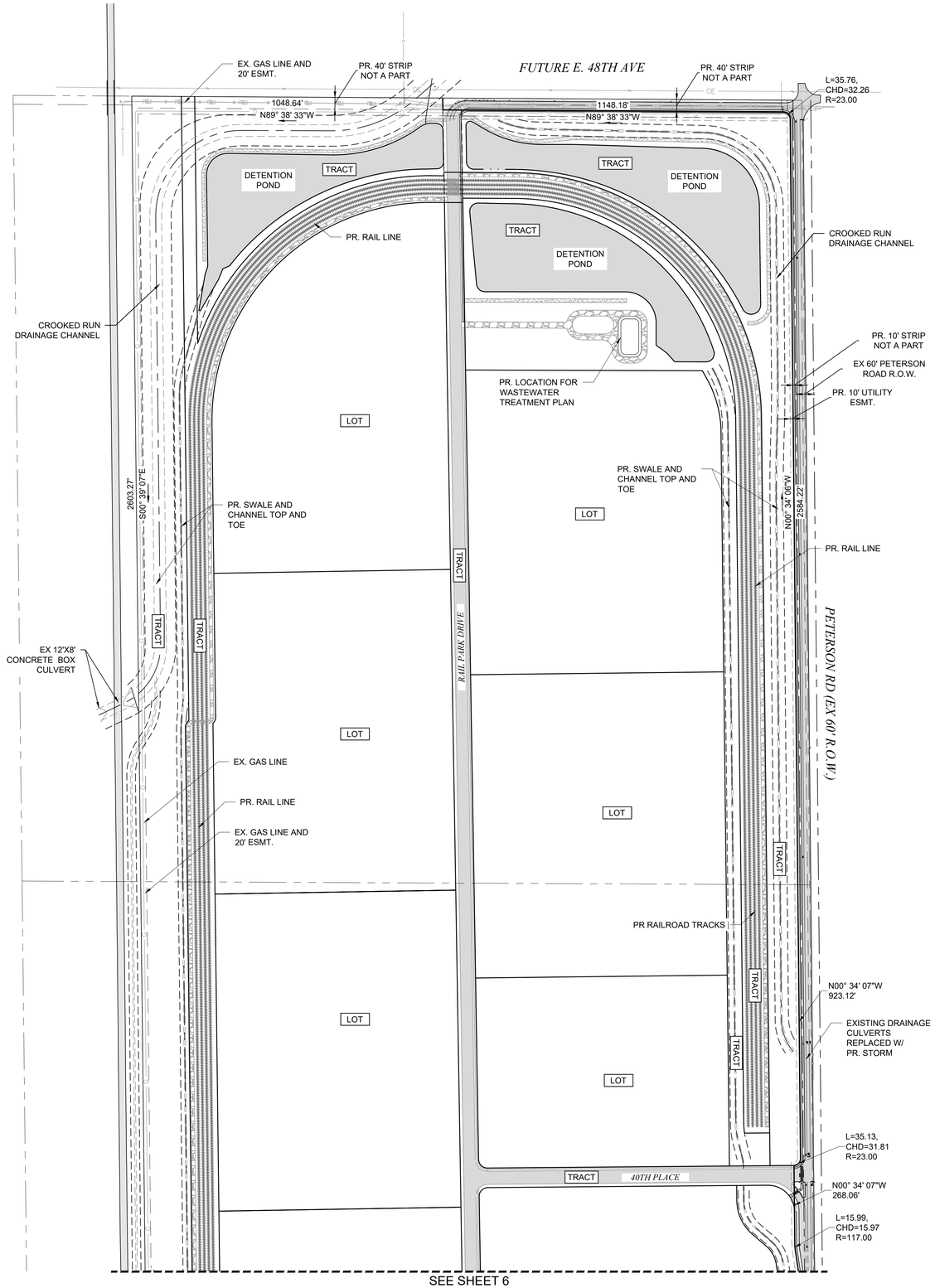
ADAMS COUNTY, COLORADO

OVERALL SITE PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	SIT-4.0
SHEET	4 OF 9

CASE #: PRC2019-00012



SEE SHEET 6

LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- EASEMENT
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE CENTERLINE
- GRAVEL/ACCESS ROAD

N

0' 100 200 400

SCALE: 1"=200'

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN RAIL PARK
EST. 2017
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

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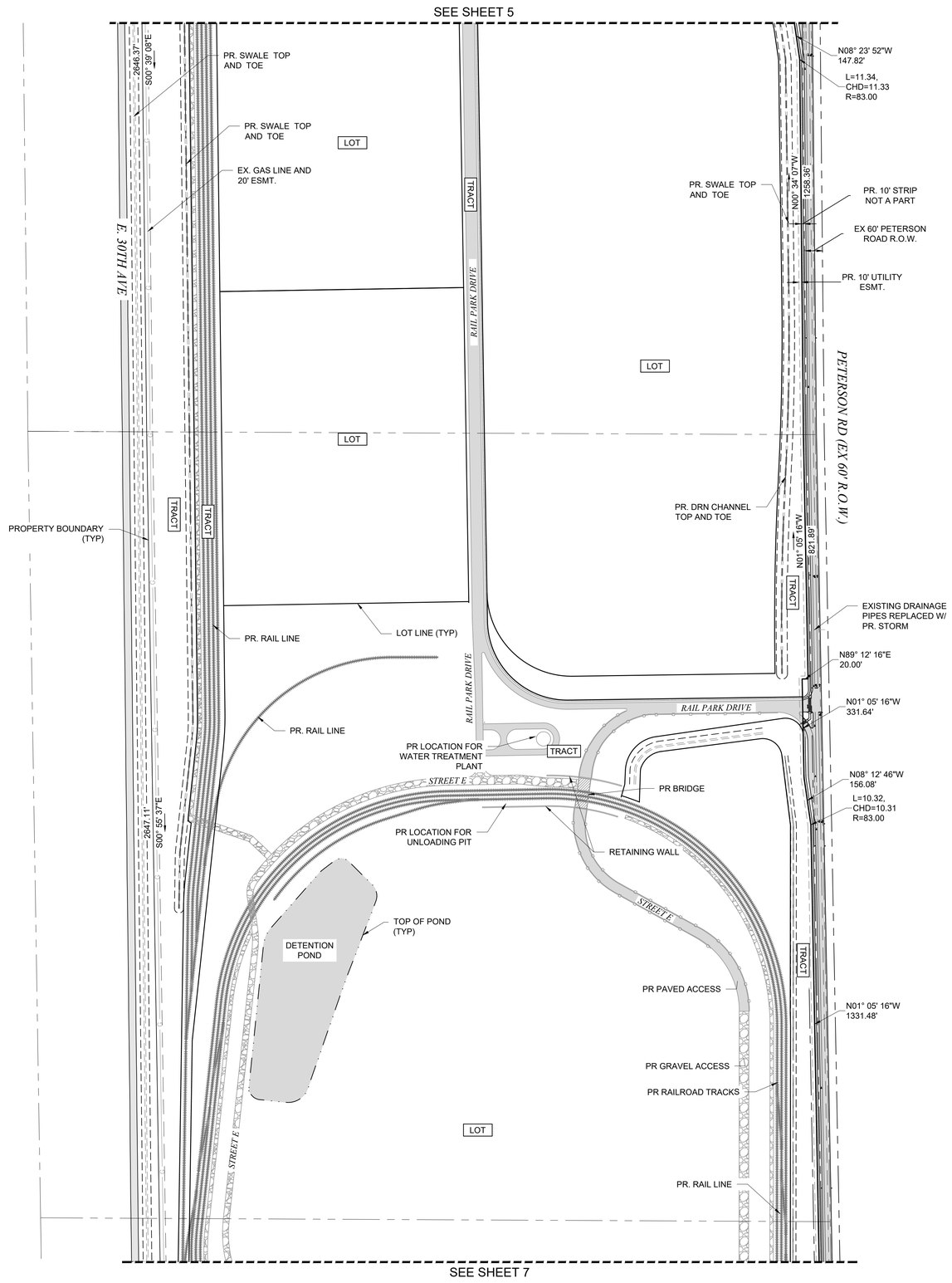
ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

SITE PLAN - 1

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	SIT-4.1
SHEET	5 OF 9

CASE #: PRC2019-00012



LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- EASEMENT
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE CENTERLINE
- GRAVEL/ACCESS ROAD

N

0' 100 200 400

SCALE: 1"=200'

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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ROCKY MOUNTAIN RAIL AUTHORITY
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

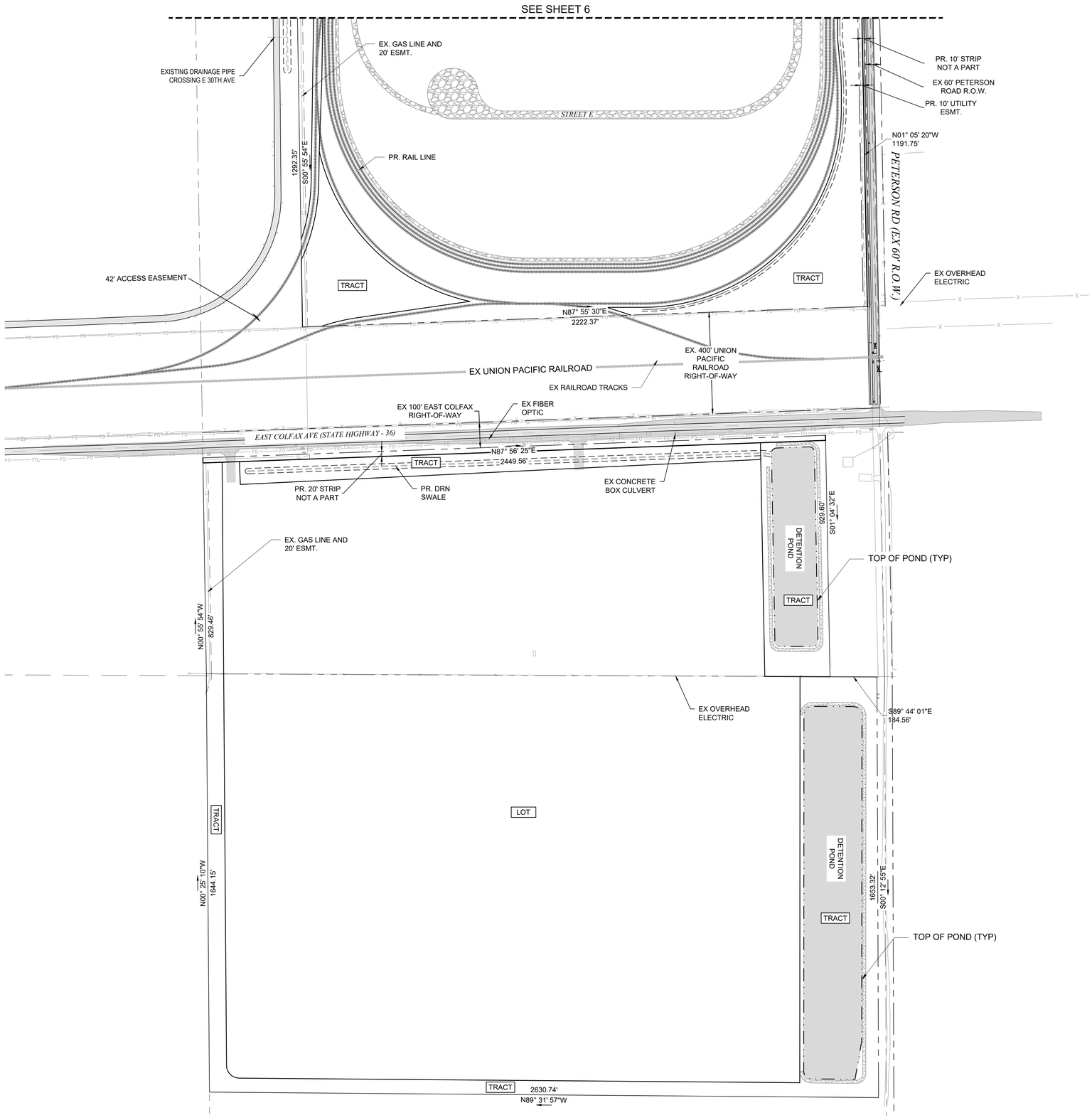
ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

SITE PLAN - 2

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	SIT-4.2
SHEET	6 OF 9

CASE #: PRC2019-00012



LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- EASEMENT
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE CENTERLINE
- GRAVEL/ACCESS ROAD

N

0' 100 200 400

SCALE: 1"=200'

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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DENVER, COLORADO 80202

ROCKY MOUNTAIN RAIL AUTHORITY
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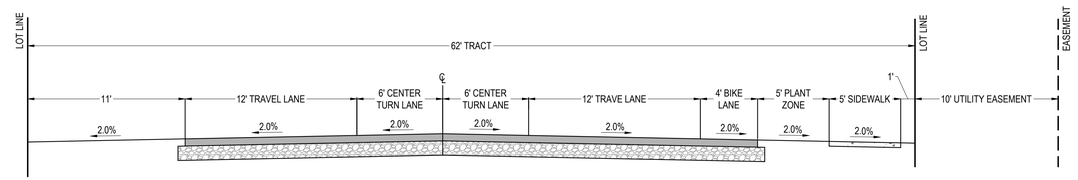
ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

SITE PLAN - 3

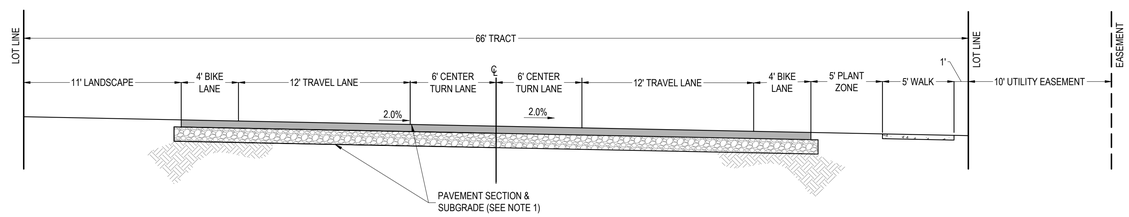
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	SIT-4.3
SHEET	7 OF 9

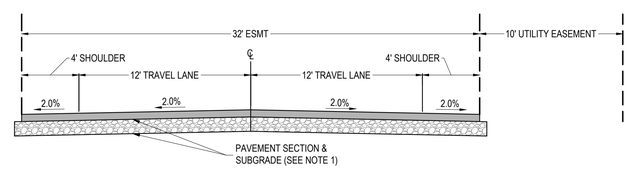
CASE #: PRC2019-00012



C RAIL PARK DRIVE (ADAMS COUNTY)
1" = 5'



D 40TH PLACE (ADAMS COUNTY)
1" = 5'



F STREET D (ADAMS COUNTY)
1" = 5'

NOTES

1. PROPOSED PAVEMENT SECTION SHALL BE IN ACCORDANCE WITH THE APPROVED PAVEMENT DESIGN AND GEOTECHNICAL STUDY FOR THE PROJECT, PRESIDING JURISDICTION VARIES.
2. EAST COLFAX AVENUE ULTIMATE SECTION IS A CONCEPTUAL SECTION FOR IMPROVEMENTS. ULTIMATE SECTION SHALL BE IN ACCORDANCE WITH THE APPROVED TRAFFIC STUDY FOR THE PROJECT.
3. EAST COLFAX AVENUE CENTERLINE BASED ON APPROXIMATE CENTERLINE OF EXISTING PAVEMENT SECTION.

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

REUSE OF DOCUMENTS: THIS DOCUMENT, AND THE IDEAS AND DESIGNS INCORPORATED HEREIN, AS AN INSTRUMENT OF PROFESSIONAL SERVICE, IS THE PROPERTY OF ENERTIA CONSULTING GROUP AND IS NOT TO BE USED, IN WHOLE OR IN PART, FOR ANY OTHER PROJECT WITHOUT THE WRITTEN AUTHORIZATION OF ENERTIA CONSULTING GROUP.

ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

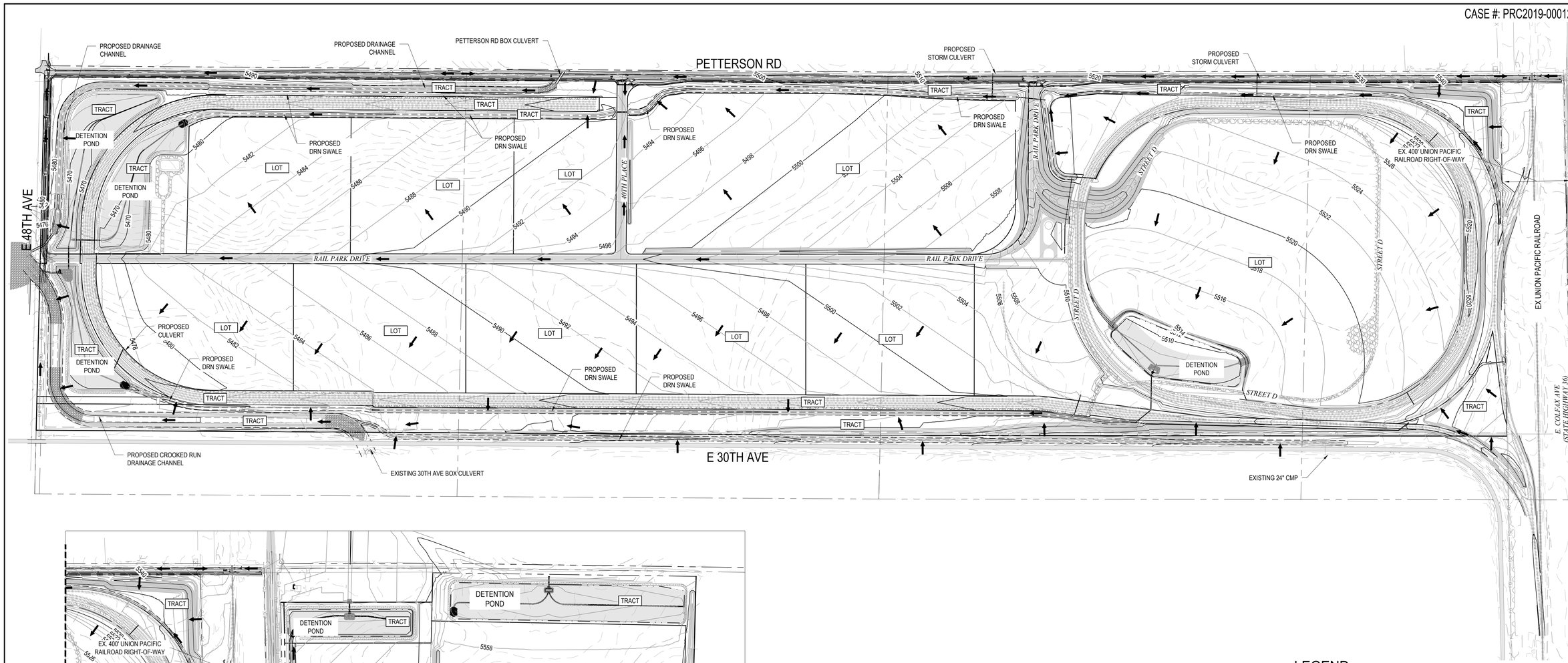
ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

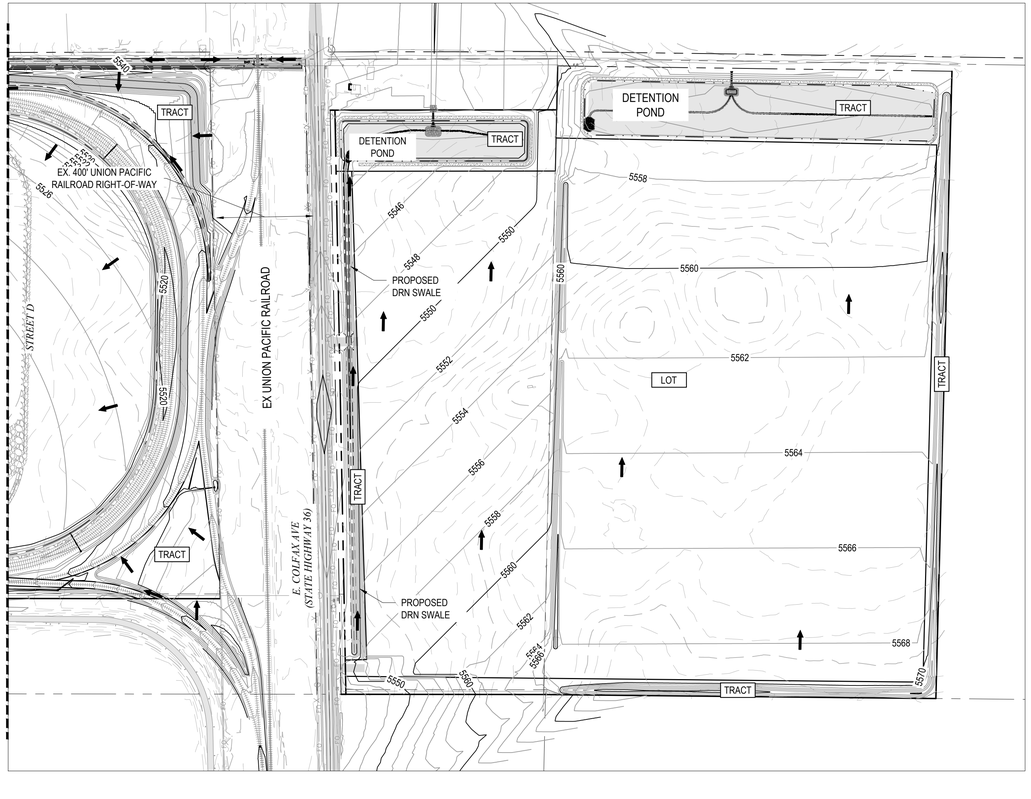
TYPICAL STREET SECTIONS & DETAILS - 1

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	DTL-5.0
SHEET	8 OF 9

CASE #: PRC2019-00012





LEGEND

- PROPERTY BOUNDARY
- ROAD CENTER LINE
- EX EDGE OF ROAD
- PR EDGE OF PAVEMENT
- LOT LINE
- RIGHT OF WAY LINE
- SECTION LINE
- EASEMENT
- DRAINAGE PIPES
- RAIL TRACKS
- SWALE CENTERLINE
- GRAVEL/ACCESS ROAD
- PR 10' CONTOUR
- PR 2' CONTOUR
- EX 10' CONTOUR
- EX 2' CONTOUR

0' 150 300 600

SCALE: 1"=300'

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

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ROCKY MOUNTAIN RAIL PARK
FINAL DEVELOPMENT PLAN
ADAMS COUNTY, COLORADO

OVERALL GRADING PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	06/17/2020
FILE	
DWG	GRD-6.0
SHEET	9 OF 9

ROCKY MOUNTAIN RAIL PARK FILING NO. 1

PRC2019-00012

LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

SHEET 1 OF 7

FINAL PLAT

Certificate of Dedication and Ownership

KNOW ALL MEN BY THESE PRESENTS THAT RAIL LAND COMPANY LLC, A COLORADO LIMITED LIABILITY COMPANY, BEING THE SOLE OWNER OF THE FOLLOWING DESCRIBED TRACT OF LAND:

LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO, DESCRIBED AS FOLLOWS:

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 TO BEAR NORTH 89°24'56" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON BEING RELATIVE THERETO.

COMMENCING AT SAID CENTER QUARTER CORNER OF SECTION 35; THENCE ALONG THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, NORTH 00°25'10" WEST, A DISTANCE OF 986.62 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING ALONG SAID WEST LINE, NORTH 00°25'10" WEST, A DISTANCE OF 1644.15 FEET TO THE SOUTH QUARTER CORNER OF SECTION 26; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°55'54" WEST, A DISTANCE OF 829.46 FEET TO A POINT ON A LINE TWENTY FEET (20') SOUTH OF AND PARALLEL WITH THE SOUTH RIGHT-OF-WAY LINE OF EAST COLFAX AVENUE, SAID POINT BEING "POINT A"; THENCE ALONG SAID PARALLEL LINE, NORTH 87°55'30" EAST, A DISTANCE OF 2449.56 FEET; THENCE SOUTH 01°04'32" EAST, A DISTANCE OF 929.60 FEET TO THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SECTION 26; THENCE ALONG SAID ALIQUOT LINE, SOUTH 89°44'09" EAST, A DISTANCE OF 184.56 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF PETTERSON ROAD; THENCE ALONG SAID WEST RIGHT-OF-WAY LINE, SOUTH 00°12'55" EAST, A DISTANCE OF 1653.32 FEET, MORE OR LESS; THENCE DEPARTING SAID RIGHT-OF-WAY LINE, NORTH 89°31'58" WEST, A DISTANCE OF 2630.74 FEET TO A POINT ON THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, SAID POINT ALSO BEING THE POINT OF BEGINNING.

ALSO COMMENCING AT SAID POINT A; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°55'54" WEST, A DISTANCE OF 520.07 FEET TO A POINT ON THE NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID NORTH RIGHT-OF-WAY LINE, NORTH 87°55'30" EAST, A DISTANCE OF 400.10 FEET TO A POINT, SAID POINT BEING "POINT B", A SECOND POINT OF BEGINNING; THENCE NORTH 00°55'54" WEST, A DISTANCE OF 1292.35 FEET; THENCE NORTH 00°55'37" WEST, A DISTANCE OF 2646.15 FEET; THENCE NORTH 00°39'08" WEST, A DISTANCE OF 2647.33 FEET; THENCE NORTH 00°39'07" WEST, A DISTANCE OF 2603.27 FEET TO A POINT ON A LINE FORTY FEET (40') SOUTH OF AND PARALLEL WITH THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 23; THENCE ALONG SAID PARALLEL LINE, SOUTH 89°38'34" EAST, A DISTANCE OF 2196.82 FEET TO A POINT OF CURVATURE; THENCE 35.76 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 23.00 FEET AND AN INCLUDED ANGLE OF 89°04'27", SUBTENDED BY A CHORD BEARING SOUTH 45°06'20" EAST, A DISTANCE OF 32.26 FEET; THENCE SOUTH 00°34'06" EAST, A DISTANCE OF 2584.22 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 923.12 FEET TO A POINT OF CURVATURE; THENCE 35.13 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 23.00 FEET AND AN INCLUDED ANGLE OF 87°30'29", SUBTENDED BY A CHORD BEARING SOUTH 43°11'07" WEST, A DISTANCE OF 31.81 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 268.06 FEET TO A POINT OF CURVATURE; THENCE 15.99 FEET ALONG THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 117.00 FEET AND AN INCLUDED ANGLE OF 07°49'45", SUBTENDED BY A CHORD BEARING, SOUTH 04°29'00" EAST, A DISTANCE OF 15.97 FEET; THENCE SOUTH 08°23'52" EAST, A DISTANCE OF 147.82 FEET TO A POINT OF CURVATURE; THENCE 11.34' FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 83.00 FEET AND AN INCLUDED ANGLE OF 07°49'45", SUBTENDED BY A CHORD BEARING SOUTH 04°29'00" EAST, A DISTANCE OF 11.33 FEET; THENCE SOUTH 00°34'07" EAST, A DISTANCE OF 1258.36 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 821.89 FEET; THENCE SOUTH 89°12'16" WEST, A DISTANCE OF 20.00 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 324.35 FEET TO A POINT OF CURVATURE; THENCE 14.55 FEET ALONG THE ARC OF A CURVE TO THE LEFT, HAVING A RADIUS OF 117.00 FEET AND AN INCLUDED ANGLE OF 07°07'30", SUBTENDED BY A CHORD BEARING SOUTH 04°39'01" EAST, A DISTANCE OF 14.55' FEET; THENCE SOUTH 08°12'46" EAST, A DISTANCE OF 148.79 FEET TO A POINT OF CURVATURE; THENCE 10.32 FEET ALONG THE ARC OF A CURVE TO THE RIGHT, HAVING A RADIUS OF 83.00 FEET AND AN INCLUDED ANGLE OF 07°07'30", SUBTENDED BY A CHORD BEARING SOUTH 04°39'01" EAST, A DISTANCE OF 10.31 FEET; THENCE SOUTH 01°05'16" EAST, A DISTANCE OF 1331.48 FEET; THENCE SOUTH 01°05'20" EAST, A DISTANCE OF 1191.75 FEET TO A POINT ON SAID NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID RIGHT-OF-WAY LINE, SOUTH 87°55'30" WEST, A DISTANCE OF 2222.37 FEET, MORE OR LESS, TO POINT B, THE SECOND POINT OF BEGINNING.

CONTAINING 26,737,891 SQ. FT. OR 613.81 ACRES MORE OR LESS.

HAS BY THESE PRESENTS LAID OUT, PLATTED AND SUBDIVIDED THE SAME INTO LOTS, TRACTS AND EASEMENTS AS SHOWN ON THIS PLAT UNDER THE NAME AND STYLE OF "ROCKY MOUNTAIN RAIL PARK FILING NO. 1", A SUBDIVISION IN THE COUNTY OF ADAMS, STATE OF COLORADO.

THE UNDERSIGNED DOES HEREBY DEDICATE, GRANT AND CONVEY TO ADAMS COUNTY THOSE PUBLIC EASEMENTS AS SHOWN ON THE PLAT, AND FURTHER RESTRICTS THE USE OF ALL PUBLIC EASEMENT TO ADAMS COUNTY AND/OR ITS ASSIGNS, PROVIDED HOWEVER, THAT THE SOLE RIGHT AND AUTHORITY TO RELEASE OR QUITCLAIM ALL OR ANY SUCH PUBLIC EASEMENTS SHALL REMAIN EXCLUSIVELY VESTED IN ADAMS COUNTY.

PRIVATE STREET, RAIL PARK DRIVE, SHALL BE PRIVATELY OWNED AND MAINTAINED BY ROCKY MOUTAIN RAIL PARK METRO DISTRICT.

FOR THE APPROVAL OF "ROCKY MOUNTAIN RAIL PARK FILING NO. 1" AND THE DEDICATIONS AND CONDITIONS WHICH APPLY

THERETO THIS _____ DAY OF _____, 2020.

BY: RAIL LAND COMPANY, LLC, A COLORADO LIMITED LIABILITY COMPANY

_____
NAME, TITLE

ACKNOWLEDGMENT

STATE OF _____)
) SS.
COUNTY OF _____)

THE FOREGOING INSTRUMENT WAS ACKNOWLEDGED BEFORE ME THIS _____
DAY OF _____, 2020, BY _____ AS _____
OF RAIL LAND COMPANY, LLC, A COLORADO LIMITED LIABILITY COMPANY

BY _____

WITNESS MY HAND AND SEAL.

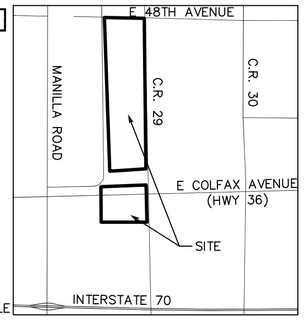
MY COMMISSION EXPIRES: _____

[SEAL]

_____
NOTARY PUBLIC

Notes

1. FIDELITY NATIONAL TITLE INSURANCE COMPANY COMMITMENT NUMBERS N0011731-020-PN-DS1-1, DATED JANUARY 16, 2020 AT 8:00 A.M. AND N0017815-020-PN-DS1, AMENDMENT NO. 1, DATED JANUARY 16, 2020 AT 8:00 A.M., WERE ENTIRELY RELIED UPON FOR RECORDED INFORMATION REGARDING RIGHTS-OF-WAY, EASEMENTS AND ENCUMBRANCES IN THE PREPARATION OF THIS SURVEY. THE PROPERTY SHOWN AND DESCRIBED HEREON IS A PORTION OF THE PROPERTY DESCRIBED IN SAID TITLE COMMITMENTS.
2. ACCORDING TO COLORADO LAW, YOU MUST COMMENCE ANY LEGAL ACTION BASED UPON ANY DEFECT IN THIS EXHIBIT WITHIN THREE YEARS AFTER YOU FIRST DISCOVER SUCH DEFECT. IN NO EVENT MAY ANY ACTION BASED UPON ANY DEFECT IN THIS EXHIBIT BE COMMENCED MORE THAN TEN YEARS FROM THE DATE OF THE CERTIFICATION SHOWN HEREON.
3. THIS EXHIBIT IS VALID ONLY IF PRINT HAS SEAL AND SIGNATURE OF SURVEYOR.
4. BASIS OF BEARINGS: GPS DERIVED BEARINGS BASED ON A BEARING OF N89°24'56"W ALONG THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35, BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND 6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" AS SHOWN HEREON. COLORADO STATE PLANE COORDINATE SYSTEM, CENTRAL ZONE (0502), NORTH AMERICAN DATUM 1983 (NAD83). ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.
5. THE DISTANCE MEASUREMENTS SHOWN HEREON ARE U.S. SURVEY FOOT.
6. ANY PERSON WHO KNOWINGLY REMOVES, ALTERS OR DEFACES ANY PUBLIC LAND SURVEY MONUMENT AND/OR BOUNDARY MONUMENT OR ACCESSORY, COMMITS A CLASS TWO (2) MISDEMEANOR PURSUANT TO STATE STATUTE C.R.S. SEC 18-4-508. WHOEVER WILLFULLY DESTROYS, DEFACES, CHANGES, OR REMOVES TO ANOTHER PLACE ANY SECTION CORNER, QUARTER-SECTION CORNER, OR MEANDER POST, ON ANY GOVERNMENT LINE OF SURVEY, OR WILLFULLY CUTS DOWN ANY WITNESS TREE OR ANY TREE BLAZED TO MARK THE LINE OF A GOVERNMENT SURVEY, OR WILLFULLY DEFACES, CHANGES, OR REMOVES ANY MONUMENT OR BENCH MARK OF ANY GOVERNMENT SURVEY, SHALL BE FINED UNDER THIS TITLE OR IMPRISONED NOT MORE THAN SIX MONTHS, OR BOTH. 18 U.S.C. § 1858.
7. THE WORD "CERTIFY" AS SHOWN AND USED HEREON MEANS AN EXPRESSION OF PROFESSIONAL OPINION REGARDING THE FACTS OF THIS SURVEY AND DOES NOT CONSTITUTE A WARRANTY OR GUARANTEE, EXPRESSED OR IMPLIED.
8. FLOOD INFORMATION: THE SUBJECT PROPERTY IS LOCATED IN ZONE X UNSHADED, AREAS DETERMINED TO BE OUTSIDE THE 0.2% ANNUAL CHANCE FLOODPLAIN ACCORDING TO THE FEMA FLOOD INSURANCE RATE MAP; COMMUNITY-PANEL NO. 08001C-0695 H, DATED MARCH 5, 2007 AND THE FEMA FLOOD INSURANCE RATE MAP; COMMUNITY-PANEL NO. 08001C-0690 H, DATED MARCH 5, 2007. FLOOD INFORMATION IS SUBJECT TO CHANGE.
9. DATES OF SURVEY: MARCH 13-16, 2018 (CREW CHIEF T. FENDICK)
10. LOT OWNERS SHALL BE RESPONSIBLE FOR THE OPERATION AND MAINTENANCE OF ALL STORM DRAINAGE FACILITIES LOCATED ON RESPECTIVE LOT(S) INCLUDING INLETS, PIPES, CULVERTS, CHANNELS, DITCHES, HYDRAULIC STRUCTURES, AND DETENTION BASINS. THE ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT ('DISTRICT') SHALL BE RESPONSIBLE FOR THE OPERATIONS AND MAINTENANCE OF STORM DRAINAGE FACILITIES ON PRIVATELY OWNED LOTS LOCATED WITHIN MAINTENANCE AND ACCESS EASEMENT(S) DEDICATED TO THE DISTRICT BY SEPARATE INSTRUMENTS. SHOULD A LOT OWNER, OR THE DISTRICT WHERE APPLICABLE, FAIL TO MAINTAIN SAID STORM DRAINAGE FACILITIES, THE COUNTY SHALL HAVE THE RIGHT TO ENTER UPON SAID LOT FOR THE SOLE PURPOSE OF OPERATIONS AND MAINTENANCE OF THE STORM DRAINAGE FACILITIES.
11. BY THIS PLAT, TRACT A IS CREATED FOR USE BY THE OWNERS OF THE LOTS FOR ACCESS ONTO 48TH AVENUE AND PETTERSON ROAD. THE CONSTRUCTION AND MAINTENANCE OF A PRIVATE ROAD WITHIN THE TRACT A IS THE FULL AND COMPLETE RESPONSIBILITY OF THE RMRP METROPOLITAN DISTRICT, IN PERPETUITY. ADAMS COUNTY ASSUMES NO RESPONSIBILITY OR LIABILITY REGARDING THE PRIVATE ROAD, AND WILL NOT PERFORM MAINTENANCE OPERATIONS INCLUDING SNOW REMOVAL.
12. ALL STREETS WITHIN LOTS ARE OWNED BY RAIL LAND COMPANY LLC. ALL STREETS LOCATED WITHIN TRACTS ARE DEDICATED TO THE RMRP METROPOLITAN DISTRICT. RMRPMD SHALL BE RESPONSIBLE FOR MAINTENANCE OF ROADS WITHIN TRACT A AND IN LOCATIONS WHERE THERE ARE PUBLIC ACCESS EASEMENTS.
13. 10' UTILITY EASEMENT, ALONG E. 48TH AVENUE AND PETTERSON ROAD, IS DEDICATED TO ADAMS COUNTY FOR THE BENEFIT OF THE APPLICABLE UTILITY PROVIDERS FOR THE INSTALLATION, MAINTENANCE, AND REPLACEMENT OF ELECTRIC, GAS, TELEVISION, CABLE, AND TELECOMMUNICATIONS FACILITIES (DRY UTILITIES). PERMANENT STRUCTURES, IMPROVEMENTS, OBJECTS, BUILDINGS, WELLS, (INTERFERING OBJECTS) SHALL NOT BE PERMITTED WITHIN SAID UTILITY EASEMENTS AND THE UTILITY PROVIDERS, AS GRANTEES, MAY REMOVE ANY INTERFERING OBJECTS AT NO COST TO SUCH GRANTEES, INCLUDING, WITHOUT LIMITATION, VEGETATION. PUBLIC SERVICE COMPANY OF COLORADO (PSCO) AND ITS SUCCESSORS RESERVE THE RIGHT TO REQUIRE ADDITIONAL EASEMENTS AND TO REQUIRE THE PROPERTY OWNER TO GRANT PSCO AN EASEMENT ON ITS STANDARD FORM.
14. THE ROAD MAINTENANCE PLAN FOR THE PRIVATE ROADS IS ON FILE WITH THE ADAMS COUNTY CLERK AND RECORDER'S OFFICE AT RECEPTION #_____.
15. DETENTION PONDS ARE INTENDED TO BE "DRY" PONDS WHICH MINIMIZE MOSQUITOS. IN THE EVENT THAT MOSQUITO ABATEMENT IS REQUIRED, THE APPLICANT OR ITS DESIGNEE SHALL BE RESPONSIBLE TO PAY FOR A SERVICE TO MITIGATE IMPACT TO ADJACENT PROPERTIES CAUSED BY THE DETENTION PONDS.



Vicinity Map
NOT TO SCALE -
MAP DATA ©2018 GOOGLE

Board of County Commissioners Approval

APPROVED BY THE ADAMS COUNTY BOARD OF COMMISSIONERS THIS ____ DAY OF _____, 2020.

_____
CHAIR

Clerk and Recorder

THIS FINAL PLAT WAS FILED FOR RECORD IN THE OFFICE OF THE ADAMS COUNTY CLERK AND RECORDER IN THE STATE OF COLORADO, AT _____M. ON THE ____ DAY OF _____, 2020.

_____
COUNTY CLERK AND RECORDER

BY: _____
DEPUTY

RMRP Metropolitan District Approval

RMRPMD APPROVES AND ACCEPTS THE DEDICATION OF THE FOLLOWING: TRACTS A, B, C, D, E, F, G, H, I, J, AND K.

ROCKY MOUNTAIN RAIL PARK METROPOLITAN DISTRICT, A QUASI-MUNICIPAL CORPORATION AND POLITICAL SUBDIVISION OF THE STATE OF COLORADO THIS_____DAY OF _____, 2020.

_____
BY:

Surveyor's Certificate

I, JOHN B. GUYTON, A DULY REGISTERED LAND SURVEYOR, LICENSED IN THE STATE OF COLORADO, HEREBY STATE FOR AND ON BEHALF OF FLATIRONS, INC. THAT THIS PLAT OF ROCKY MOUNTAIN RAIL PARK FILING NO. 1, AS SHOWN HEREON, WAS PREPARED BY ME OR UNDER MY DIRECT SUPERVISION AND THAT SAID PLAT IS IN SUBSTANTIAL ACCORDANCE WITH C.R.S. TITLE 38, ARTICLE 51 AND STANDARDS OF PRACTICE, IS ACCURATE TO THE BEST OF MY KNOWLEDGE, INFORMATION, AND BELIEF, AND EXCEPT AS SPECIFICALLY STATED HEREON IS NOT A GUARANTY OR WARRANTY, EITHER EXPRESSED OR IMPLIED.

JOHN B. GUYTON
COLORADO P.L.S. #16406

(SEAL)

Sheet Index

SHEET 1 COVER SHEET
SHEET 2 OVERALL BOUNDARY
SHEETS 3-7 INTERIOR LOT CONFIGURATION

FILE NO. _____

MAP NO. _____

RECEPTION NO. _____

Flatirons, Inc.
Surveying, Engineering & Geomatics
www.FlatironsInc.com
3825 IRIS AVE, STE 395
BOULDER, CO 80301
PH: (303) 443-7001
FAX: (303) 443-9830

JOB NUMBER: 18-71,096
DATE: 06/15/2020
DRAWN BY: M. VOYLES
CHECKED BY: BO/JZG/JK/ETB

BY:MVOYLES FILE:71096_FINAL PLAT_(NORTH)2020-06-15_.DWG DATE:6/16/2020 2:00 PM

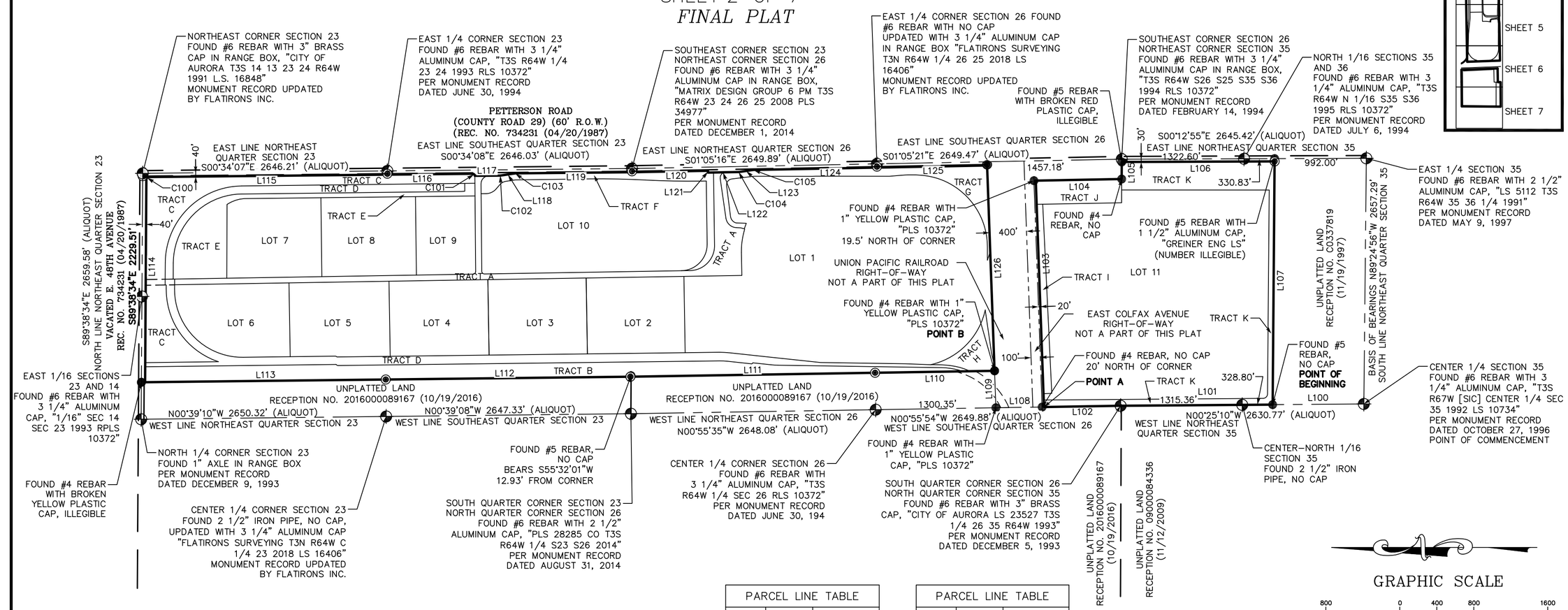
ROCKY MOUNTAIN RAIL PARK FILING NO. 1

PRC2019-00012

LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

SHEET 2 OF 7

FINAL PLAT



PARCEL LINE TABLE		
LINE #	LENGTH	DIRECTION
L100	986.62	N00°25'10"W
L101	1644.15	N00°25'10"W
L102	829.46	N00°55'54"W
L103	2449.56	N87°55'30"E
L104	929.60	S01°04'32"E
L105	184.56	S89°44'09"E
L106	1653.32	S00°12'55"E
L107	2630.74	N89°31'58"W
L108	520.07	N00°55'54"W
L109	400.10	N87°55'30"E
L110	1292.35	N00°55'54"W
L111	2646.15	N00°55'37"W
L112	2647.33	N00°39'08"W
L113	2603.27	N00°39'07"W

PARCEL LINE TABLE		
LINE #	LENGTH	DIRECTION
L114	2196.82	S89°38'33"E
L115	2584.22	S00°34'06"E
L116	923.12	S00°34'07"E
L117	268.06	S00°34'07"E
L118	147.82	S08°23'52"E
L119	1258.36	S00°34'07"E
L120	821.89	S01°05'16"E
L121	20.00	S89°12'16"W
L122	324.35	S01°05'16"E
L123	148.79	S08°12'46"E
L124	1331.48	S01°05'16"E
L125	1191.75	S01°05'20"E
L126	2222.37	S87°55'30"W

CURVE TABLE					
CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C100	35.76	23.00	89°04'27"	S45°06'20"E	32.26
C101	35.13	23.00	87°30'29"	S43°11'07"W	31.81
C102	15.99	117.00	7°49'45"	S04°29'00"E	15.97
C103	11.34	83.00	7°49'45"	S04°29'00"E	11.33
C104	14.55	117.00	7°07'30"	S04°39'01"E	14.54
C105	10.32	83.00	7°07'30"	S04°39'01"E	10.31

GRAPHIC SCALE

800 0 400 800 1600

(IN FEET)
1 inch = 800 ft.

OWNERSHIP AND MAINTENANCE TABLE

LOTS	AREA	OWNER	DEDICATIONS	MAINTENANCE	USE
LOT 1	131.55	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 2	19.98	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 3	19.99	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 4	19.99	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 5	20.00	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 6	19.99	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 7	19.99	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 8	19.99	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 9	12.49	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 10	54.86	RAIL LAND COMPANY, LLC		OWNER	PUD
LOT 11	121.59	RAIL LAND COMPANY, LLC		OWNER	PUD
SUB TOTAL LOT AREA (AC)	460.42				
TRACTS					
TRACT A	15.34	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	ACCESS, DRN, LA, OS, U
TRACT B	27.86	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT C	24.13	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT D	25.38	RAIL LAND COMPANY, LLC	RLC, LLC	RMRPMD	DRN, LA, OS, U
TRACT E	12.82	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT F	6.90	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT G	9.78	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT H	3.62	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT I	3.66	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT J	5.44	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
TRACT K	18.46	RAIL LAND COMPANY, LLC	RMRPMD	RMRPMD	DRN, LA, OS, U
SUB TOTAL TRACT AREA (AC)	153.39				
TOTAL	**613.81**				

**RMRPMD = ROCKY MOUNTAIN RAIL PARK METRO DISTRICT
RLC, LLC = RAIL LAND COMPANY, LLC

Legend

- FOUND ALIQUOT MONUMENT AS DESCRIBED
- FOUND MONUMENT AS DESCRIBED
- SET 18" #5 REBAR WITH 1 1/2" ALUMINUM CAP "FLATIRONS SURV 16406"
- BOUNDARY LINE
- ADJACENT PROPERTY LINE
- LOT AND/OR TRACT LINE
- SECTION LINE
- RIGHT-OF-WAY LINE
- EXISTING EASEMENT LINE

Overall Boundary

Flatirons, Inc.
Surveying, Engineering & Geomatics
www.FlatironsInc.com
3825 IRIS AVE, STE 395
BOULDER, CO 80301
PH: (303) 443-7001
FAX: (303) 443-9830

(SEAL)

JOB NUMBER: 18-71,096
DATE: 06/15/2020
DRAWN BY: M. VOYLES
CHECKED BY: BO/JZG/JK/ETB

BY:MVOYLES FILE:71096_FINAL PLAT_(NORTH)2020-06-15_.DWG DATE:6/16/2020 2:00 PM

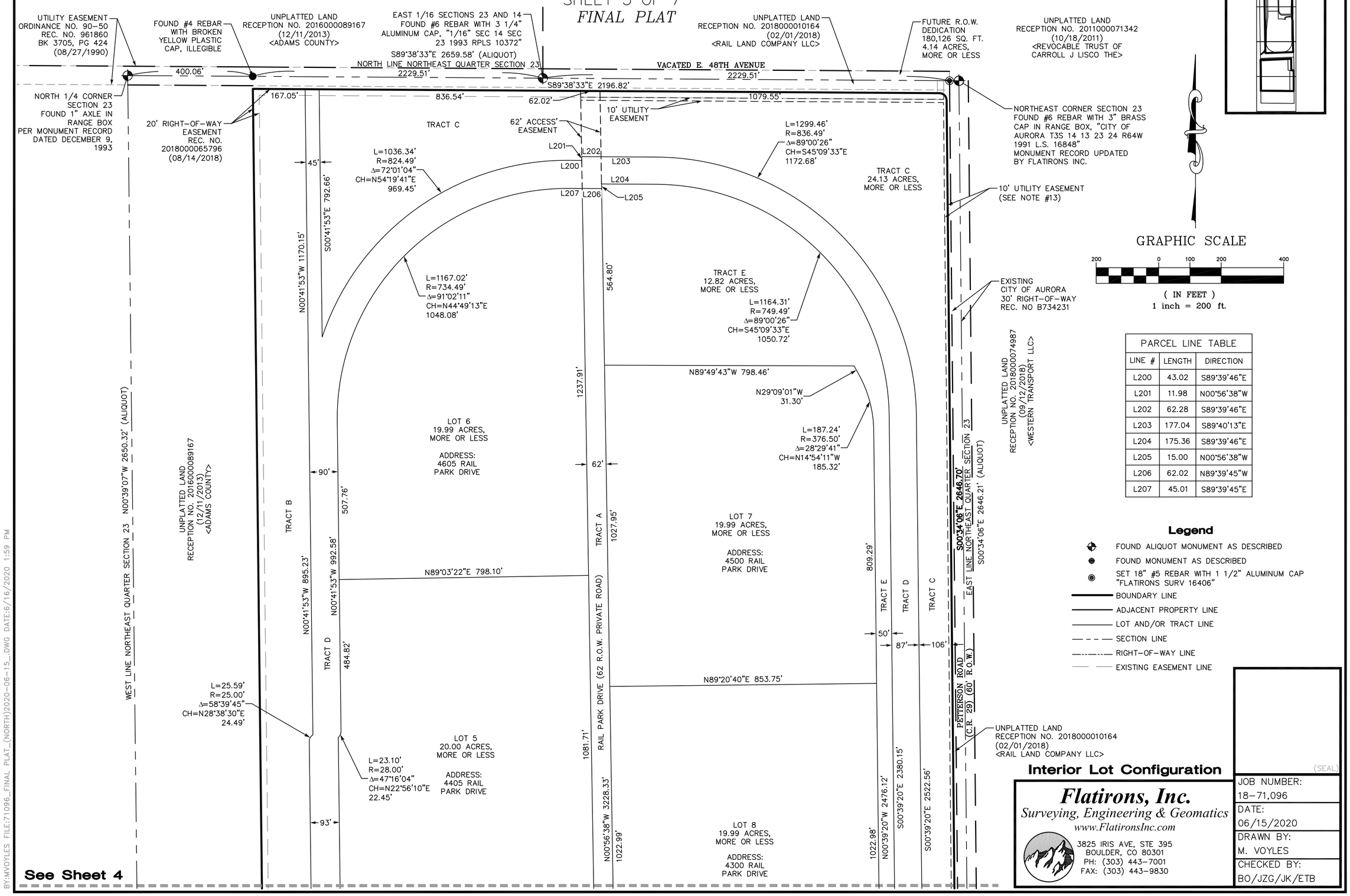
ROCKY MOUNTAIN RAIL PARK FILING NO. 1

PRC2019-00012

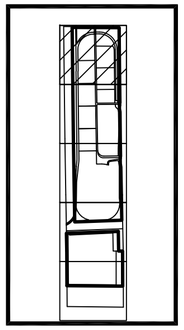
LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

SHEET 3 OF 7

FINAL PLAT



Key Map



PARCEL LINE TABLE		
LINE #	LENGTH	DIRECTION
L200	43.02	S89°39'46"E
L201	11.98	N00°56'38"W
L202	62.28	S89°39'46"E
L203	177.04	S89°40'13"E
L204	175.36	S89°39'46"E
L205	15.00	N00°56'38"W
L206	62.02	N89°39'45"W
L207	45.01	S89°39'45"E

Legend

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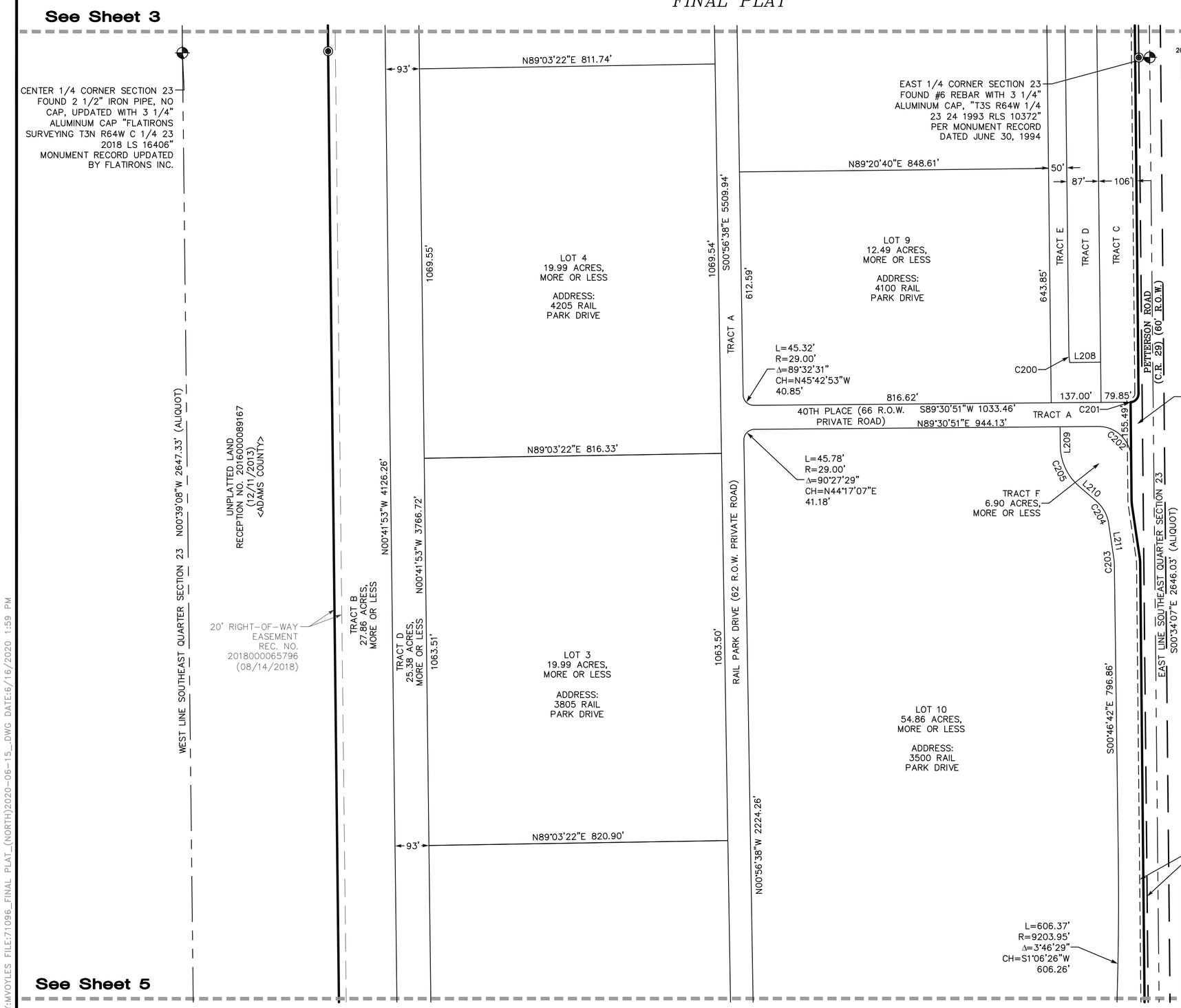
ROCKY MOUNTAIN RAIL PARK FILING NO. 1

PRC2019-00012

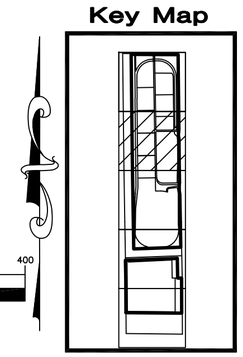
LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

SHEET 4 OF 7

FINAL PLAT



Key Map



GRAPHIC SCALE

200 0 100 200 400

(IN FEET)
1 inch = 200 ft.

Legend

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- FOUND MONUMENT AS DESCRIBED
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- BOUNDARY LINE
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PARCEL LINE TABLE

LINE #	LENGTH	DIRECTION
L208	84.72	S89°23'37"W
L209	52.24	S00°29'08"E
L210	61.86	S49°19'50"E
L211	101.12	S08°23'52"E

CURVE TABLE

CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C200	31.92	223.50	8°11'03"	S04°44'52"E	31.90
C201	1.03	23.00	2°34'30"	S88°13'37"W	1.03
C202	140.61	89.60	89°55'01"	S45°31'38"E	126.62
C203	8.53	66.30	7°22'31"	S04°27'58"E	8.53
C204	72.51	101.50	40°55'58"	S28°51'51"E	70.98
C205	126.60	148.50	48°50'42"	S24°54'29"E	122.80

Interior Lot Configuration

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ROCKY MOUNTAIN RAIL PARK FILING NO. 1

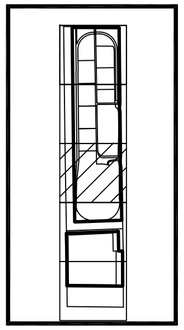
PRC2019-00012

LOCATED IN THE EAST HALF OF SECTION 23, THE EAST HALF OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

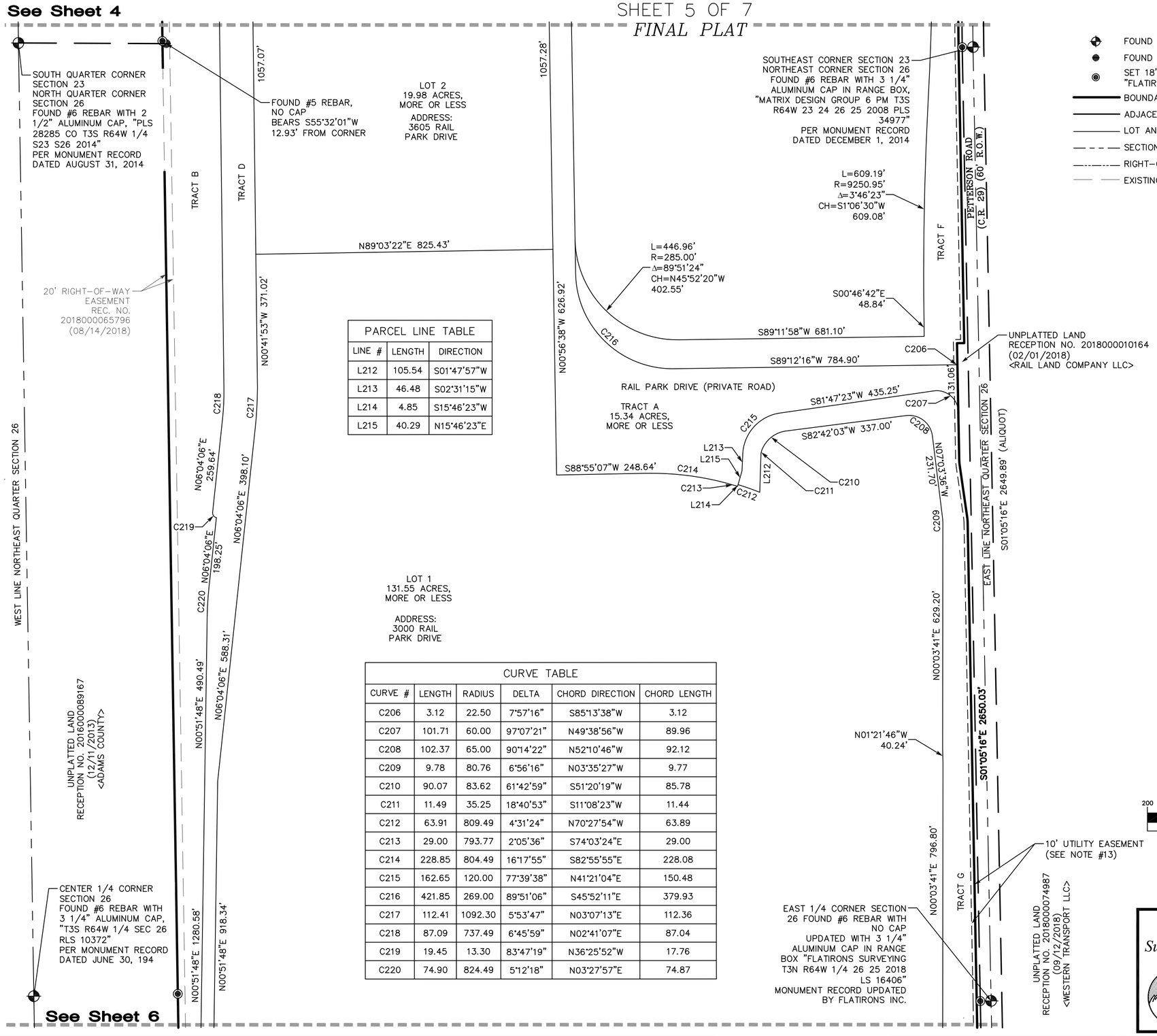
SHEET 5 OF 7

FINAL PLAT

Key Map



See Sheet 4



See Sheet 6

PARCEL LINE TABLE		
LINE #	LENGTH	DIRECTION
L212	105.54	S01°47'57"W
L213	46.48	S02°31'15"W
L214	4.85	S15°46'23"W
L215	40.29	N15°46'23"E

CURVE TABLE					
CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C206	3.12	22.50	7°57'16"	S85°13'38"W	3.12
C207	101.71	60.00	97°07'21"	N49°38'56"W	89.96
C208	102.37	65.00	90°14'22"	N52°10'46"W	92.12
C209	9.78	80.76	6°56'16"	N03°35'27"W	9.77
C210	90.07	83.62	61°42'59"	S51°20'19"W	85.78
C211	11.49	35.25	18°40'53"	S11°08'23"W	11.44
C212	63.91	809.49	4°31'24"	N70°27'54"W	63.89
C213	29.00	793.77	2°05'36"	S74°03'24"E	29.00
C214	228.85	804.49	16°17'55"	S82°55'55"E	228.08
C215	162.65	120.00	77°39'38"	N41°21'04"E	150.48
C216	421.85	269.00	89°51'06"	S45°52'11"E	379.93
C217	112.41	1092.30	5°53'47"	N03°07'13"E	112.36
C218	87.09	737.49	6°45'59"	N02°41'07"E	87.04
C219	19.45	13.30	83°47'19"	N36°25'52"W	17.76
C220	74.90	824.49	5°12'18"	N03°27'57"E	74.87

Legend

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GRAPHIC SCALE

200 0 100 200 400

(IN FEET)
1 inch = 200 ft.

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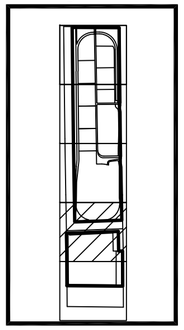
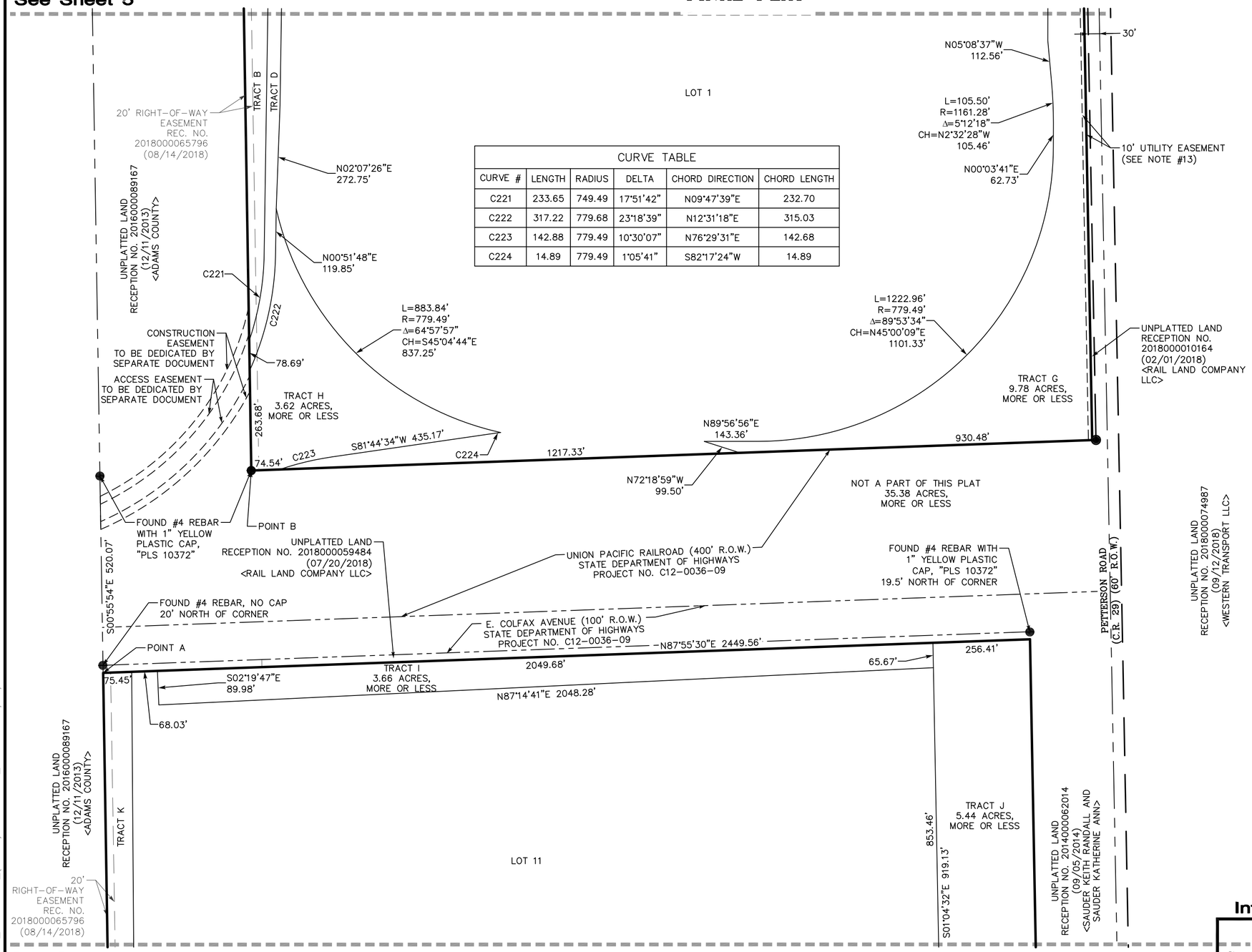
SHEET 6 OF 7

FINAL PLAT

Key Map



See Sheet 5



CURVE TABLE					
CURVE #	LENGTH	RADIUS	DELTA	CHORD DIRECTION	CHORD LENGTH
C221	233.65	749.49	17°51'42"	N09°47'39"E	232.70
C222	317.22	779.68	23°18'39"	N12°31'18"E	315.03
C223	142.88	779.49	10°30'07"	N76°29'31"E	142.68
C224	14.89	779.49	1°05'41"	S82°17'24"W	14.89

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See Sheet 7

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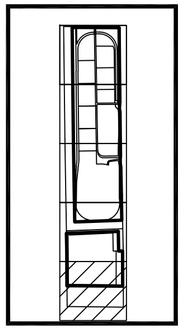
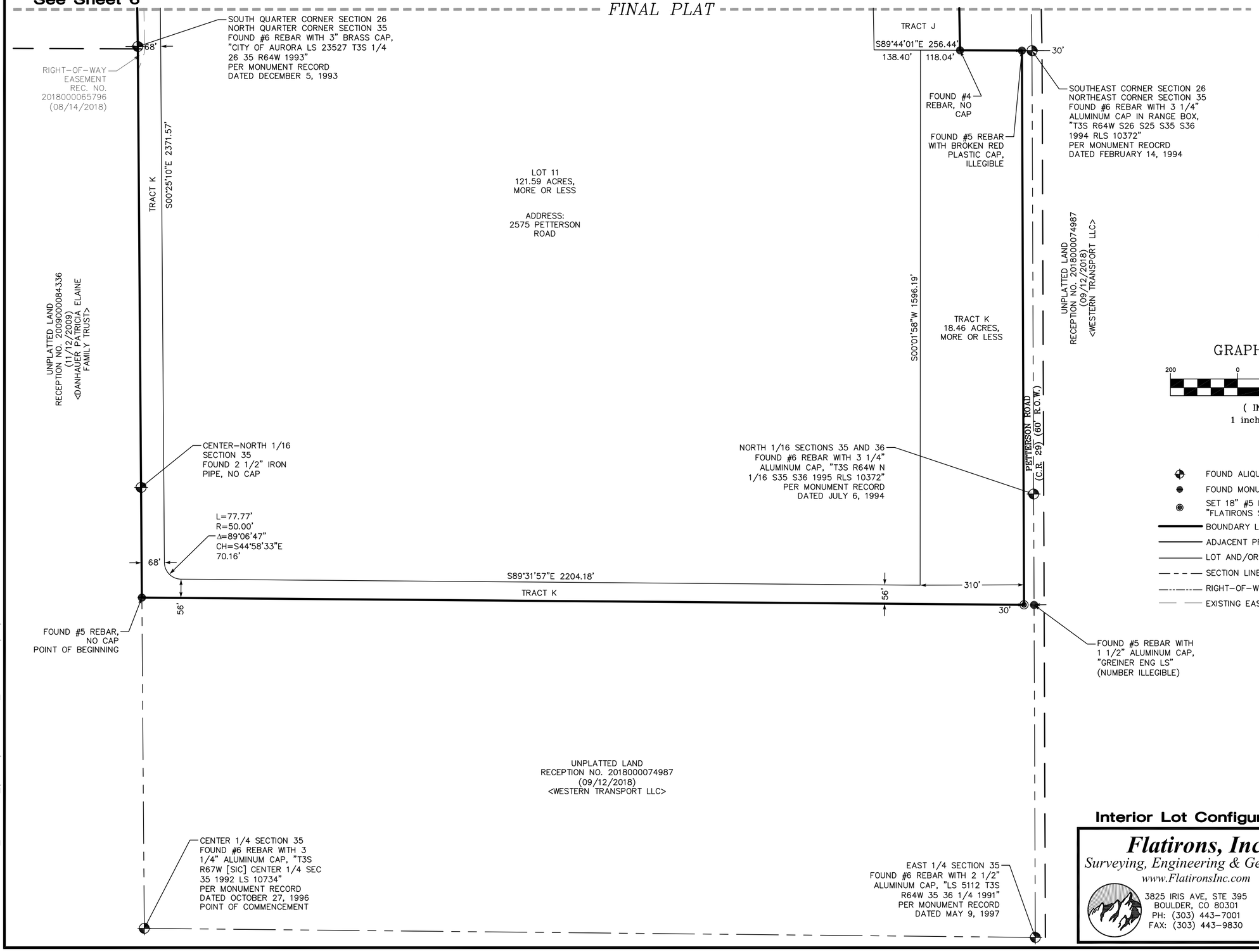
SHEET 7 OF 7

FINAL PLAT

Key Map





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ROADWAY CONSTRUCTION PLANS
PETTERSON ROAD AND EAST 48TH AVENUE

LOCATED IN THE EAST HALF OF SECTIONS 26 AND 23, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN
CITY OF AURORA, COUNTY OF ADAMS, STATE OF COLORADO

CASE #: PRC2018-00006

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

BASIS OF BEARINGS

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 TO BEAR NORTH 89°24'01" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.

GENERAL CONSTRUCTION NOTES

1. CONTRACTOR SHALL ACCEPT TOPOGRAPHY AS SHOWN ON THE PLANS, WITHOUT EXCEPTION, OR SHALL, AT HIS EXPENSE, HAVE NEW MAPPING PREPARED BY A REGISTERED LAND SURVEYOR AND SUBMIT IT TO THE OWNER FOR ACCEPTANCE AND APPROVAL.
2. CONTRACTOR SHALL REFER TO THE GRADING, EROSION AND SEDIMENT CONTROL (GESC) PLAN FOR ADDITIONAL REQUIREMENTS. NO WORK SHALL OCCUR UNTIL THE BMPS DEPICTED ON THE GESC HAVE BEEN INSTALLED AND ARE APPROVED BY THE PRESIDING JURISDICTION . THE CONTRACTOR SHALL ADHERE TO ALL TERMS AND CONDITIONS AS OUTLINED IN THE GENERAL PERMIT FOR STORM WATER DISCHARGE ASSOCIATED WITH CONSTRUCTION ACTIVITIES.
3. EXISTING CONTOURS ARE SHOWN AT 1-FOOT INTERVALS; PROPOSED CONTOURS ARE SHOWN AT 1-FOOT INTERVALS.
4. ALL EARTHWORK, GRADING, OVERLOT GRADING, BACKFILLING, FILLING, EXCAVATION, COMPACTION, PAVEMENT, AND FLATWORK SHALL BE IN CONFORMANCE WITH THE APPROVED GEOTECHNICAL REPORT PREPARED BY FOR THIS PROJECT. THE CONTRACTOR IS REQUIRED TO HAVE A SIGNED AND SEALED COPY OF THE REPORT AT THE SITE AT ALL TIMES. DURING CONSTRUCTION, IF UNANTICIPATED CONDITIONS ARE ENCOUNTERED. THE GEOTECHNICAL ENGINEER SHALL BE CONTACTED BY THE CONTRACTOR FOR RECOMMENDATIONS.
5. THE CONTRACTOR IS RESPONSIBLE FOR:
   - A. VERIFYING ALL UTILITIES, WITHIN AND ADJACENT TO THE AREAS OF WORK, AND NOTIFYING THE APPROPRIATE UTILITY COMPANY PRIOR TO STARTING WORK. ALL UTILITIES SHALL BE PROTECTED FROM DAMAGE. EXISTING AND PROPOSED UTILITY SURFACE APPURTENANCES SHALL BE RAISED TO FINAL GRADE THIS INCLUDES BY IT NOT LIMITED TO MANHOLE LIDS, VALVE COVERS, UTILITY PEDESTALS, ETC.
   - B. OBTAINING ALL NECESSARY PERMITS FOR THE PRESIDING JURISDICTIONS .
   - C. KEEPING ONE COPY OF THE APPROVED CONSTRUCTION PLANS AND SPECIFICATIONS ON THE JOB SITE AT ALL TIMES. PRIOR TO THE START OF CONSTRUCTION, CONTRACTOR TO VERIFY WITH PROJECT ENGINEER THE LATEST REVISION DATE OF THE APPROVED CONSTRUCTION PLANS.
   - D. DOCUMENTING APPROVED DEVIATIONS FROM THE PLANS AND PROVIDING THE ENGINEER OF RECORD WITH AS-BUILT DRAWINGS OF ALL IMPROVEMENTS REQUIRED FOR THE WORK.
   - E. PERFORMING WORK IN ACCORDANCE WITH THE PRESIDING JURISDICTIONS REQUIREMENTS, STANDARDS, AND SPECIFICATIONS, NOTIFICATIONS AND PERMITTING REQUIREMENTS.
   - a. PROVIDING AS-BUILT DRAWINGS OF CONSTRUCTED GRADING, UTILITIES, AND SURFACE IMPROVEMENTS UPON COMPLETION OF THE WORK.
6. THE STORMWATER ADMINISTRATOR IS RESPONSIBLE FOR ENSURING SITE GESC STANDARDS ARE MEET AND TO MAKE NECESSARY FIELD ADJUSTMENTS TO SITE BMPS AND GESC PLAN TO ENSURE THE SITE STABILIZED, TO PREVENT EROSION AND SEDIMENT TRANSPORT.

VICINITY MAP
SCALE 1" = 3000'

ABBREVIATIONS

BW/TW	BOTTOM OF WALL / TOP OF WALL
CBC	CONCRETE BOX CULVERT
DIP	DUCTILE IRON PIPE
ESMT	EASEMENT
EL	ELEVATION
EOA	EDGE OF ASPHALT
EX	EXISTING
FES	FLARED END SECTION
FG	FINISHED GRADE
FL	FLOWLINE
FU	FUTURE
GB	GRADE BREAK
GV	GATE VALVE
HP	HIGH POINT
INV	INVERT
KB/TB	KICK BLOCK / THRUST BLOCK
LP	LOW POINT
ME	MATCH EXISTING
MH	MANHOLE
OHE	OVERHEAD ELECTRIC
PR	PROPOSED
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
REF	REFERENCE
TC	TOP OF CURB
TP	TOP OF PIPE
UE	UTILITY EASEMENT
VC	VERTICAL CURB
W/	WITH

SHEET INDEX

SHEET NO.	SHEET TITLE	DESCRIPTION
1	CVR-1.0	COVER SHEET
2	GNR-1.1	GENERAL NOTES
4	4 OUP - 2	OVERALL UTILITY PLAN - 2
3	3 OUP - 1	OVERALL UTILITY PLAN - 1
5	5 OUP - 3	OVERALL UTILITY PLAN - 3
6	STM-1	STORM PLANS - 1
7	STM-2	STORM PLANS - 2
8	STM-3	STORM PLANS - 3
9	RDW-2.1	PETTERSON RD STA: 0+00 TO 9+20
10	RDW-2.2	PETTERSON RD STA: 9+20 TO 18+40
11	RDW-2.3	PETTERSON RD STA: 18+40 TO 27+60
12	RDW-2.4	PETTERSON RD STA: 27+60 TO 36+80
13	RDW-2.5	PETTERSON RD STA: 36+80 TO 46+00
14	RDW-2.6	PETTERSON RD STA: 46+00 TO 55+20
15	RDW-2.7	PETTERSON RD STA: 55+20 TO 64+40
16	RDW-2.8	PETTERSON RD STA: 64+40 TO 73+60
17	RDW-2.9	PETTERSON RD STA: 73+60 TO 82+80
18	RDW-2.10	PETTERSON ROAD STA: 82+80 TO 91+50
19	RDW-2.11	PETERSON RD STA: 91+50 TO 99+00
20	RDW-2.12	E 48TH AVE STA: -1+50 TO 7+00
21	RDW-2.13	E48TH AVE STA: 7+00 TO 15+50
22	SEC-3.0	PETERSON ROAD STA: 4+00 TO 28+00
23	SEC-3.1	PETERSON ROAD STA: 29+00 TO 49+00
24	SEC-3.2	PETERSON ROAD STA: 50+00 TO 69+00
25	SEC-3.3	PETERSON ROAD STA: 70+00 TO 89+00
26	SEC-3.4	PETERSON ROAD STA: 90+00 TO 96+09
27	SEC-3.5	E. 48TH AVENUE STA: 0+53 TO 11+19
28	SSP-4.0	PETERSON RD STA: 1+00 TO 27+00
29	SSP-4.1	PETERSON RD STA: 27+00 TO 55+50
30	SSP-4.2	PETERSON RD STA: 55+50 TO 84+50
31	SSP-4.3	PETERSON RD STA: 91+00 TO 97+00
32	SSP-4.4	E 48TH AVE STA: 1+00 TO 12+00
33	DTL-5.0	TYPICAL SECTIONS & ROADWAY DETAILS

CONTACT LIST

DEVELOPER
RAIL LAND COMPANY, LLC
4601 DTC BOULEVARD, SUITE 130
DENVER, COLORADO 80237
CONTACT: ROBERT SINGER, PE
303) 257-8865

ADAMS COUNTY
ADAMS COUNTY
4430 S. ADAMS COUNTY PARKWAY
BRIGHTON, CO 80601
CONTACT:
720 523 6869

SURVEYOR
FLATIRONS, INC.
3825 IRIS AVE, SUITE 395
BOULDER, CO 80301
CONTACT: BOB LUND
(303) 443-7001 EXT. 183

ENGINEER
ENERTIA CONSULTING GROUP, LLC
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202
CONTACT: CLIFFORD N. NETUSCHIL, PE
(303) 525-7768

AURORA
AURORA
15151 E. ALAMEDA PARKWAY, 2ND FLOOR
AURORA, COLORADO 80012
CONTACT:
(303) 739-7000

CITY AURORA CONSTRUCTION NOTES

1. CITY OF AURORA PLAN REVIEW IS ONLY FOR GENERAL CONFORMANCE WITH CITY OF AURORA DESIGN CRITERIA AND THE CITY CODE. THE CITY IS NOT RESPONSIBLE FOR THE ACCURACY AND ADEQUACY OF THE DESIGN, DIMENSIONS, AND ELEVATIONS WHICH SHALL BE CONFIRMED AND CORRELATED AT THE JOB SITE. THE CITY OF AURORA, THROUGH THE APPROVAL OF THIS DOCUMENT, ASSUMES NO RESPONSIBILITY FOR THE COMPLETENESS AND/OR ACCURACY OF THIS DOCUMENT.
2. ALL ROADWAY CONSTRUCTION SHALL CONFORM TO CITY OF AURORA "ROADWAY DESIGN & CONSTRUCTION SPECIFICATIONS," LATEST EDITION.
3. ALL WATER DISTRIBUTION, SANITARY SEWER, AND STORM DRAINAGE CONSTRUCTION SHALL CONFORM TO CITY OF AURORA "STANDARDS AND SPECIFICATIONS REGARDING WATER, SANITARY SEWER AND STORM DRAINAGE INFRASTRUCTURE," LATEST REVISION.
4. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE CITY. THE CITY RESERVES THE RIGHT TO ACCEPT OR REJECT ANY MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO THE CITY STANDARDS AND SPECIFICATIONS.
5. THE CONTRACTOR SHALL NOTIFY THE CITY PUBLIC IMPROVEMENT INSPECTIONS DIVISION, 303-739-7420, 24 HOURS PRIOR TO THE BEGINNING OF CONSTRUCTION.
6. LOCATION OF EXISTING UTILITIES SHALL BE VERIFIED BY THE CONTRACTOR PRIOR TO ACTUAL CONSTRUCTION. FOR INFORMATION, CONTACT UTILITY NOTIFICATION CENTER OF COLORADO, 1-800-922-1987 OR 811.
7. THE CONTRACTOR SHALL HAVE ONE SIGNED COPY OF THE PLANS (APPROVED BY THE CITY OF AURORA), ONE COPY OF THE APPROPRIATE STANDARDS AND SPECIFICATIONS AT THE JOB SITE AT ALL TIMES, AND A COPY OF ANY PERMITS AND EXTENSION AGREEMENTS NEEDED AT THE JOB SITE AT ALL TIMES.
8. IT IS THE CONSULTANT'S RESPONSIBILITY TO ACCURATELY SHOW EXISTING CONDITIONS, BOTH ON-SITE, AND OFF-SITE, ON THE CONSTRUCTION PLANS. ANY MODIFICATIONS NEEDED DUE TO CONFLICTS, OMISSIONS, OR CHANGED CONDITIONS EITHER ON-SITE OR OFF-SITE, WHICH ARISE IN THE FIELD, WILL BE ENTIRELY THE DEVELOPER'S RESPONSIBILITY. THE COST TO RECTIFY ANY ADVERSE SITUATION TO MEET THE CITY STANDARDS AND SPECIFICATIONS AND THE CITY CODE SHALL BE BORNE SOLELY BY THE DEVELOPER.
9. THE OWNER/DEVELOPER MUST OBTAIN THE WRITTEN PERMISSION OF THE ADJACENT PROPERTY OWNER(S) PRIOR TO ANY OFF-SITE GRADING OR CONSTRUCTION.
10. CONCRETE SHALL NOT BE PLACED UNTIL THE FORMS HAVE BEEN INSPECTED AND A POUR SLIP ISSUED.
11. PAVING OF PUBLIC STREETS SHALL NOT START UNTIL A SOIL REPORT AND PAVEMENT DESIGN IS APPROVED BY THE CITY ENGINEER, PROOF ROLLING, AND SUBGRADE AND TRENCH COMPACTION TESTS TAKEN BY THE DEVELOPER'S GEOTECH ARE APPROVED BY PUBLIC IMPROVEMENTS INSPECTIONS/MATERIALS LAB.
12. STANDARD CITY OF AURORA CURB RAMPS SHALL BE CONSTRUCTED AT ALL CURB RETURNS, AT ALL "T" INTERSECTIONS AND AT ALL CURBSIDE KIOSKS OR CLUSTERS, UNLESS OTHERWISE MODIFIED BY THESE PLANS.
13. ALL STATIONING IS BASED ON CENTERLINE OF ROADWAYS UNLESS OTHERWISE NOTED.
14. ALL ELEVATIONS ARE FLOW LINE UNLESS OTHERWISE NOTED.
15. THE CITY OF AURORA SHALL NOT BE LIABLE FOR THE MAINTENANCE OF ROCKY MOUNTAIN RAIL PARK INTERNAL IMPROVEMENTS. THESE FACILITIES MAY NOT MEET CITY STANDARDS AND SHALL REMAIN IN PRIVATE MAINTENANCE BY METRODISTRICT IN PERPETUITY. THESE PRIVATE FACILITIES INCLUDE, IF PROVIDED, THE PRIVATE UNDERDRAIN SYSTEM PLACED WITHIN THE PUBLIC RIGHT-OF-WAY.
16. THE C ONTRACTOR/DEVELOPER IS RESPONSIBLE FOR CONTACTING CDOT TO ENSURE ALL WORK ON OR ADJACENT TO STATE HIGHWAYS OR CDOT R.O.W. MEETS CDOT REQUIREMENTS.
17. THE STREETLIGHT INSTALLATION COST IS FUNDED BY THE DEVELOPER/OWNER. COORDINATE THE STREETLIGHT LOCATIONS AND INSTALLATION WITH XCEL ENERGY USING THE APPROVED SITE PLANS/CIVIL PLANS THAT SHOW THE PROPOSED STREET LIGHT LOCATIONS.
18. THE OWNER/CONTRACTOR MUST OBTAIN A C.D.P.S. STORM WATER DISCHARGE PERMIT FROM THE COLORADO DEPARTMENT OF PUBLIC HEALTH AND ENVIRONMENT, IF REQUIRED.
19. THE OWNER/CONTRACTOR IS RESPONSIBLE FOR COORDINATING WITH THE ARMY CORP OF ENGINEERS FOR WETLAND MITIGATION OR WORK WITHIN THE WATERS OF THE U.S., IF REQUIRED. IT IS THE RESPONSIBILITY OF THE OWNER/CONTRACTOR TO PROVIDE A COPY OF THE ARMY CORP OF ENG TO THE CITY OF AURORA. IF THERE ARE NO REQUIREMENTS BY THE ARMY CORP OF ENGINEERS, THEN A WRITTEN NOTIFICATION FROM THE ARMY CORP OF ENGINEERS SHALL BE SUBMITTED TO THE CITY OF AURORA STATING SUCH. CITY APPROVAL OF THE CONSTRUCTION PLANS IS SUBJECT TO THE OWNER/CONTRACTOR OBTAINING A 404 PERMIT, IF APPLICABLE. A COPY OF THIS PERMIT SHALL BE SUBMITTED TO THE CITY OF AURORA PRIOR TO ANY PERMITS BEING ISSUED.
20. ALL SIGNAGE AND STRIPING SHALL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, UNLESS OTHERWISE NOTED BY THE CITY OF AURORA.

FOR BURIED UTILITY INFORMATION
THREE (3) BUSINESS DAYS BEFORE YOU DIG
CALL 811
(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

ENGINEERS APPROVAL

I HEREBY AFFIRM THAT THESE CONSTRUCTION PLANS FOR PETTERSON ROAD AND EAST 48TH AVENUE WERE PREPARED BY ME (OR UNDER MY DIRECT SUPERVISION) IN ACCORDANCE WITH THE REQUIREMENTS OF THE PRESIDING JURISDICTION DEVELOPMENT STANDARDS AND REGULATIONS, FOR ON BEHALF OF ENERTIA CONSULTING GROUP.

CLIFFORD N. NETUSCHIL, P.E.
COLORADO PROFESSIONAL ENGINEER #38138
ENERTIA CONSULTING GROUP, LLC

Approved One Year From This Date

City Engineer — Date

COLORADO REGISTERED PROFESSIONAL ENGINEER CLIFFORD D. NETUSCHIL 38138 PE
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD

DSGN	DR	CHK	APVD

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN METROPOLIS
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

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PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

COVER SHEET

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 — 1"

DATE	5/15/2020
FILE	
DWG	CVR-1.0
SHEET	1 OF 33

CASE #: PRC2018-00006

CONSTRUCTION GENERAL NOTES

1. ANY WORK WITHIN THE CDOT ROW WILL BE COORDINATED AND PERMITTED WITH CDOT.

2. ALL MATERIALS, INSTALLATION, AND CONSTRUCTION OF IMPROVEMENTS SHALL BE IN ACCORDANCE WITH THE PRESIDING AGENCY (CITY, COUNTY, STATE, DISTRICT OR FEDERAL) AND OR APPLICABLE DISTRICT STANDARDS AND SPECIFICATIONS. WHERE THERE IS A CONFLICT BETWEEN THESE PLANS AND ANY APPLICABLE STANDARDS, THE MORE STRINGENT STANDARDS AND OR SPECIFICATION SHALL APPLY. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE IN ACCORDANCE WITH THE PRESIDING AGENCIES STANDARDS AND SPECIFICATIONS. REFERENCES TO STANDARDS SHALL REFER TO THE LATEST REVISION OF SAID STANDARDS, UNLESS SPECIFICALLY STATED OTHERWISE.

3. THE CONTRACTOR IS RESPONSIBLE FOR PROVIDING ALL LABOR AND MATERIALS NECESSARY FOR THE COMPLETION OF THE INTENDED IMPROVEMENTS SHOWN ON THESE PLANS OR DESIGNATED TO BE PROVIDED, INSTALLED, CONSTRUCTED, REMOVED, AND RELOCATED UNLESS SPECIFICALLY NOTED OTHERWISE.

4. THE LOCATION HORIZONTAL AND VERTICAL OF EXISTING UTILITIES, SHOWN ON THESE PLANS, IS BASED ON INFORMATION PROVIDED BY OTHERS. THE INFORMATION IS NOT TO BE RELIED ON AS BEING EXACT OR COMPLETE. THE CONTRACTOR SHALL FIELD VERIFY THE LOCATION OF EXISTING UTILITIES PRIOR TO THE START OF WORK. LOCATION OF PROPOSED CONNECTION POINTS AND UTILITY CROSSING SHALL BE FIELD LOCATED AND VERIFIED WITH THE PROPOSED DESIGN SHOWN ON THESE PLANS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO RELOCATE ALL EXISTING UTILITIES THAT CONFLICT WITH THE PROPOSED IMPROVEMENTS SHOWN ON THESE PLANS AND TO COORDINATE RELOCATIONS WITH THE UTILITY OWNER.

5. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE WITH THE APPROPRIATE UTILITY PROVIDER TO OBTAIN TEMPORARY POWER, TELEPHONE, AND WATER SERVICE DURING CONSTRUCTION. ALL TEMPORTY UTILITY COSTS, POWER WATER SEWER ECT., SHALL BE BORNE BY THE CONTRACTOR.

6. THE CONTRACTOR SHALL COORDINATE WITH THE PRESIDING GOVERNING AGENCY AND ALL UTILITY PROVIDERS AND OR DISTRICTS INVOLVED WITH REGARD TO RELOCATIONS OR ADJUSTMENTS OF EXISTING UTILITIES DURING CONSTRUCTION AND TO ASSURE THAT THE WORK IS ACCOMPLISHED IN A TIMELY FASHION AND WITH A MINIMUM DISRUPTION OF SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING ALL PARTIES AFFECTED BY ANY DISRUPTION OF ANY UTILITY SERVICE.

7. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO COORDINATE ALL UTILITY RELOCATIONS, WHETHER SHOWN OR NOT SHOWN ON THE PLANS.

8. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PROJECT SAFETY INCLUDING, BUT NOT LIMITED TO, EXCAVATION, TRENCHING, SHORING, TRAFFIC CONTROL, MATERIAL HANDLING.

9. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING ALL WORK AND INSPECTIONS AS REQUIRED BY THE PRESIDING GOVERNING AGENCY, STATE, OR LOCAL DISTRICTS. ALL NECESSARY INSPECTIONS AND/OR CERTIFICATIONS REQUIRED SHALL BE PERFORMED PRIOR TO ANNOUNCED BUILDING POSSESSION AND THE FINAL CONNECTION OF SERVICES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COST ASSOCIATED WITH INSPECTION.

10. THE CONTRACTOR SHALL NOTIFY THE PRESIDING GOVERNING AGENCY AND LOCAL UTILITY COMPANIES/ DISTRICTS AT LEAST 48 HOURS PRIOR TO THE START OF ANY CONSTRUCTION.

11. THE CONTRACTOR SHALL BE RESPONSIBLE FOR KEEPING ROADWAYS FREE AND CLEAR OF ALL CONSTRUCTION DEBRIS AND DIRT TRACKED FROM THE SITE.

12. RIM ELEVATIONS SHOWN MAY VARY FROM ACTUAL CONSTRUCTED FINISHED GRADE. UTILITY SURFACE APPURTENANCE (MANHOLE LIDS, VALVE COVERS, UTILITY PEDESTALS, RANGE POINTS ETC.) SHALL BE ADJUSTED TO FINISHED GRADE IN ACCORDANCE WITH THE PRESIDING JURISDICTIONS STANDARDS AND SPECIFICATIONS.

13. IF, DURING THE CONSTRUCTION PROCESS, CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES, WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE PROJECT MANAGER FOR THE OWNER/DEVELOPER AND THE ENGINEER IMMEDIATELY.

14. BENCHMARK VERIFICATION: THE CONTRACTOR SHALL USE BENCHMARKS AND DATUM SHOWN HEREON TO SET PROJECT BENCHMARK(S), BY RUNNING A LEVEL LOOP BETWEEN AT LEAST TWO BENCHMARKS, AND SHALL PROVIDE SURVEY NOTES OF SUCH TO PROJECT ENGINEER PRIOR TO COMMENCING CONSTRUCTION.

15. ALL WORK WITHIN THE PUBLIC RIGHT-OF-WAY SHALL BE DONE IN ACCORDANCE WITH THE PRESIDING AGENCIES STANDARDS AND SPECIFICATIONS.

20. IMPORT MATERIAL SHALL BE SUBJECT TO REVIEW AND ACCEPTANCE BY THE GEOTECHNICAL ENGINEER OF RECORD FOR SITE CONSTRUCTION. THE CONTRACTOR SHALL PROVIDE A LETTER OF CERTIFICATION TO THE OWNER THAT THE MATERIAL IS CLEAN AND FEE OF CONTAMINATES PRIOR TO ANY WORK."

SITE/GENERAL NOTES:

1. CONTOURS SHOWN REPRESENT FINISHED ELEVATIONS. AND DO NOT ACCOUNT FOR MATERIAL DEPTHS.

2. EXISTING SPOT ELEVATIONS AT MATCH POINTS WERE DERIVED FROM CONTOURS PROVIDED WITH THE SITE MAPPING AND ARE ANTICIPATED TO BE +/- AND SHALL BE VERIFIED PRIOR TO ANY CONSTRUCTION. ANY DEVIATION SHALL BE REPORTED TO THE ENGINEER.

3. PRIOR TO PLACING ANY CONCRETE CURB, GUTTER, PANS, AND ACCESSIBLE RAMPS, THE FORMWORK ELEVATIONS SHALL BE VERIFIED AND PROVIDED TO THE PROJECT ENGINEER FOR REVIEW AND APPROVAL. ANY WORK THAT DOES NOT COMPLY WITH THIS REQUIREMENT AND IS PLACED IN ERROR WILL BE THE RESPONSIBILITY OF THE CONTRACTOR.

4. NO PROPOSED LANDSCAPED SLOPE SHALL EXCEED 4:1, OR AS SPECIFIED OTHERWISE BY LOCAL CODE.

5. CONTRACTOR SHALL REFER TO BUILDING ARCHITECTURAL PLANS FOR EXACT DIMENSIONS AND LOCATIONS OF BUILDINGS AND APPURTENANCES. THE EARTHWORK FOR ALL BUILDING FOUNDATIONS AND SLABS SHALL BE IN ACCORDANCE WITH THE ARCHITECTURAL BUILDING PLANS AND SPECIFICATIONS AND THE GEOTECHNICAL REPORT PREPARED SPECIFICALLY FOR THIS SITE.

6. ALL LANDSCAPED AREAS ARE TO BE CONDITIONED PER THE REQUIREMENTS OF THE LANDSCAPE PLANS. ALL DISTURBED AREAS THAT ARE NOT DESIGNATED TO BE PAVED SHALL BE LANDSCAPED OR SEEDED ACCORDING TO THE LANDSCAPE PLAN(S) AND GRADING, EROSION AND SEDIMENT CONTROL PLAN(S).

7. ALL PROPOSED SPOT ELEVATIONS ARE FINISHED GRADE ELEVATIONS UNLESS OTHERWISE NOTED. ELEVATION ALONG THE PROPOSED RAIL LINE REPRESENT FINISHED SUB-GRADE ELEVATIONS. SPOT ELEVATIONS TAKE PRECEDENCE OVER CONTOURS AND SLOPES SHOWN. CONTRACTOR SHALL NOTIFY ENGINEER OF SPOT ELEVATIONS WHICH DO NOT APPEAR TO BE CONSISTENT WITH THE CONTOURS AND SLOPES. SPOT ELEVATIONS AND SPECIFIC PROFILE DATA SHALL BE USED FOR SETTING ELEVATIONS OF CURB AND GUTTER AND UTILITIES. THE CONTRACTOR SHALL ADJUST AND/OR CUT EXISTING PAVEMENT AS NECESSARY TO ASSURE A SMOOTH FIT AND CONTINUOUS GRADE.

8. ALL PAVEMENT SECTIONS AND MATERIALS SHALL BE IN ACCORDANCE WITH THE APPROVED GEOTECHNICAL REPORT AND RECOMMENDATION FOR THE PROJECT. CONTRACTOR SHALL REFERENCE THE APPROVED GEOTECHNICAL REPORT PREPARED FOR THE PROJECT.

9. ASPHALT GRADES SHALL NOT BE LESS THAN 2.00% IN THE DIRECTION OF FLOW; CURB AND GUTTER AND CONCRETE PAN GRADES SHALL NOT BE LESS THAN 0.50% IN THE DIRECTION OF FLOW. THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN ALL PAVEMENT AREAS AND ALONG ALL CURBS. PAVEMENT OR CURBS WHICH DO NOT PROVIDE PROPER DRAINAGE MUST BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

10. HANDICAP PARKING STALLS SHALL BE PAINTED AND SIGNED IN ACCORDANCE WITH CURRENT ADA STANDARDS AND REGULATIONS.

11. HANDICAP RAMPS SHALL BE CONSTRUCTED IN ACCORDANCE WITH CURRENT ADA STANDARDS AND REGULATIONS.

12. THE FOLLOWING IS APPLICABLE TO ALL CONSTRUCTION WITHIN THE LIMITS OF THIS PROJECT:

    a. THE MAXIMUM CROSS SLOPE OF ANY SIDEWALK OR PEDESTRIAN ACCESS ROUTE SHALL BE 2.0%. THE SLOPE SHALL BE MEASURED PERPENDICULAR TO THE DIRECTION OF TRAVEL.

    b. THE GRADE OF HANDICAP PARKING STALLS SHALL NOT EXCEED 2.0% IN ANY DIRECTION. HANDICAP PARKING SHALL HAVE AN ACCESSIBLE ROUTE TO THE BUILDING ENTRY AS SHOWN ON THE DRAWINGS.

    c. HANDICAP ACCESSIBLE ROUTES SHALL HAVE A MAXIMUM LONGITUDINAL GRADE OF 5.0%. ACCESSIBLE ROUTES EXCEEDING 5.0% SHALL BE CONSTRUCTED WITH RAMPS AND HAND RAILS HAVING A MAXIMUM SLOPE OF 8.33% FOR A MAXIMUM RISE OF 30 INCHES. A 5-FOOT BY 5-FOOT LANDING PAD (WITH A 2 PERCENT MAXIMUM SLOPE IN ANY DIRECTION) SHALL BE CONSTRUCTED AT THE BOTTOM AND TOP OF ALL RAMP.

13. PRIOR TO PLACEMENT OF CURB AND PAVEMENT CONTRACTOR SHALL VERIFY COMPLIANCE WITH ADA STANDARDS.

14. IF DURING THE GRADING AND CONSTRUCTION PROCESS CONDITIONS ARE ENCOUNTERED BY THE CONTRACTOR, HIS SUBCONTRACTORS, OR OTHER AFFECTED PARTIES WHICH COULD INDICATE A SITUATION THAT IS NOT IDENTIFIED IN THE PLANS OR SPECIFICATIONS, THE CONTRACTOR SHALL CONTACT THE PROJECT MANAGER FOR THE DEVELOPER/OWNER, THE ENGINEER, AND THE GEOTECHNICAL ENGINEER IMMEDIATELY.

15. ALL GRATES, MANHOLE RIMS, VALVE BOXES, VALVE COVERS, CLEANOUTS, AND VAULT OR BOX COVERS SHALL BE ADJUSTED TO "AS CONSTRUCTED" FINISHED GRADE PRIOR TO THE FINAL LIFT OF ASPHALT.

16. ALL CONCRETE PAVEMENT, CONCRETE FLATWORK, CONCRETE STRUCTURES, AND CONCRETE UTILITIES SHALL BE INSTALLED IN ACCORDANCE WITH THE MATERIAL RECOMMENDATIONS OF THE GEOTECHNICAL REPORT PREPARED SPECIFICALLY FOR THIS SITE.

17. CONCRETE PAVEMENT JOINTS SHALL MEET THE REQUIREMENTS OF CDOT STANDARDS AND SPECIFICATIONS, AND CDOT STANDARD PLAN NO. M-412-1, UNLESS NOTED OTHERWISE.

18. ALL SIGNING AND STRIPING WILL BE IN ACCORDANCE WITH THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (MUTCD) AND LOCAL CODES.

19. CONTRACTOR SHALL COORDINATE SIGNAGE PERMIT, INSTALLATION, AND SPECIFICATIONS WITH OWNER AND SIGNAGE MANUFACTURER.

20. CONTRACTOR SHALL VERIFY SIZE AND LOCATION OF ANY AND ALL ALLOWABLE MONUMENT OR POLE SIGNAGE WITH OWNER AND SIGNAGE MANUFACTURER, AND PROVIDE SHOP DRAWINGS PRIOR TO FABRICATION TO THE OWNER FOR APPROVAL.

21. GRADES WITHIN ASPHALT PAVING AREAS SHALL BE CONSTRUCTED TO WITHIN 0.10 FEET OF THE DESIGN GRADE. HOWEVER, THE CONTRACTOR SHALL MAINTAIN POSITIVE DRAINAGE IN ALL PAVEMENT AREAS AND ALONG CURBS. CURBS OR PAVEMENT AREAS WHICH DO NOT PROVIDE PROPER DRAINAGE MUST BE REMOVED AND REPLACED AT THE CONTRACTOR'S EXPENSE.

22. THE CONTRACTOR SHALL ASSURE POSITIVE DRAINAGE AWAY FROM BUILDINGS FOR ALL LANDSCAPED AND PAVED AREAS.

23. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE RESTORATION OF ANY EXISTING IMPROVEMENTS INCLUDING, BUT NOT LIMITED TO STREET PAVEMENT, FENCES, SOD, LANDSCAPING, SPRINKLER SYSTEMS, AND UTILITIES DISTURBED DURING CONSTRUCTION TO THEIR ORIGINAL LOCATION AND CONDITION.

24. IF ANY EXISTING STRUCTURES, SIDEWALK, AND/OR CURB AND GUTTER MODIFIED OR TO REMAIN ARE DAMAGED DURING CONSTRUCTION, IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO REPAIR AND/OR REPLACE THE EXISTING STRUCTURE AS NECESSARY TO RETURN IT TO EXISTING CONDITIONS OR BETTER PRIOR TO PROJECT CLOSE OUT.

25. THE CONTRACTOR SHALL PROTECT THE PROJECT BENCHMARK(S) THROUGHOUT CONSTRUCTION AND SET ADDITIONAL PROJECT BENCHMARKS AS NECESSARY TO MAINTAIN VERTICAL CONTROL THROUGHOUT THE DURATION OF THE PROJECT.

26. THE CONTRACTOR SHALL INSPECT AND REPAIR EXISTING DRAINAGE STRUCTURES AS NEEDED, AND CLEAN OUT EXISTING PIPES TO REMOVE ALL SILT AND DEBRIS.

RETAINING WALL NOTES

1. ELEVATIONS SHOWN ALONG WALLS REPRESENT THE FINISHED GRADE ELEVATION AT THE TOP AND BOTTOM OF THE WALL. THE ELEVATIONS SHOW ON THESE PLANS DO NOT ACCOUNT FOR THE FINISHED WALL ELEVATIONS, SUCH AS TOP OF WALL AND CAP OR BOTTOM OF WALL AND FOUNDATION.

2. CONTRACTOR SHALL PROVIDE FULL DESIGN AND CONSTRUCTION OF RETAINING WALLS IN ACCORDANCE WITH ALL PRESIDING JURISDICTION, STANDARDS AND SPECIFICATIONS. MODIFICATIONS TO RETAINING WALL ALIGNMENT AND LAYOUT SHALL BE COORDINATED WITH THE PROJECT ENGINEER PRIOR TO WALL INSTALLATION.

3. SUB SURFACE DRAINAGE SYSTEMS FOR RETAINING WALL SHALL BE DESIGNED BY OTHERS AND IS PART OF THE RETAINING WALL DESIGN.

4. THE ALIGNMENT OF THE RETAINING WALLS IS SHOWN ON THE PLANS AND ASSOCIATED PROFILES DEPICTING THE FINISHED GRADE AT THE TOP OF THE WALL (TW) AND BOTTOM OF THE WALL (BW) AND IS NOT INTENDED TO SHOW ANY PORTION OF THE WALL BELOW FINISHED GRADE. CHANGES IN THE TOP OF WALL ELEVATIONS SHALL BE ACHIEVED BY UNIFORMLY STEPPING THE WALL.

5. CONTRACTOR IS REQUIRED TO:

    a. REVIEW THE GEOTECHNICAL REPORT FOR THE SITE AND VERIFY THE REPORT PROVIDES ALL INFORMATION REQUIRED FOR THE DESIGN OF THE WALL.

    b. PROVIDE FULL DESIGN OF THE WALL INCLUDING DESIGN CALCULATIONS AND CONSTRUCTION DRAWINGS PREPARED BY A LICENSED ENGINEER TO BE SUBMITTED TO THE PRESIDING JURISDICTION FOR APPROVAL AND PERMITTING AS REQUIRED. THE DESIGN SHALL CONSIDER DRAINAGE BEHIND THE WALL.

    c. VERIFY EXISTING AND PROPOSED FINISHED GRADE ELEVATIONS AT TOP AND BOTTOM OF THE WALL.

    d. PROVIDE MATERIAL TESTING OF ALL MATERIALS USED FOR CONSTRUCTION OF WALLS.

    e. DETAIL, PROVIDE, AND INSTALL A PEDESTRIAN GUARDRAIL ALONG THE TOP OF ALL WALLS INSTALLED. SUBMIT DETAILS TO THE ARCHITECT FOR APPROVAL PRIOR TO INSTALLATION.

    f. SUBMIT MATERIAL, COLOR, TEXTURE AND MANUFACTURE SAMPLE OF THE BLOCK TO BE USED PRIOR TO INSTALLATION.

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BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

COLORADO REGISTERED
MATTHEW D. NELLENBACH
38375
FOR REVIEW ONLY
5/15/20
PROFESSIONAL ENGINEER

NO.	DATE	REVISION	BY	APVD

DSGN	DR	CHK	APVD

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

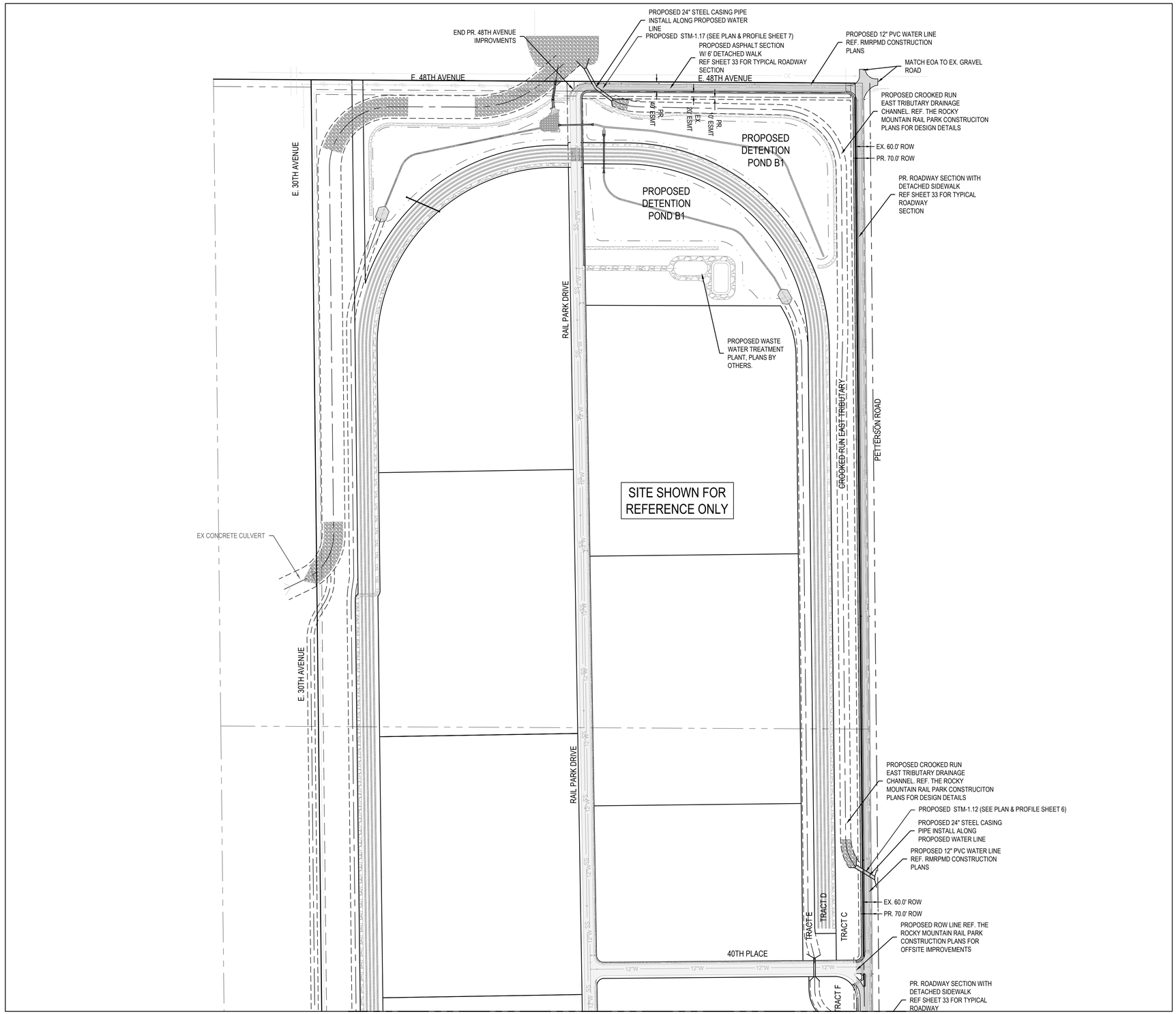
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

GENERAL NOTES

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	GNR-1.1
SHEET	2 OF 33

CASE #: 2020-00000



ABBREVIATIONS

DIP	DUCTILE IRON PIPE
TB/KB	THRUST BLOCK
ESMT	EASEMENT
ELEV	ELEVATION
EX	EXISTING
FG	FINISHED GRADE
FL	FLOW LINE
FU	FUTURE
GB	GRADE BREAK
GV	GATE VALVE
HP	HIGH POINT
INV	INVERT
LP	LOW POINT
ME	MATCH EXISTING
MH	MANHOLE
OHE	OVERHEAD ELECTRIC
PR	PROPOSED
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
REF	REFERENCE
TC	TOP OF CURB
TP	TOP OF PIPE
UE	UTILITY EASEMENT
VC	VERTICAL CURB
W/	WITH

LEGEND

- PROPERTY LINE
- LOT LINE
- PROPOSED EASEMENT
- EXISTING EASEMENT
- EXISTING RIGHT-OF-WAY
- PROPOSED CURB
- PROPOSED FLOW LINE
- PROPOSED PAN/LIP
- PROPOSED CONCRETE
- PROPOSED ASPHALT
- SS — PROPOSED SANITARY SEWER
- ss — EXISTING SANITARY SEWER
- W — PROPOSED WATERLINE
- w — EXISTING WATERLINE
- PROPOSED STORM SEWER
- st — EXISTING STORM SEWER
- UNDERGROUND UTILITY CROSSING

GENERAL SITE NOTES

1. SURVEY MAY NOT REPRESENT ALL EXISTING UTILITIES AND SITE ENCUMBRANCES, CONTRACTOR IS TO FIELD VERIFY AND CONTACT ENGINEER WITH DISCREPANCIES PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES. EXISTING UTILITIES SHOWN ON THESE PLANS ARE BASED ON THE BEST AVAILABLE INFORMATION AT THE TIME OF DESIGN AND DO NOT REFLECT A COMPLETE SURVEY OF EXISTING UTILITIES. CONTACT THE ENGINEER WITH DISCREPANCIES.
3. ALL MATERIALS AND WORKMANSHIP SHALL BE SUBJECT TO INSPECTION BY THE CITY OF AURORA. THE CITY OF AURORA RESERVES THE RIGHT TO ACCEPT OR REJECT ANY SUCH MATERIALS AND WORKMANSHIP THAT DOES NOT CONFORM TO ITS STANDARDS AND SPECIFICATIONS.
4. SAWCUT OF EXISTING STREETS SHALL BE IN CONFORMANCE WITH CITY OF AURORA AND OR CDOT STANDARDS & SPECIFICATIONS (MORE STRINGENT STANDARD SHALL APPLY). CUTS SHALL STRAIGHT WITH CLEAN EDGES. REMOVE EXISTING CURB AND GUTTER TO NEAREST JOINT RETURN ALONG EXISTING LINE AND GRADE.
5. UTILITIES LOCATED OUTSIDE OF PROPOSED ROADWAYS SHALL BE SURFACE MARKED WITH MARKER POST. MARKERS SHALL BE LOCATED AT ALL BENDS IN LINES AND AT SURFACE APPURTENANCES.
6. ALL UTILITY SURFACES APPURTENANCES EXISTING AND PROPOSED SUCH AS MANHOLE LIDS, VALVE BOX COVERS, UTILITY PEDESTALS, SHALL BE RAISED TO FINISHED GRADE PRIOR TO ACCEPTANCE OF DISTRICT UTILITIES.
7. STORM SEWER WITHIN THE PUBLIC ROW SHALL BE CONSTRUCTED IN ACCORDANCE TO THE CITY OF AURORA STANDARDS AND SPECIFICATION.
8. PROPOSED WATER AND SANITARY SEWER IS SHOW FOR REFERENCE ONLY AND ARE UTILITIES TO BE OWNEED AND MAINTAINED BY THE ROCKY MOUNTIAN RAIL PARK METROPOLITIAN DISTRICT. REFERENCE THE WATER AND SANITARY SEWER CONSTRUCTION PLANS FOR WATER AND SANITARY SEWER PREPARED FOR RMRPMD.
9. PROPOSED ROADWAY SECTIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DESIGN AND RECOMMENDATIONS OF THE APPROVED GEOTECHNICAL STUDY FOR THE PROJECT.

N

0' 100 200 400

SCALE: 1"=200'

COLORADO REGISTERED MATTHEW D. NELLGRAN 38176 PROFESSIONAL ENGINEER

FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

OVERALL UTILITY PLAN - 1

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	3 OUP - 1
SHEET	3 OF 33

811
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(OR 1-800-922-1987)
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BENCHMARK

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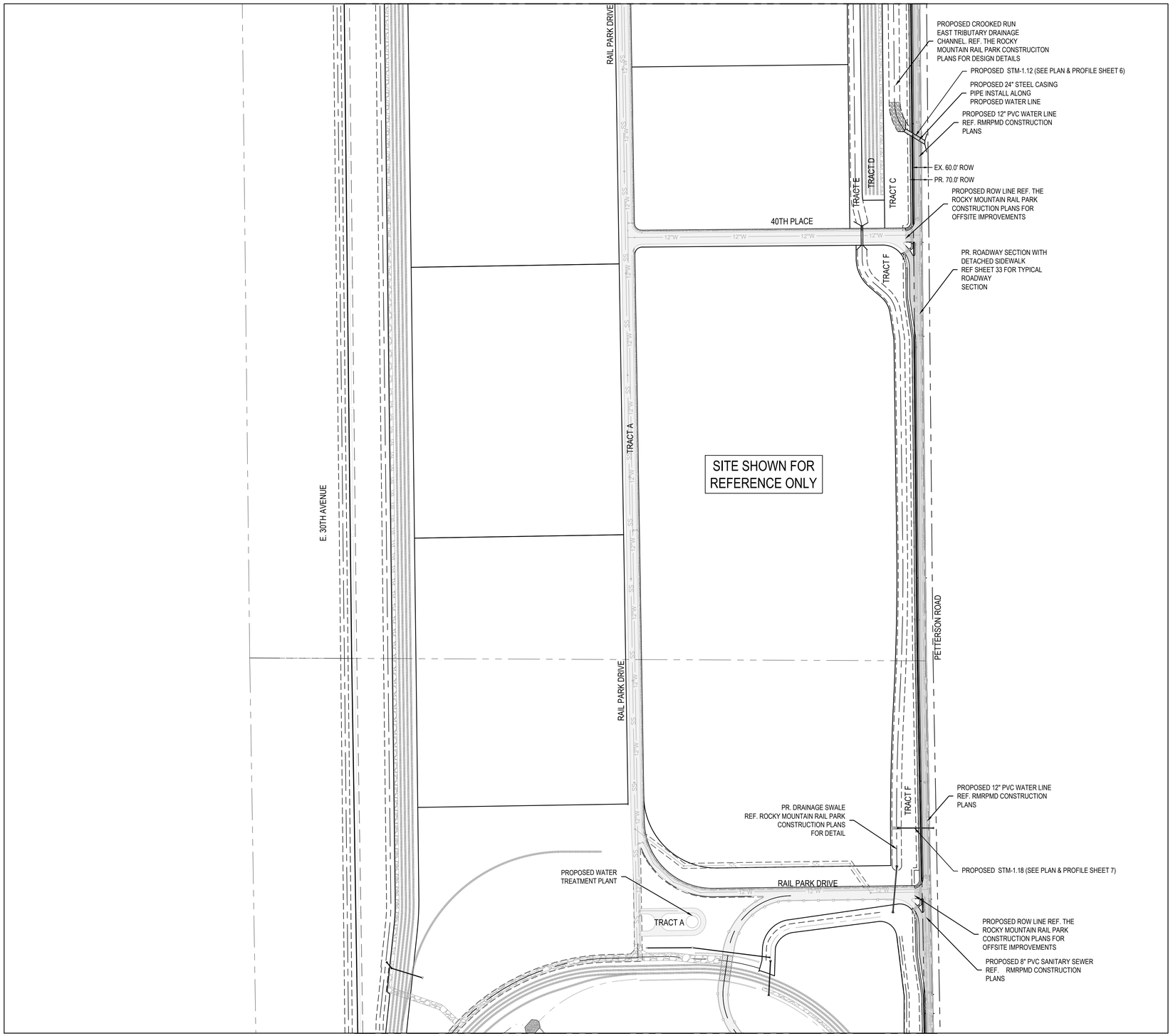
SEE SHEET 4 - OUP - 2

SEE SHEET 3 - OUP - 1



ABBREVIATIONS

DIP	DUCTILE IRON PIPE
TB/KB	THRUST BLOCK
ESMT	EASEMENT
ELEV	ELEVATION
EX	EXISTING
FG	FINISHED GRADE
FL	FLOW LINE
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HP	HIGH POINT
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MH	MANHOLE
OHE	OVERHEAD ELECTRIC
PR	PROPOSED
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
REF	REFERENCE
TC	TOP OF CURB
TP	TOP OF PIPE
UE	UTILITY EASEMENT
VC	VERTICAL CURB
W/	WITH

LEGEND

CASE #: 2020-00000

- PROPERTY LINE
- LOT LINE
- PROPOSED EASEMENT
- EXISTING EASEMENT
- EXISTING RIGHT-OF-WAY
- PROPOSED CURB
- PROPOSED FLOW LINE
- PROPOSED PAN/LIP
- PROPOSED CONCRETE
- PROPOSED ASPHALT
- SS — PROPOSED SANITARY SEWER
- ss — EXISTING SANITARY SEWER
- W — PROPOSED WATERLINE
- w — EXISTING WATERLINE
- PROPOSED STORM SEWER
- st — EXISTING STORM SEWER
- UNDERGROUND UTILITY CROSSING

GENERAL SITE NOTES

1. SURVEY MAY NOT REPRESENT ALL EXISTING UTILITIES AND SITE ENCUMBRANCES, CONTRACTOR IS TO FIELD VERIFY AND CONTACT ENGINEER WITH DISCREPANCIES PRIOR TO CONSTRUCTION.
2. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES. EXISTING UTILITIES SHOWN ON THESE PLANS ARE BASED ON THE BEST AVAILABLE INFORMATION AT THE TIME OF DESIGN AND DO NOT REFLECT A COMPLETE SURVEY OF EXISTING UTILITIES. CONTACT THE ENGINEER WITH DISCREPANCIES.
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5. UTILITIES LOCATED OUTSIDE OF PROPOSED ROADWAYS SHALL BE SURFACE MARKED WITH MARKER POST. MARKERS SHALL BE LOCATED AT ALL BENDS IN LINES AND AT SURFACE APPURTENANCES.
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9. PROPOSED ROADWAY SECTIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE DESIGN AND RECOMMENDATIONS OF THE APPROVED GEOTECHNICAL STUDY FOR THE PROJECT.

N

0' 100 200 400

SCALE: 1"=200'

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SEE SHEET 5 - OUP - 3

COLORADO REGISTERED MATTHEW D. NELLGREN 38176 PROFESSIONAL ENGINEER
FOR REVIEW ONLY
7/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

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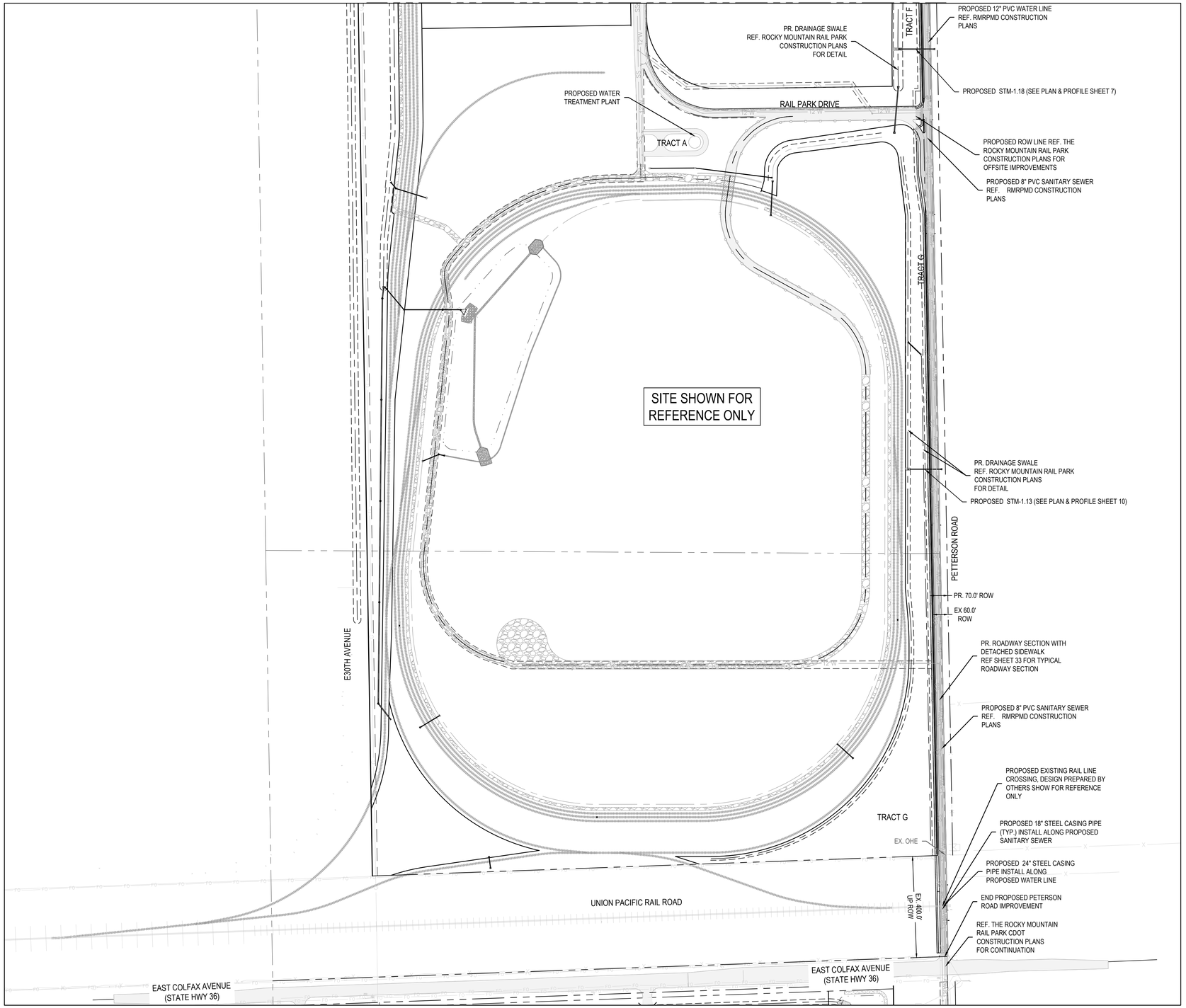
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

OVERALL UTILITY PLAN - 2

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	4 OUP - 2
SHEET	4 OF 33

SEE SHEET 4 - OUP - 2



ABBREVIATIONS

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TB/KB	THRUST BLOCK
ESMT	EASEMENT
ELEV	ELEVATION
EX	EXISTING
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LEGEND

CASE #: 2020-00000

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N

0' 100 200 400

SCALE: 1"=200'

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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLGRAVE
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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ROCKY MOUNTAIN
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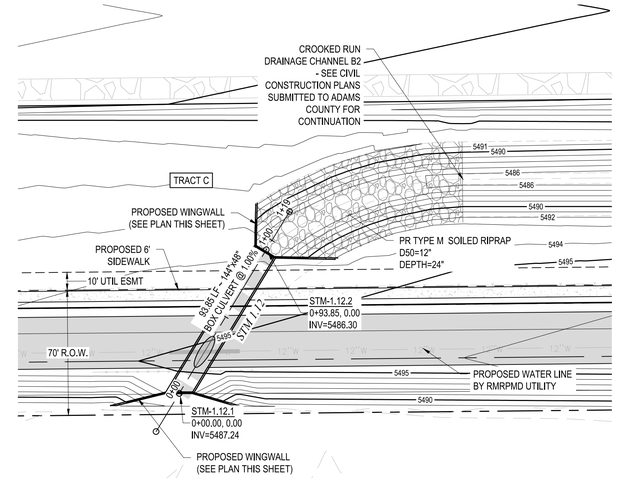
AURORA, COLORADO

OVERALL UTILITY PLAN - 3

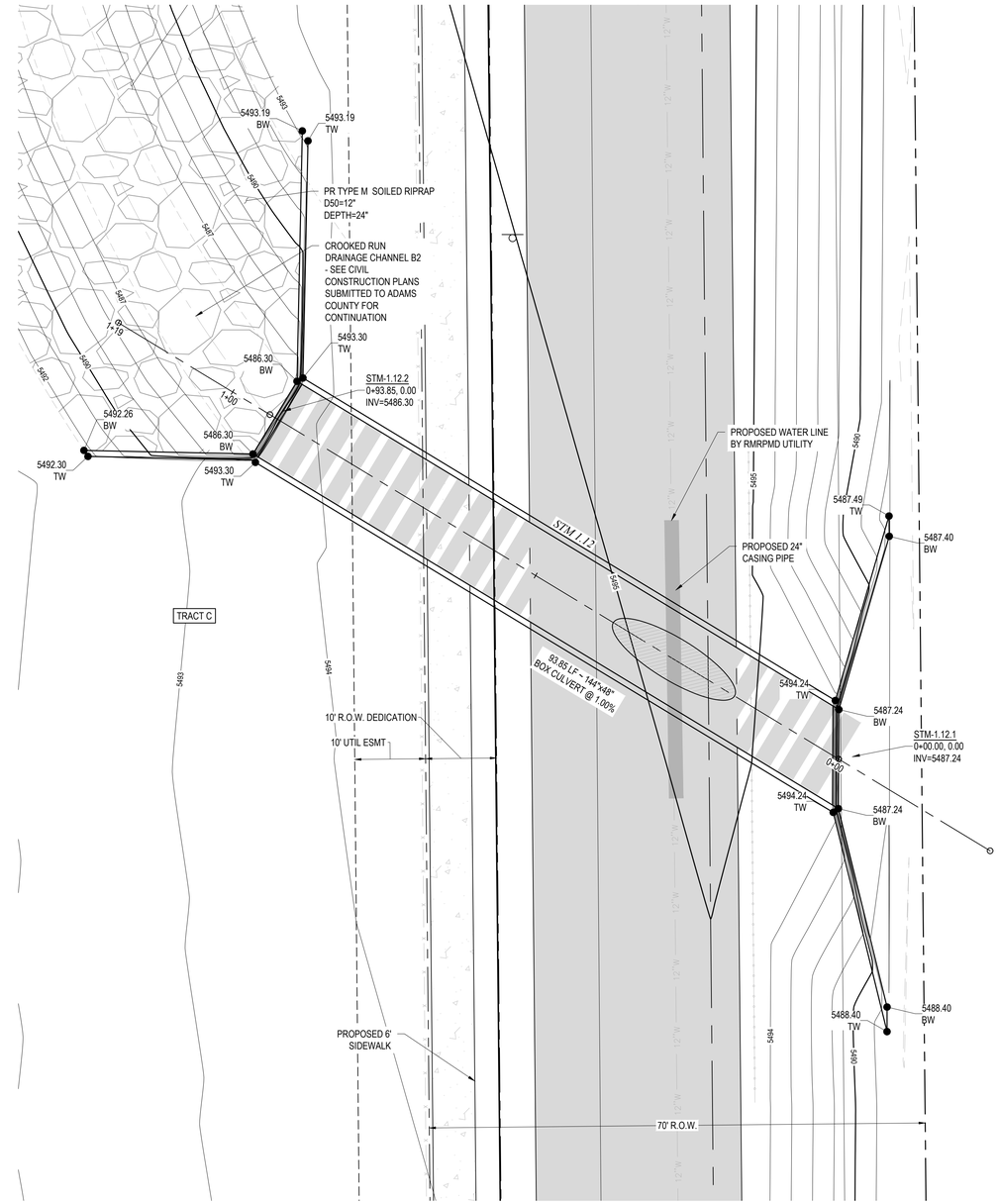
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	5 OUP - 3
SHEET	5 OF 33

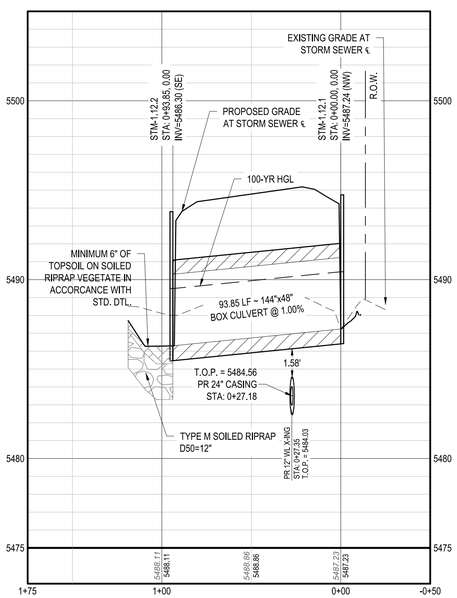
CASE #: PRC2018-00006



PLAN: STM 1.12
HORIZONTAL SCALE: 1" = 40'

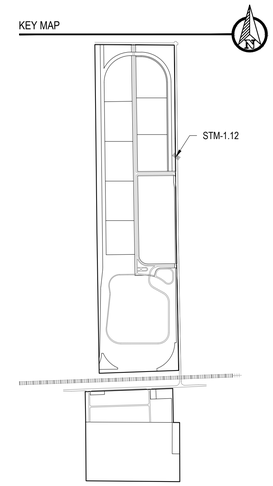


PLAN: STM 1.12 WINGWALL GRADING PLAN
HORIZONTAL SCALE: 1" = 10'



PROFILE: STM 1.12 STA: -0+50 TO 1+75
HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

KEY MAP



ABBREVIATIONS

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CSP	CORRUGATED STEEL PIPE
ESMT	EASEMENT
ELEV	ELEVATION
EX	EXISTING
FES	FLARED END SECTION
FG	FINISHED GRADE
FL	FLOW LINE
FU	FUTURE
GB	GRADE BREAK
HP	HIGH POINT
INV	INVERT
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ME	MATCH EXISTING
MH	MANHOLE
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PR	PROPOSED
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
REF	REFERENCE
ROW	RIGHT OF WAY
TP	TOP OF PIPE
UE	UTILITY EASEMENT
VC	VERTICAL CURB
WALK	SIDEWALK
W/	WITH

0' 20 40 80'
SCALE: 1"=40'

STORM SEWER NOTES

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COLORADO REGISTERED
MATTHEW D. NELLGRAVE
38176
FOR REVIEW ONLY
5/15/20
PROFESSIONAL ENGINEER

NO.	DATE	REVISION	BY	APVD

DSGN	DR	CHK	APVD

ENERTIA CONSULTING GROUP
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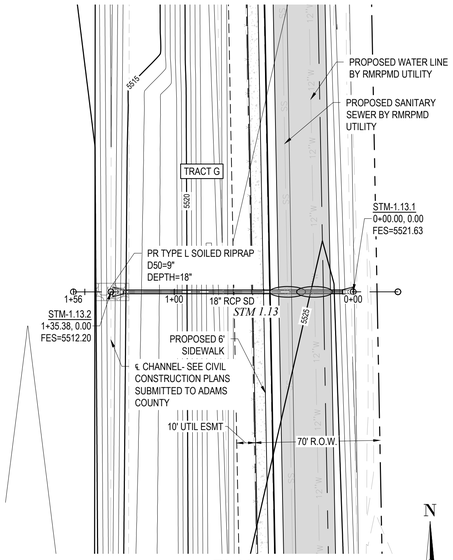
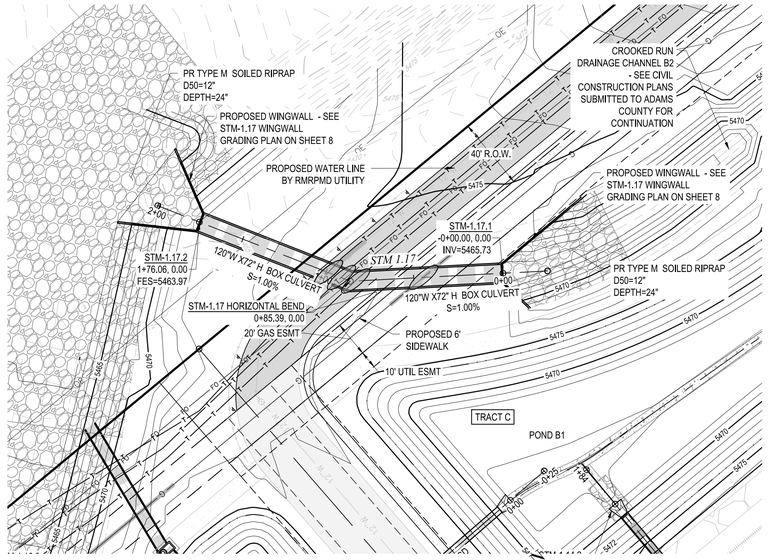
STORM PLANS - 1
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

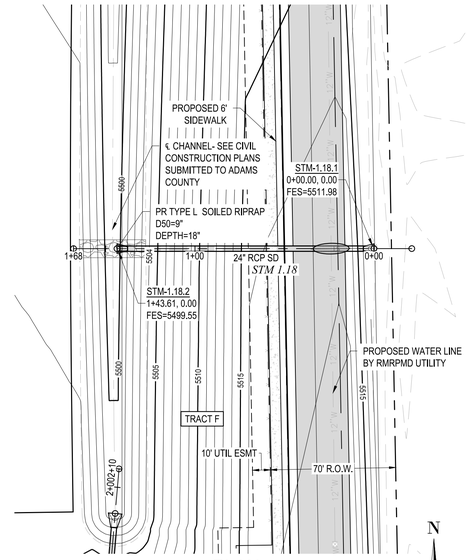
DATE	5/15/2020
FILE	
DWG	STM-1
SHEET	6 OF 33



PLAN: STM 1.13
HORIZONTAL SCALE: 1" = 40'



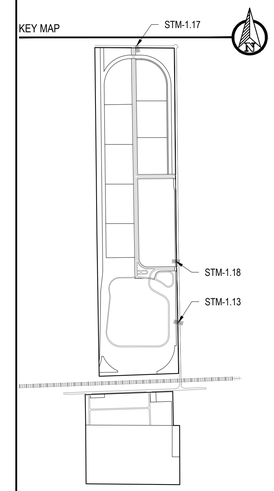
PLAN: STM 1.17
HORIZONTAL SCALE: 1" = 40'



PLAN: STM 1.18
HORIZONTAL SCALE: 1" = 40'

CASE #: PRC2018-00006

KEY MAP



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WALK	SIDEWALK
W/	WITH

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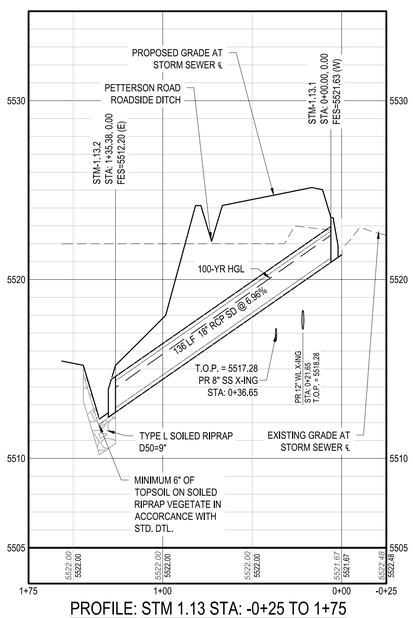
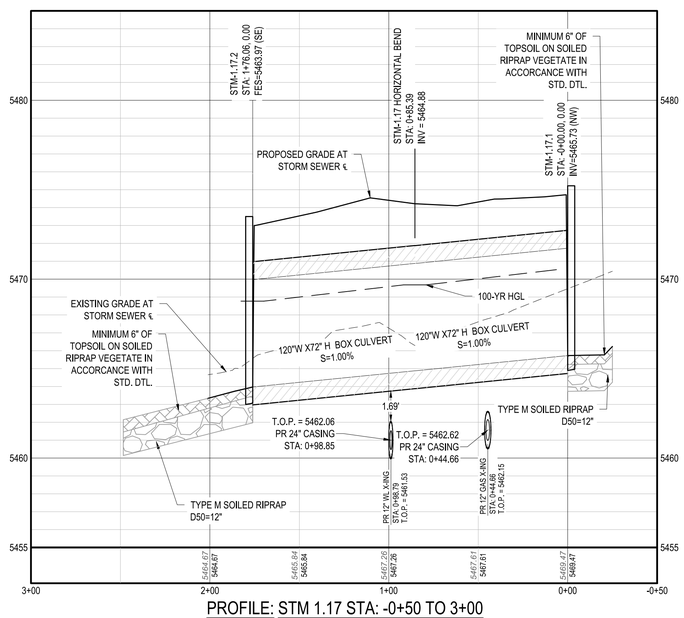


PROFILE: STM 1.13 STA: -0+25 TO 1+75
HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4



PROFILE: STM 1.17 STA: -0+50 TO 3+00
HORIZONTAL SCALE: 1" = 40'
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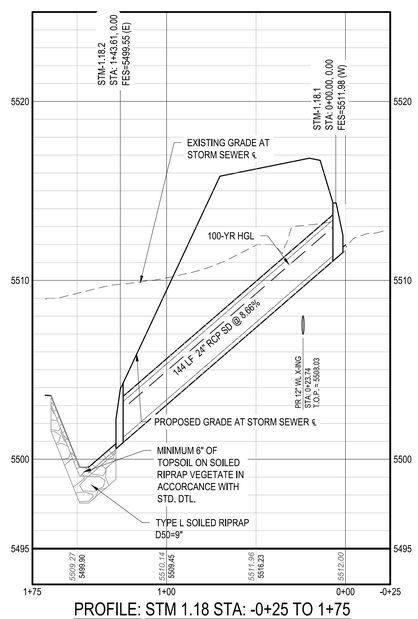

PROFILE: STM 1.18 STA: -0+25 TO 1+75
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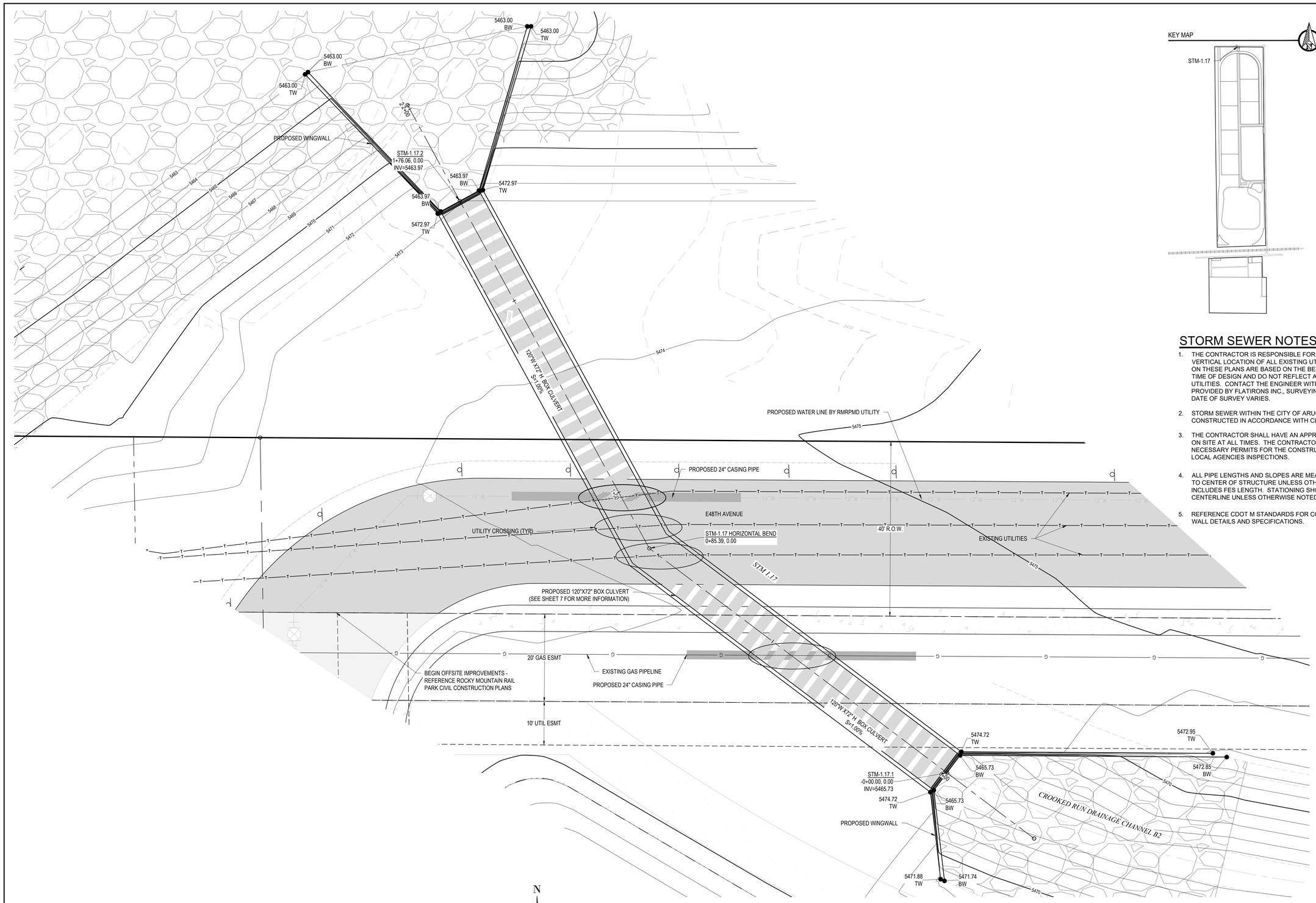
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

STORM PLANS - 2
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	STM-2
SHEET	7 OF 33

CASE #: PRC2018-00006



KEY MAP

ABBREVIATIONS

CMP	CORRUGATED METAL PIPE
CSP	CORRUGATED STEEL PIPE
ESMT	EASEMENT
ELEV	ELEVATION
EX	EXISTING
FES	FLARED END SECTION
FG	FINISHED GRADE
FL	FLOW LINE
FU	FUTURE
GB	GRADE BREAK
HP	HIGH POINT
INV	INVERT
LP	LOW POINT
ME	MATCH EXISTING
MH	MANHOLE
OHE	OVERHEAD ELECTRIC
PR	PROPOSED
PVC	POLYVINYL CHLORIDE PIPE
RCP	REINFORCED CONCRETE PIPE
REF	REFERENCE
ROW	RIGHT OF WAY
TP	TOP OF PIPE
UE	UTILITY EASEMENT
VC	VERTICAL CURB
WALK	SIDEWALK
W/	WITH

0' 20 40 80'

SCALE: 1"=40'

STORM SEWER NOTES

1. THE CONTRACTOR IS RESPONSIBLE FOR LOCATING THE HORIZONTAL AND VERTICAL LOCATION OF ALL EXISTING UTILITIES. EXISTING UTILITIES SHOWN ON THESE PLANS ARE BASED ON THE BEST AVAILABLE INFORMATION AT THE TIME OF DESIGN AND DO NOT REFLECT A COMPLETE SURVEY OF EXISTING UTILITIES. CONTACT THE ENGINEER WITH DISCREPANCIES. SITE SURVEY IS PROVIDED BY FLATIRONS INC., SURVEYING, ENGINEERING AND GEOMATICS. DATE OF SURVEY VARIES.
2. STORM SEWER WITHIN THE CITY OF ARUORA RIGHT-OF-WAY SHALL BE CONSTRUCTED IN ACCORDANCE WITH CITY STANDARDS AND SPECIFICATIONS.
3. THE CONTRACTOR SHALL HAVE AN APPROVED SET OF CONSTRUCTION PLANS ON SITE AT ALL TIMES. THE CONTRACTOR IS RESPONSIBLE FOR OBTAINING ALL NECESSARY PERMITS FOR THE CONSTRUCTION, NOTIFYING AND SCHEDULING LOCAL AGENCIES INSPECTIONS.
4. ALL PIPE LENGTHS AND SLOPES ARE MEASURED FROM CENTER OF STRUCTURE TO CENTER OF STRUCTURE UNLESS OTHERWISE NOTED. PIPE LENGTH INCLUDES FES LENGTH. STATIONING SHOWN ON THE PLANS IS ALONG PIPELINE CENTERLINE UNLESS OTHERWISE NOTED.
5. REFERENCE CDOT M STANDARDS FOR CONCRETE BOX CULVERT AND WING WALL DETAILS AND SPECIFICATIONS.

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CALL 811
(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

PLAN: STM 1.17 WINGWALL GRADING PLAN
HORIZONTAL SCALE: 1" = 10'

COLORADO REGISTERED PROFESSIONAL ENGINEER
GUSTAVO D. NELLO
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN RAIL AUTHORITY
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

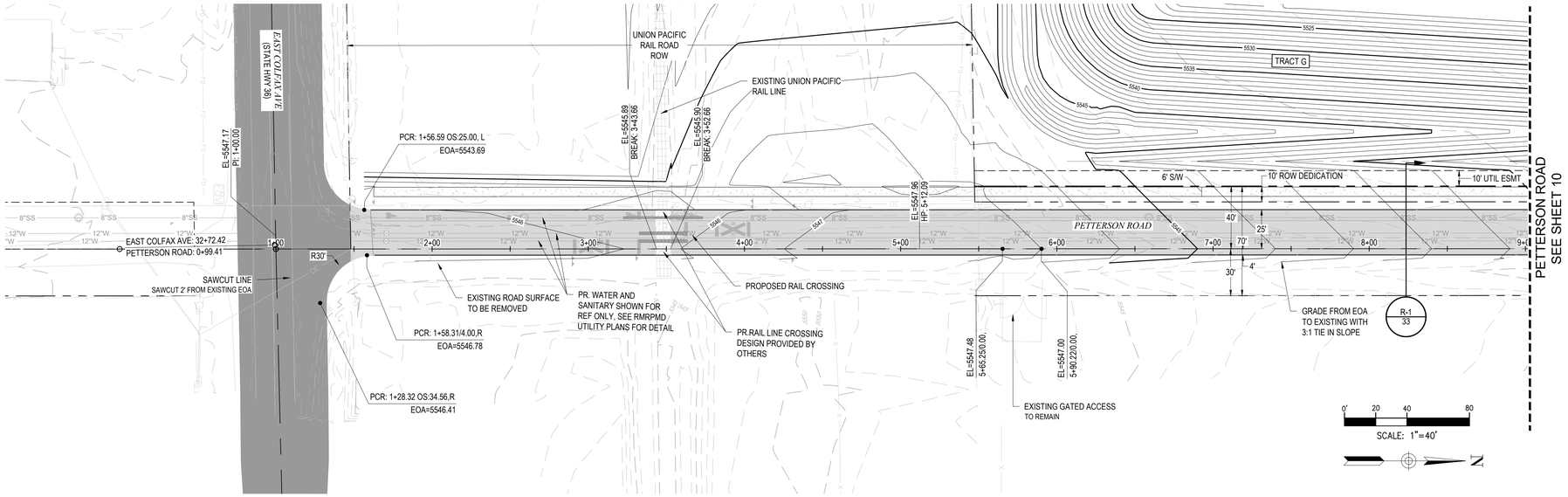
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

STORM PLANS - 3

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

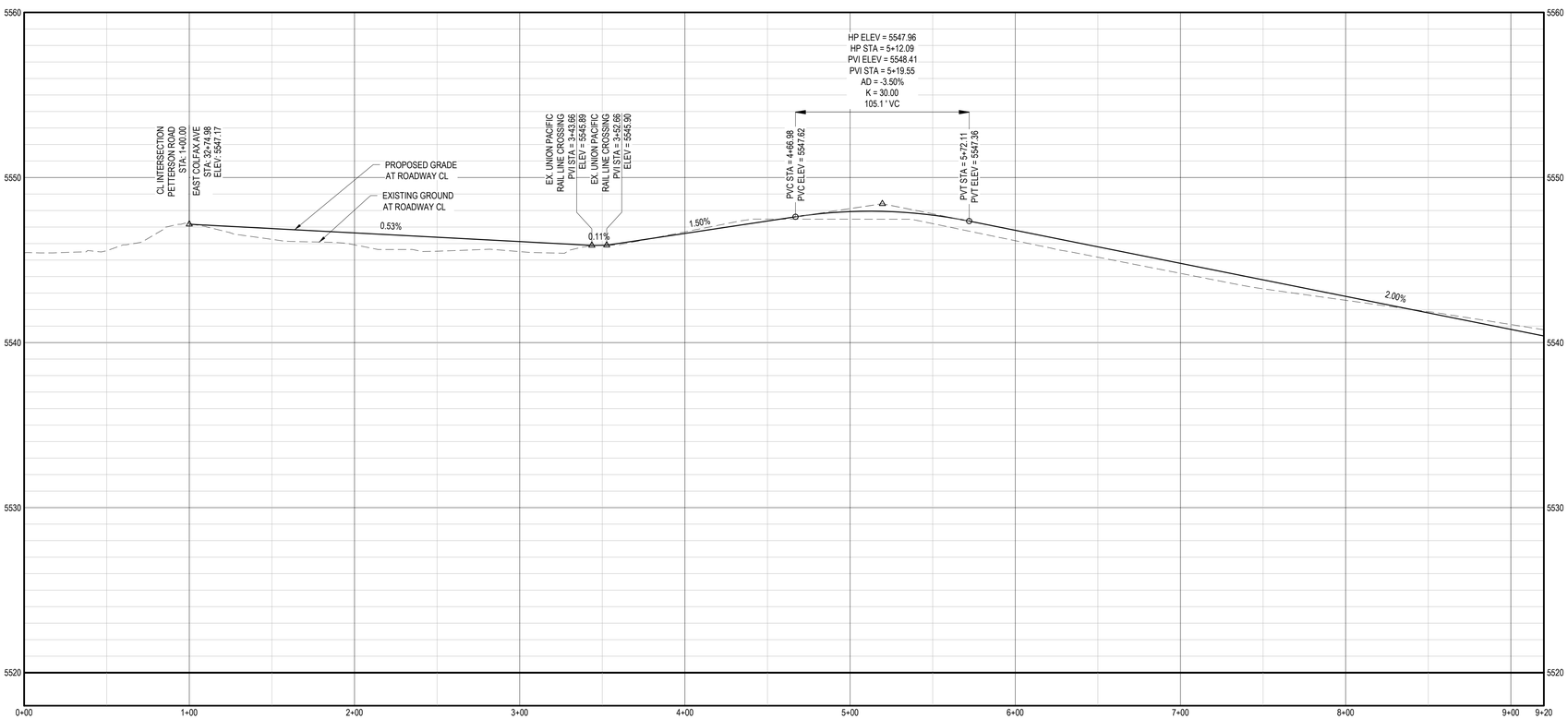
DATE	5/15/2020
FILE	
DWG	STM-3
SHEET	8 OF 33

CASE #: PRC2018-00006



PLAN: PETTERSON RD STA: 0+00 TO 9+20

HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 0+00 TO 9+20

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

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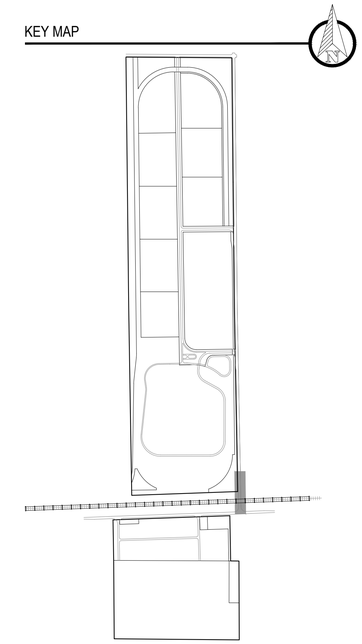

LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

ABBREVIATION LIST

BW/TW	BOTTOM OF WALL / TOP OF WALL	INV	INVERT
CBC	CONCRETE BOX CULVERT	KB/TB	KICK BLOCK / THRUST BLOCK
DIP	DUCTILE IRON PIPE	LP	LOW POINT
ESMT	EASEMENT	ME	MATCH EXISTING
EL	ELEVATION	MH	MANHOLE
EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
FG	FINISHED GRADE	RCP	REINFORCED CONCRETE PIPE
FL	FLOWLINE	REF	REFERENCE
FU	FUTURE	TC	TOP OF CURB
GB	GRADE BREAK	TP	TOP OF PIPE
GV	GATE VALVE	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURB
		W/	WITH

ROADWAY NOTES

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7. EL IS THE FINISHED GRADED ELEVATION ALONG THE CENTER LINE OF PROPOSED ROADWAY AND IS EQUIVALENT TO THE PGL ELEVATION.
8. REFERENCE THE APPROVED GEOTECHINAL STUDY FOR THE PROJECT FOR PAVEMENT DESIGN AND RECOMMENDATIONS.

COLORADO REGISTERED
MATTHEW D. NELLGREN
38175
PROFESSIONAL ENGINEER
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
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ROCKY MOUNTAIN RAIL AUTHORITY
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DENVER, CO 80237

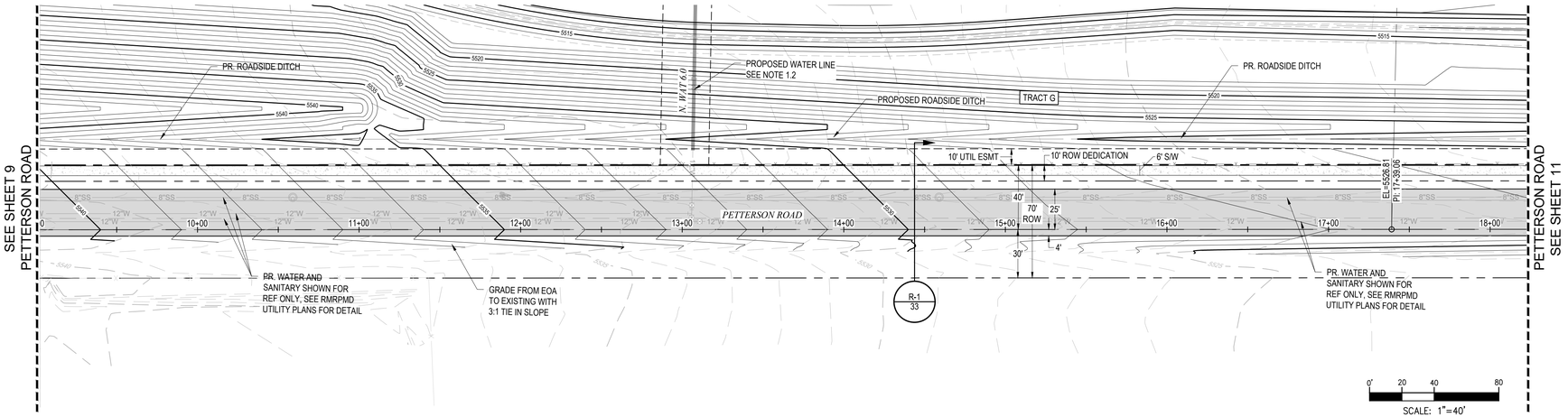
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 0+00 TO 9+20
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

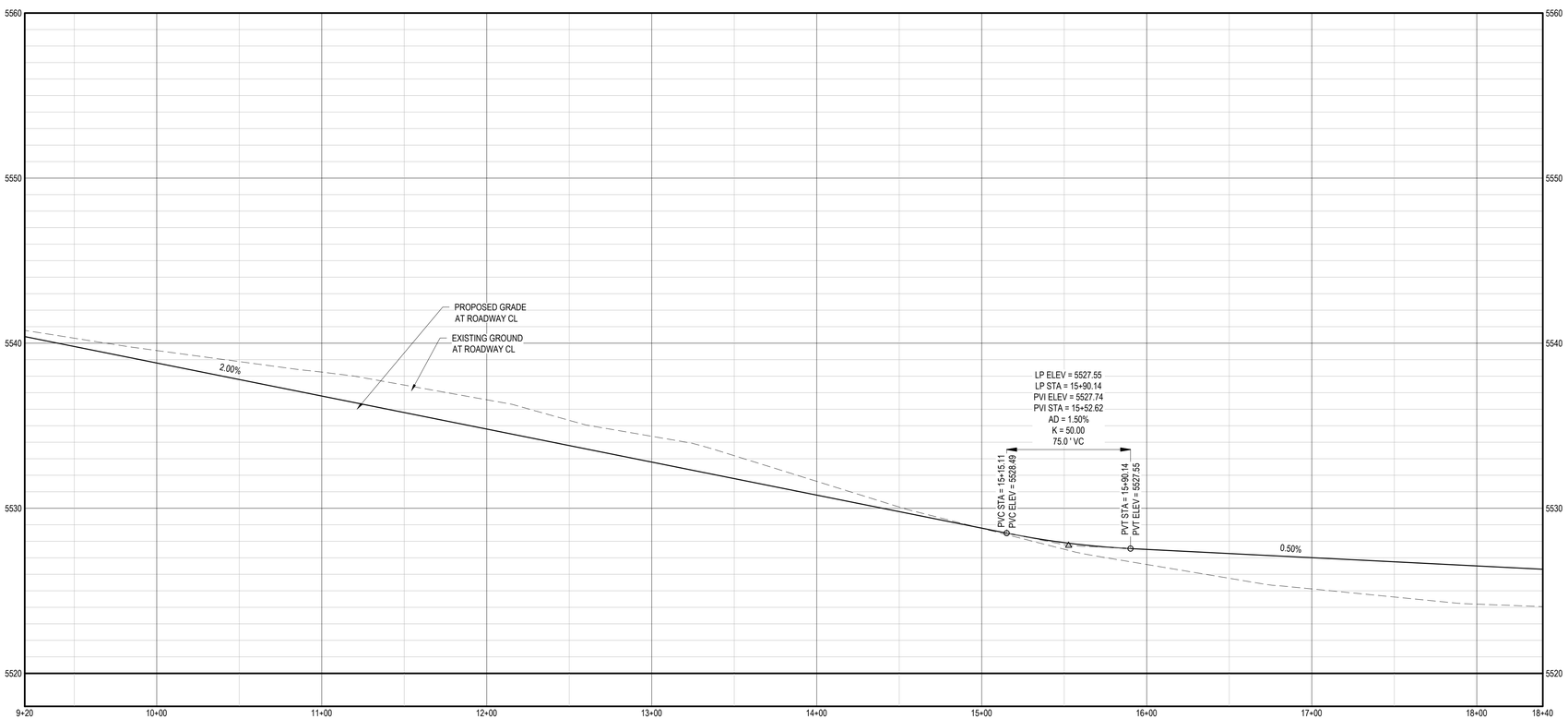
DATE	5/15/2020
FILE	
DWG	RDW-2.1
SHEET	9 OF 33

CASE #: PRC2018-00006



PLAN: PETTERSON RD STA: 9+20 TO 18+40

HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 9+20 TO 18+40

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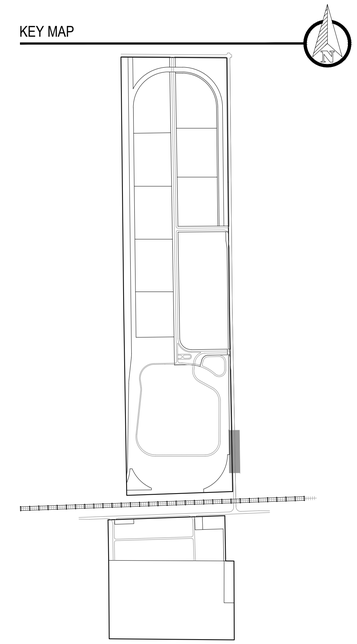

LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
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COLORADO REGISTERED PROFESSIONAL ENGINEER
CHRISTOPHER D. WELLGREEN
38176
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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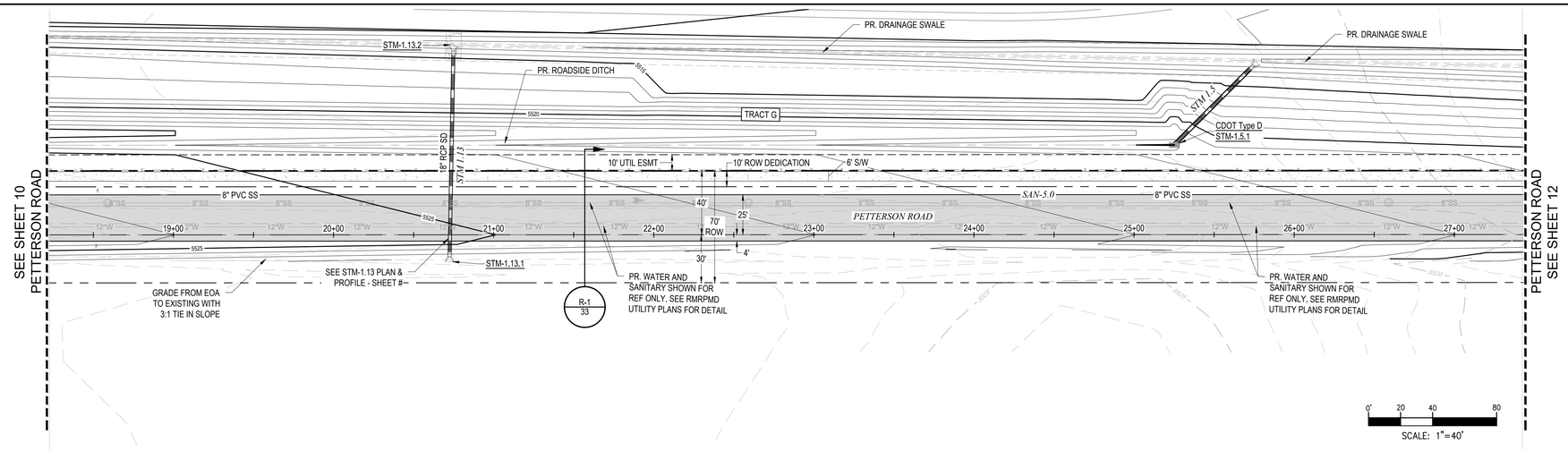
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 9+20 TO 18+40
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

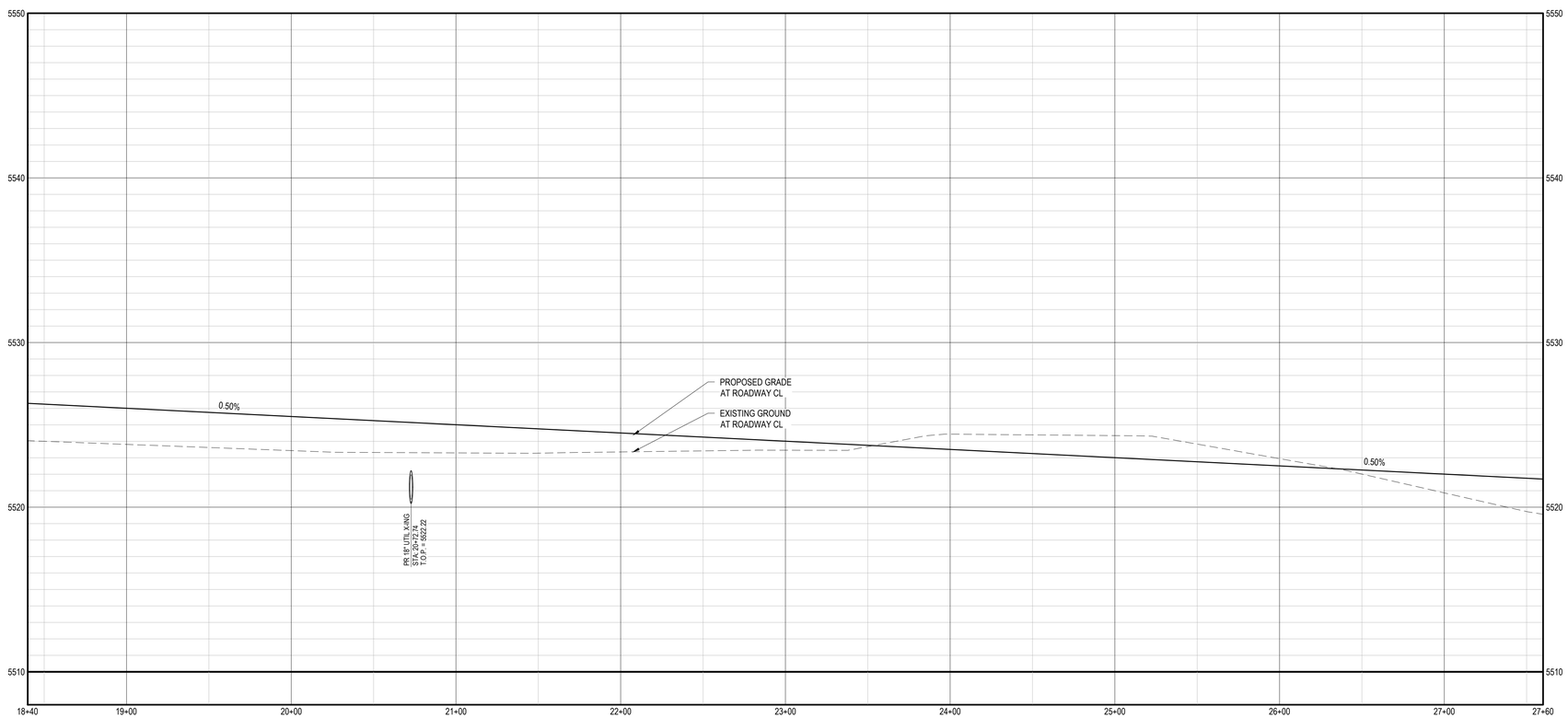
DATE	5/15/2020
FILE	
DWG	RDW-2.2
SHEET	10 OF 33

CASE #: PRC2018-00006



PLAN: PETTERSON RD STA: 18+40 TO 27+60

HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 18+40 TO 27+60

HORIZONTAL SCALE: 1" = 40'
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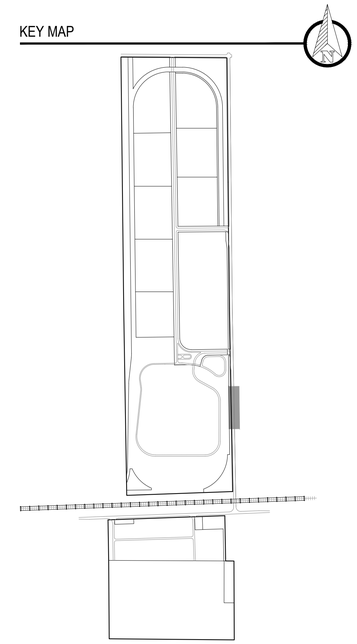

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COLORADO REGISTERED MATTHEW D. NELLGREN 38176 PROFESSIONAL ENGINEER
FOR REVIEW ONLY
5/15/20

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AURORA, COLORADO

PETTERSON RD STA: 18+40 TO 27+60
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VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
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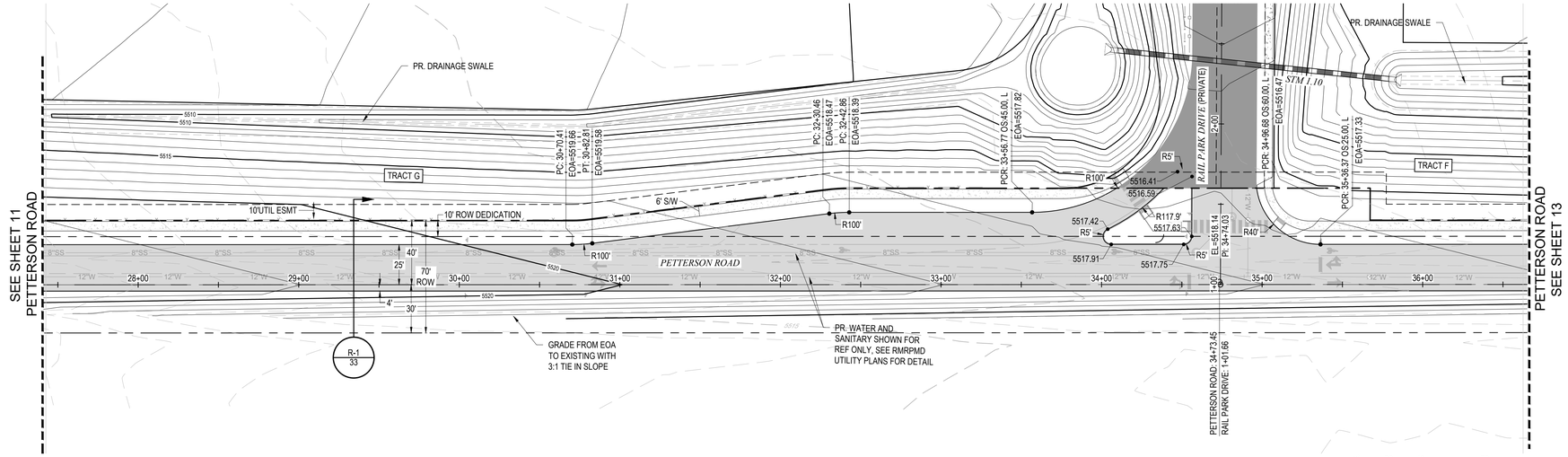
SHEET	11 OF 33

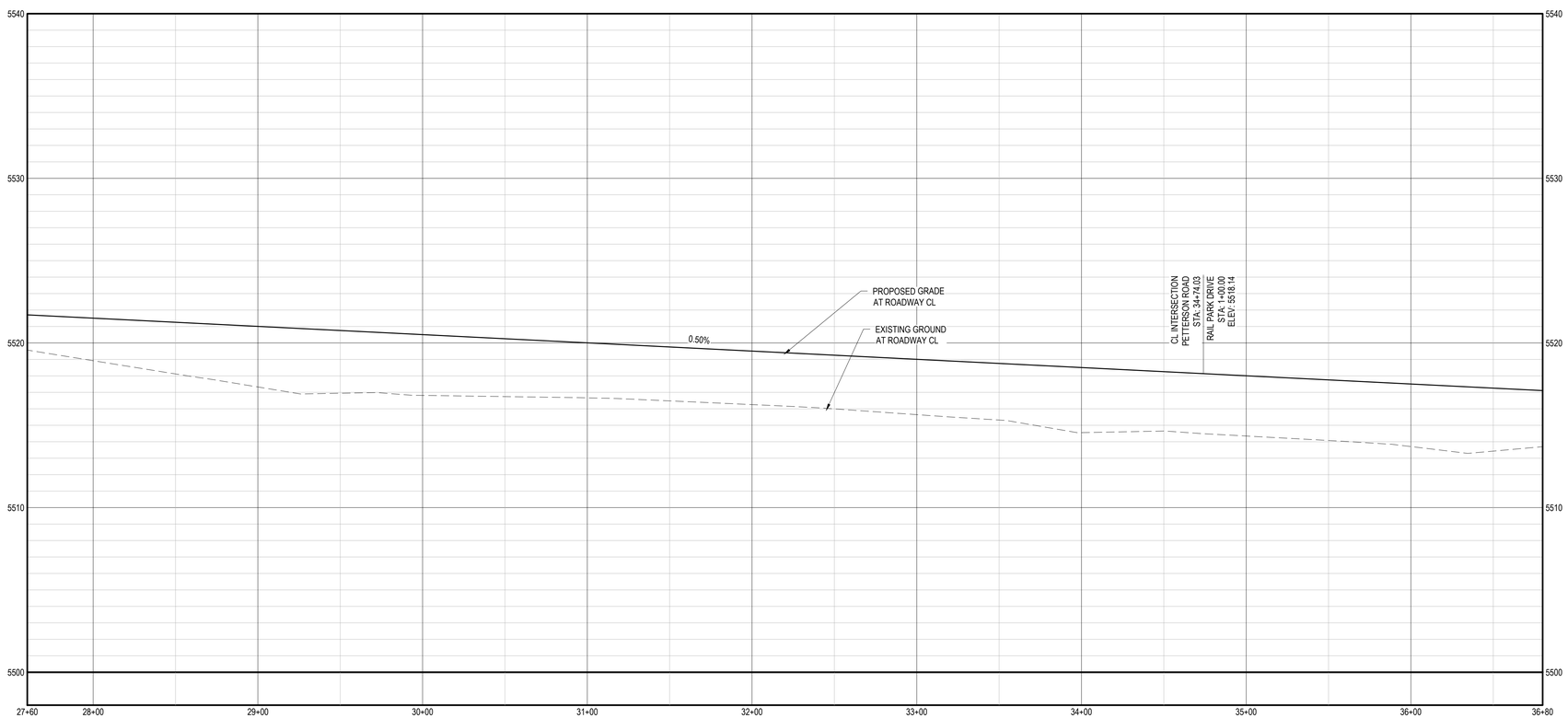
CASE #: PRC2018-00006



SCALE: 1"=40'

PLAN: PETTERSON RD STA: 27+60 TO 36+80

HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 27+60 TO 36+80

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

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(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
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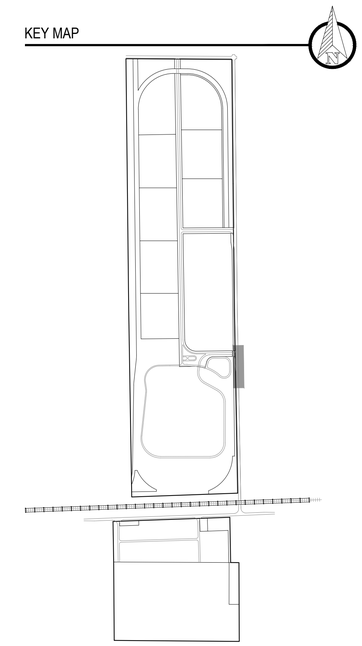
BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

ABBREVIATION LIST

BW/TW	BOTTOM OF WALL / TOP OF WALL	INV	INVERT
CBC	CONCRETE BOX CULVERT	KB/TB	KICK BLOCK / THRUST BLOCK
DIP	DUCTILE IRON PIPE	LP	LOW POINT
ESMT	EASEMENT	ME	MATCH EXISTING
EL	ELEVATION	MH	MANHOLE
EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
FG	FINISHED GRADE	RCP	REINFORCED CONCRETE PIPE
FL	FLOWLINE	REF	REFERENCE
FU	FUTURE	TC	TOP OF CURB
GB	GRADE BREAK	TP	TOP OF PIPE
GV	GATE VALVE	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURB
		W/	WITH

ROADWAY NOTES

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COLORADO REGISTERED PROFESSIONAL ENGINEER
CHRISTOPHER D. WELLEGAN
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

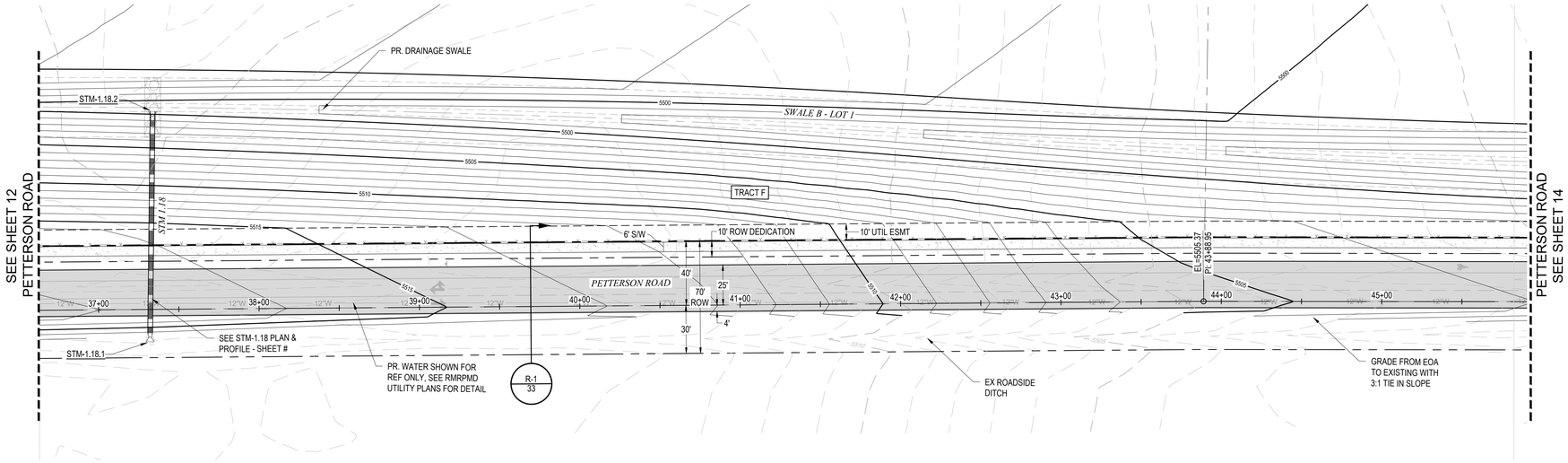
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 27+60 TO 36+80
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.4
SHEET	12 OF 33

CASE #: PRC2018-00006

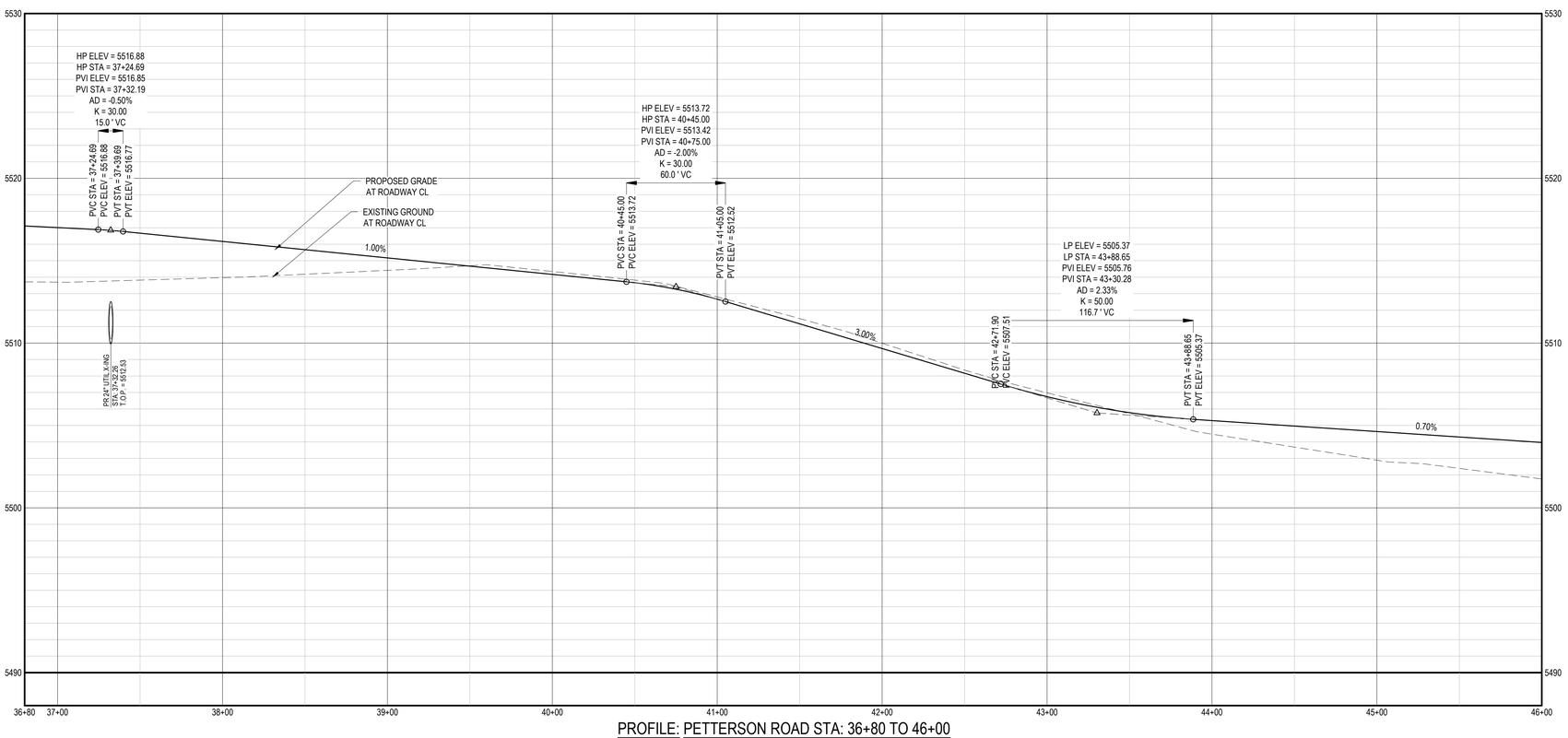


PLAN: PETTERSON RD STA: 36+80 TO 46+00

HORIZONTAL SCALE: 1" = 40'

0' 20 40 80

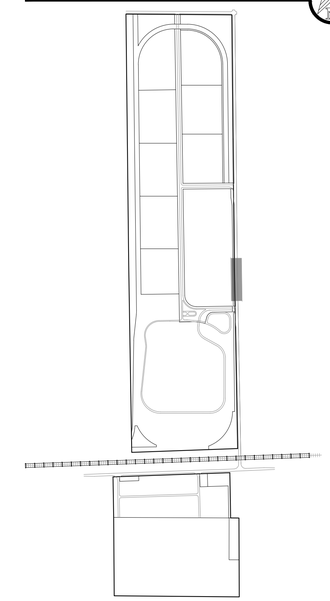
SCALE: 1"=40'



PROFILE: PETTERSON ROAD STA: 36+80 TO 46+00

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLGREN
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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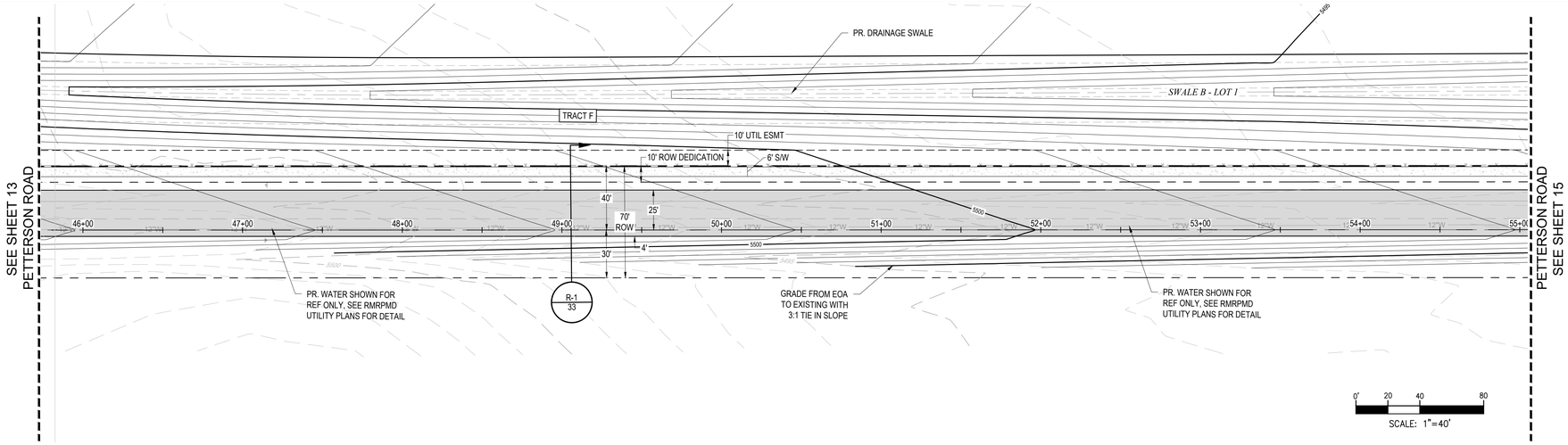
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 36+80 TO 46+00
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.5
SHEET	13 OF 33

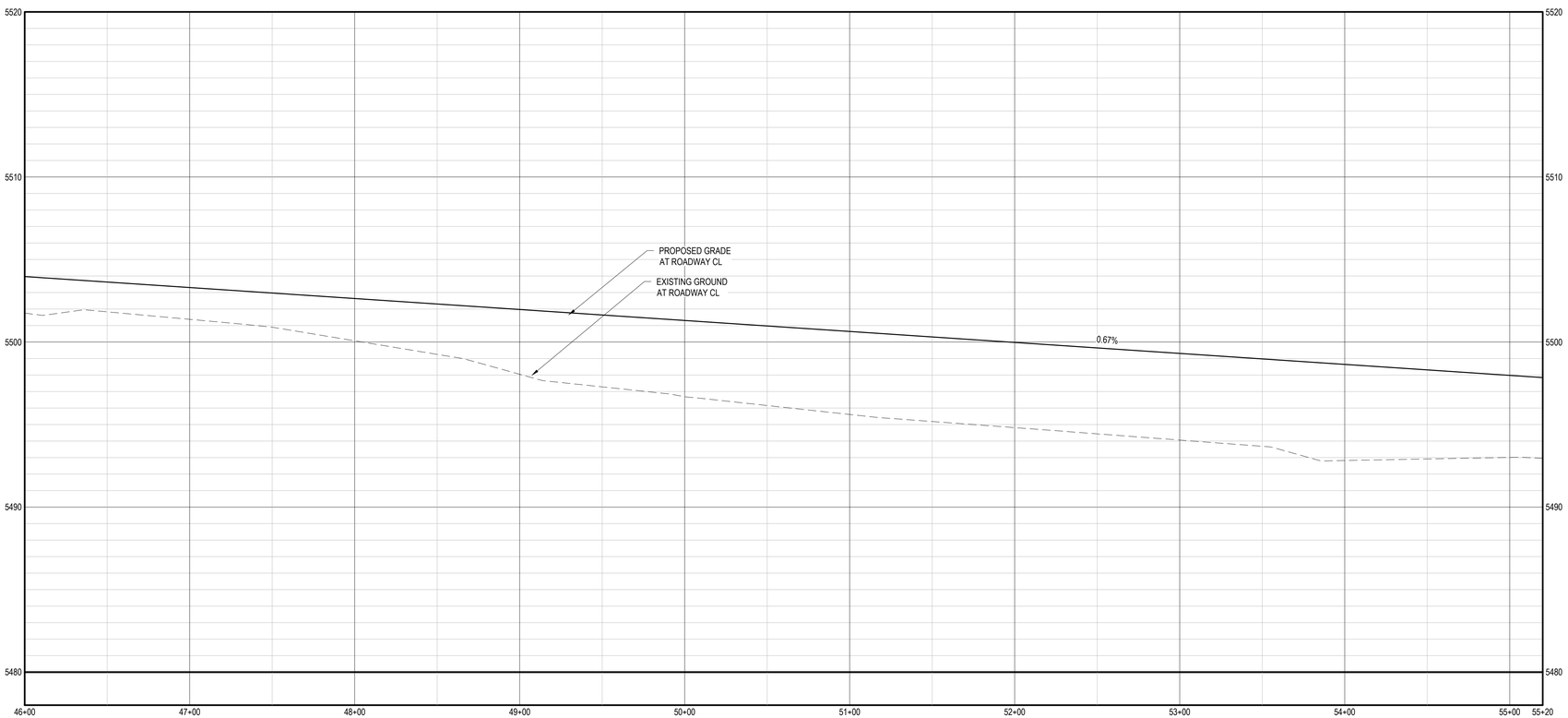
CASE #: PRC2018-00006



0' 20 40 80
SCALE: 1"=40'

PLAN: PETTERSON RD STA: 46+00 TO 55+20

HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 46+00 TO 55+20

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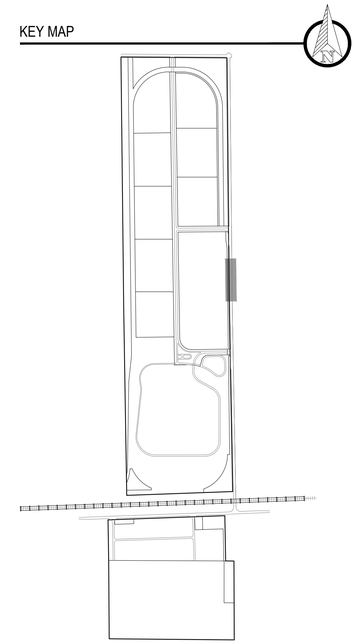

LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
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5500.00	PROPOSED ELEVATION		
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EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
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COLORADO REGISTERED PROFESSIONAL ENGINEER
CHRISTOPHER D. WELLEGAN
38176
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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PETTERSON ROAD AND EAST 48TH AVENUE
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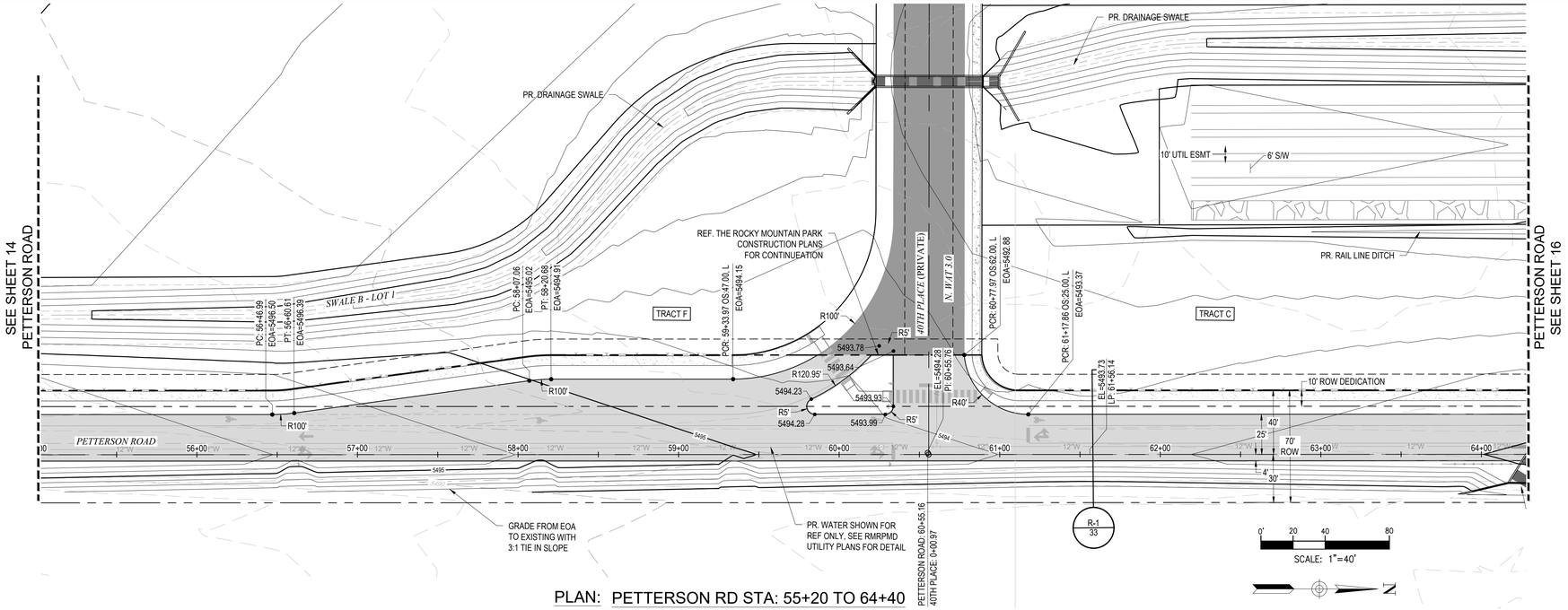
AURORA, COLORADO

PETTERSON RD STA: 46+00 TO 55+20
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

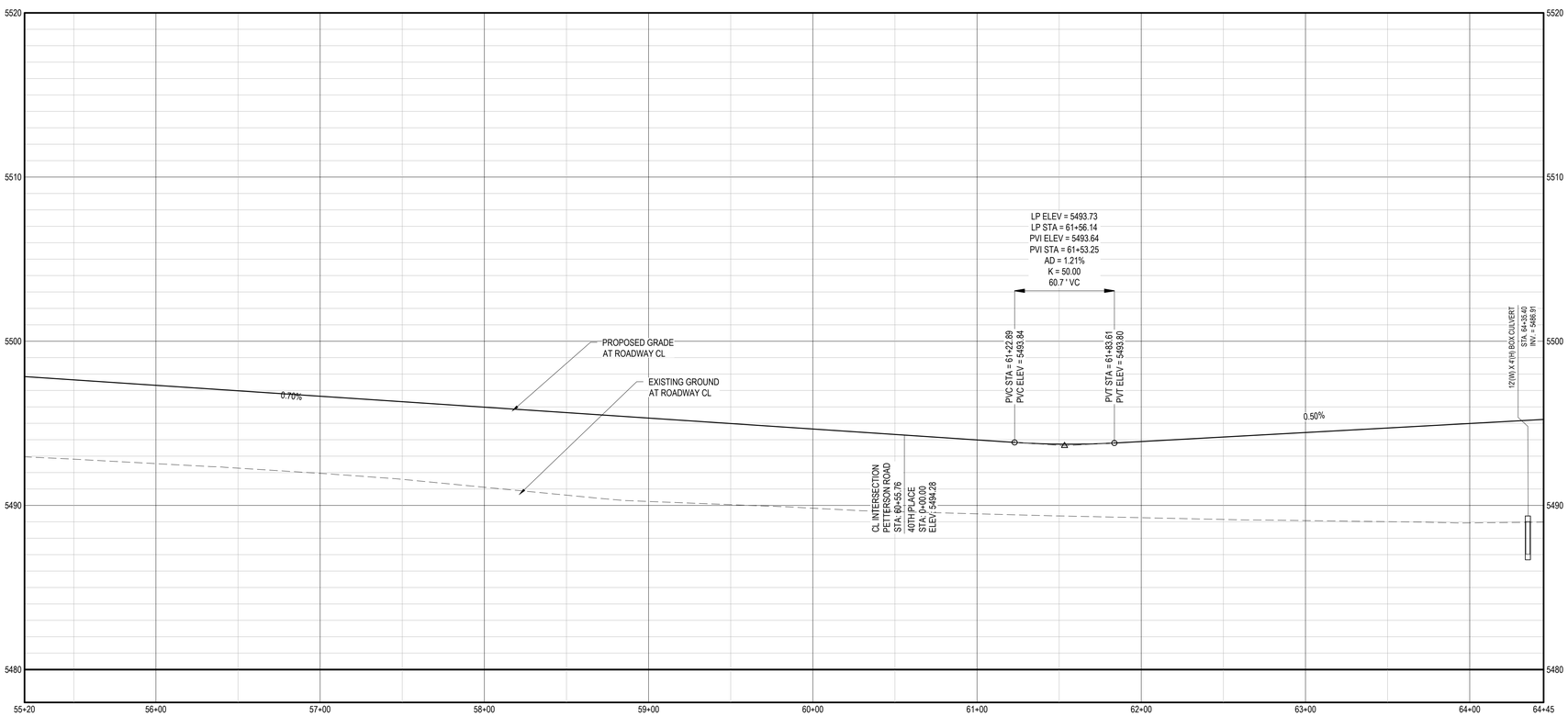
DATE	5/15/2020
FILE	
DWG	RDW-2.6
SHEET	14 OF 33

CASE #: PRC2018-00006



PLAN: PETTERSON RD STA: 55+20 TO 64+40

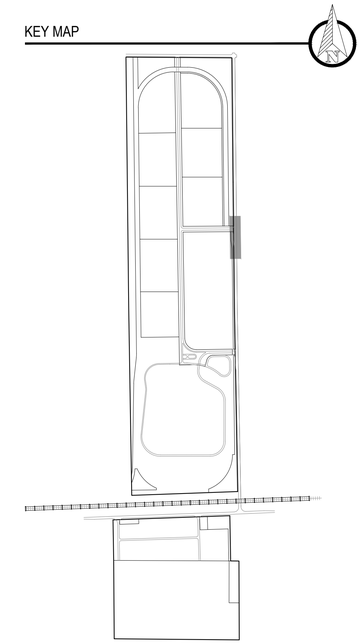
HORIZONTAL SCALE: 1" = 40'



PROFILE: PETTERSON ROAD STA: 55+20 TO 64+45

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

KEY MAP



LEGEND

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ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

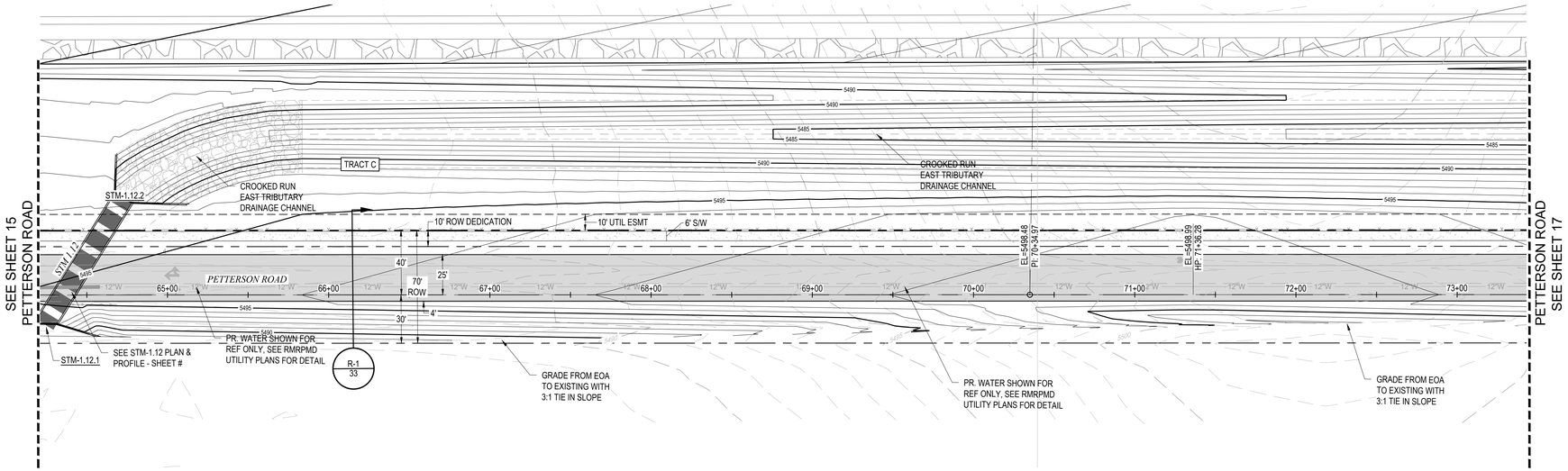
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 55+20 TO 64+40
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.7
SHEET	15 OF 33

CASE #: PRC2018-00006

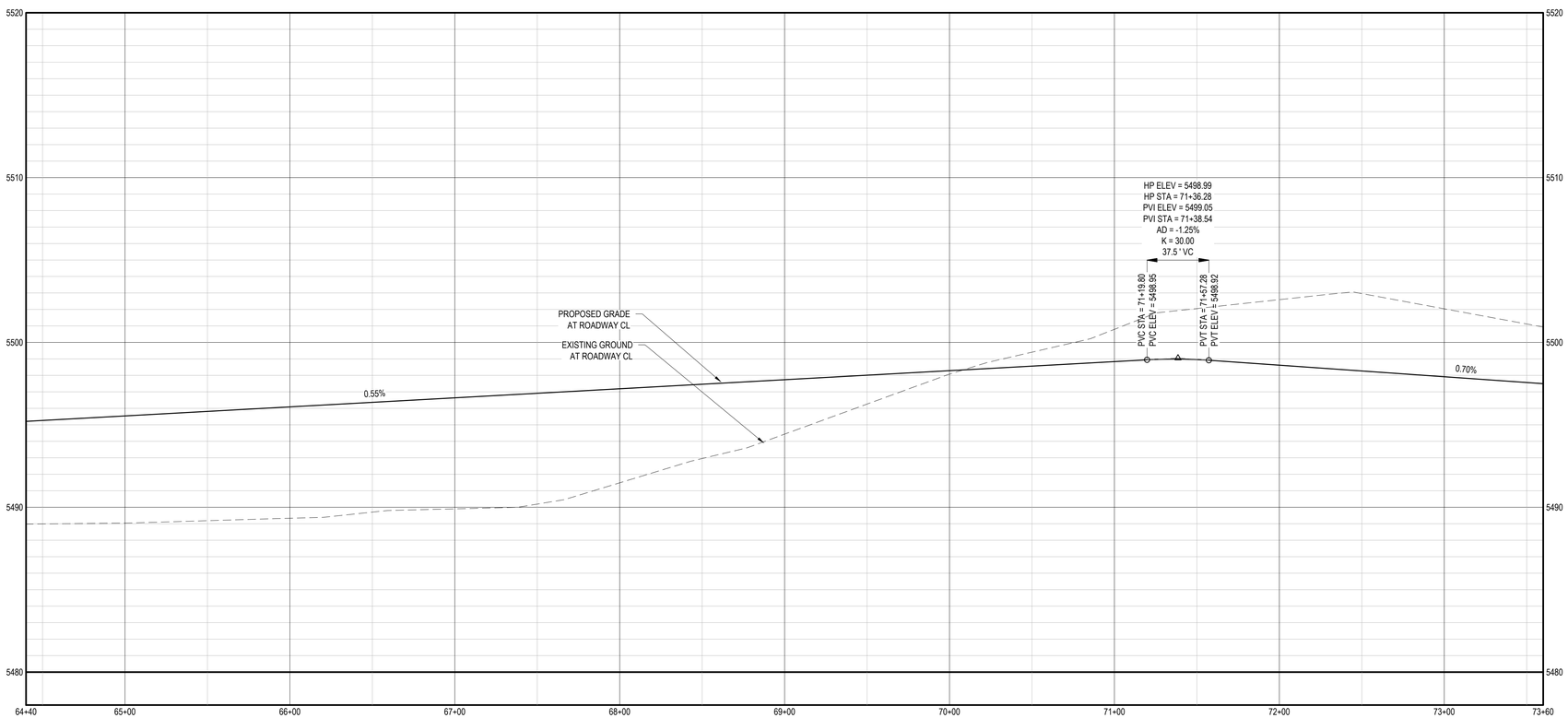


PLAN: PETTERSON RD STA: 64+40 TO 73+60

HORIZONTAL SCALE: 1" = 40'

0' 20 40 80

SCALE: 1"=40'



PROFILE: PETTERSON ROAD STA: 64+40 TO 73+60

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

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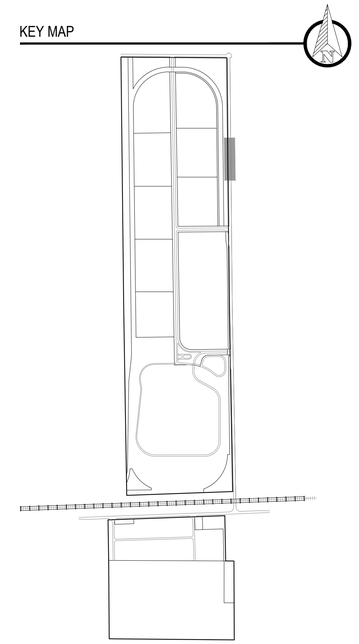
BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

ABBREVIATION LIST

BW/TW	BOTTOM OF WALL / TOP OF WALL	INV	INVERT
CBC	CONCRETE BOX CULVERT	KB/TB	KICK BLOCK / THRUST BLOCK
DIP	DUCTILE IRON PIPE	LP	LOW POINT
ESMT	EASEMENT	ME	MATCH EXISTING
EL	ELEVATION	MH	MANHOLE
EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
FG	FINISHED GRADE	RCP	REINFORCED CONCRETE PIPE
FL	FLOWLINE	REF	REFERENCE
FU	FUTURE	TC	TOP OF CURB
GB	GRADE BREAK	TP	TOP OF PIPE
GV	GATE VALVE	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURB
		W/	WITH

ROADWAY NOTES

1. PROPOSED DEVELOPMENT OF THE ROCKY MOUNTAIN RAIL PARK INCLUDE PUBLIC AND PRIVATE STREET, GRADING & DRAINAGE, WATER & SEWER IMPROVEMENTS AND GRADING AND EROSION SEDIMENT CONTROL (GESC). THESE PLANS ONLY INCLUDE THE PROPOSED CITY OF AURORA PUBLIC ROADWAY IMPROVEMENTS. THE PLANS FOR THE OTHER IMPROVEMENTS ARE ALL PREPARED BY ENERTIA CONSULTING GROUP, LLC. AND ARE UNDER SEPARATE COVER AS FOLLOWS:
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COLORADO REGISTERED PROFESSIONAL ENGINEER
RICHARD D. NELLGRAN
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
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PETTERSON RD STA: 64+40 TO 73+60
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
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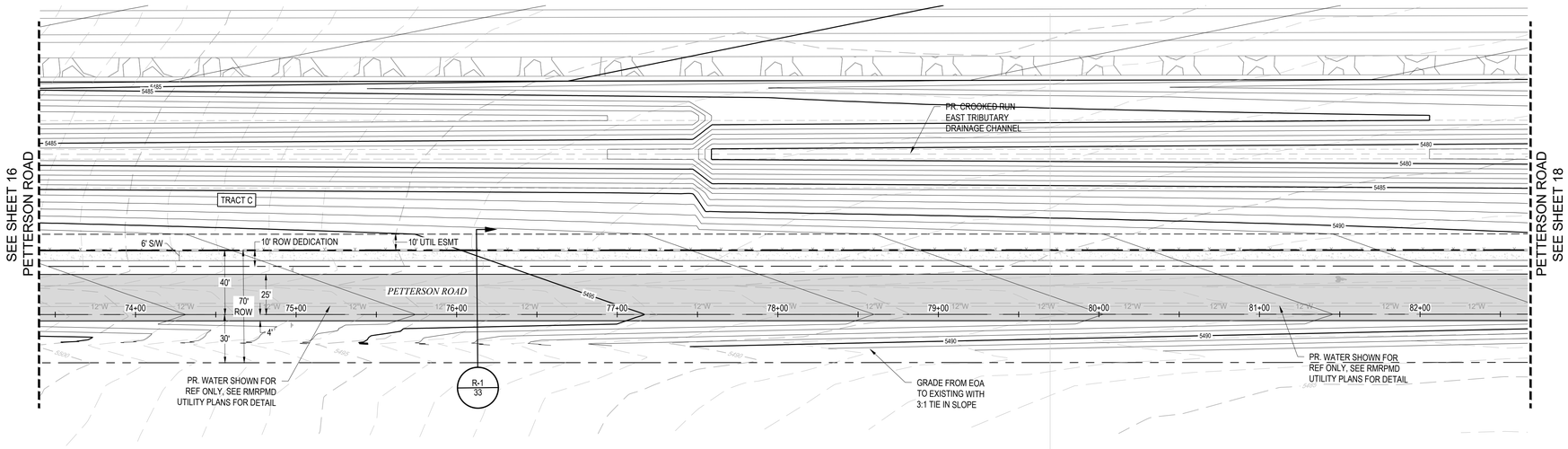
DWG	RDW-2.8
SHEET	16 OF 33

CASE #: PRC2018-00006

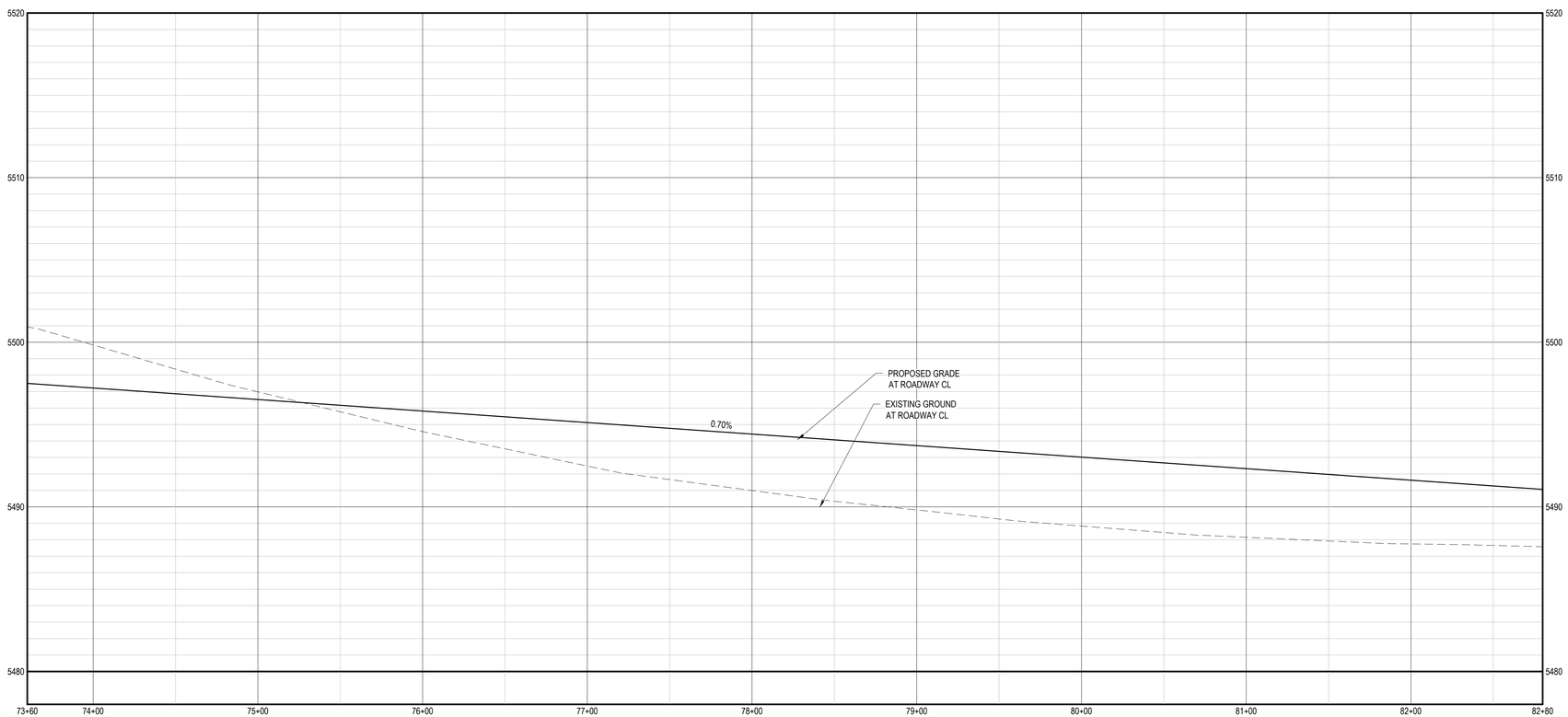


PLAN: PETTERSON RD STA: 73+60 TO 82+80

HORIZONTAL SCALE: 1" = 40'

0' 20 40 80

SCALE: 1"=40'



PROFILE: PETTERSON ROAD STA: 73+60 TO 82+80

HORIZONTAL SCALE: 1" = 40'
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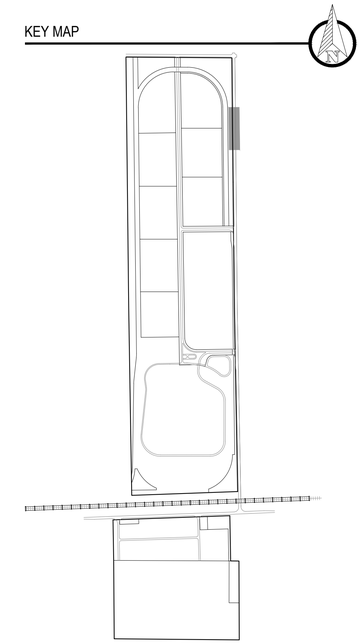

LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
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	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

ABBREVIATION LIST

BW/TW	BOTTOM OF WALL / TOP OF WALL	INV	INVERT
CBC	CONCRETE BOX CULVERT	KB/TB	KICK BLOCK / THRUST BLOCK
DIP	DUCTILE IRON PIPE	LP	LOW POINT
ESMT	EASEMENT	ME	MATCH EXISTING
EL	ELEVATION	MH	MANHOLE
EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
FG	FINISHED GRADE	RCP	REINFORCED CONCRETE PIPE
FL	FLOWLINE	REF	REFERENCE
FU	FUTURE	TC	TOP OF CURB
GB	GRADE BREAK	TP	TOP OF PIPE
GV	GATE VALVE	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURB
		W/	WITH

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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLGREN
38176
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
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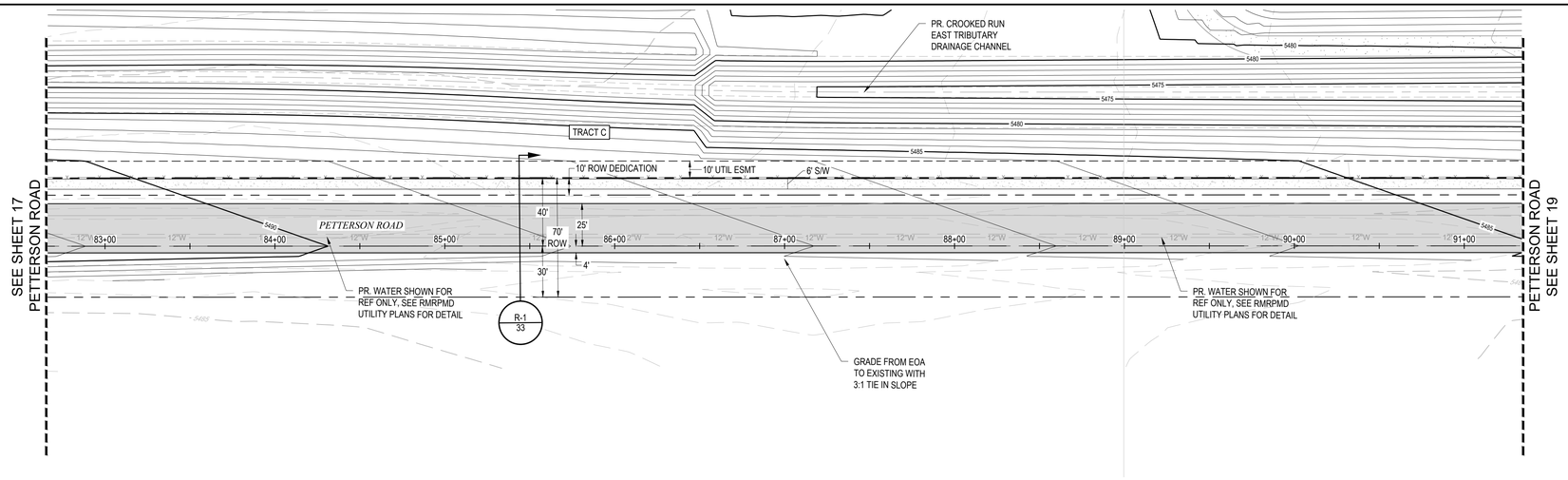
AURORA, COLORADO

PETTERSON RD STA: 73+60 TO 82+80
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.9
SHEET	17 OF 33

CASE #: PRC2018-00006

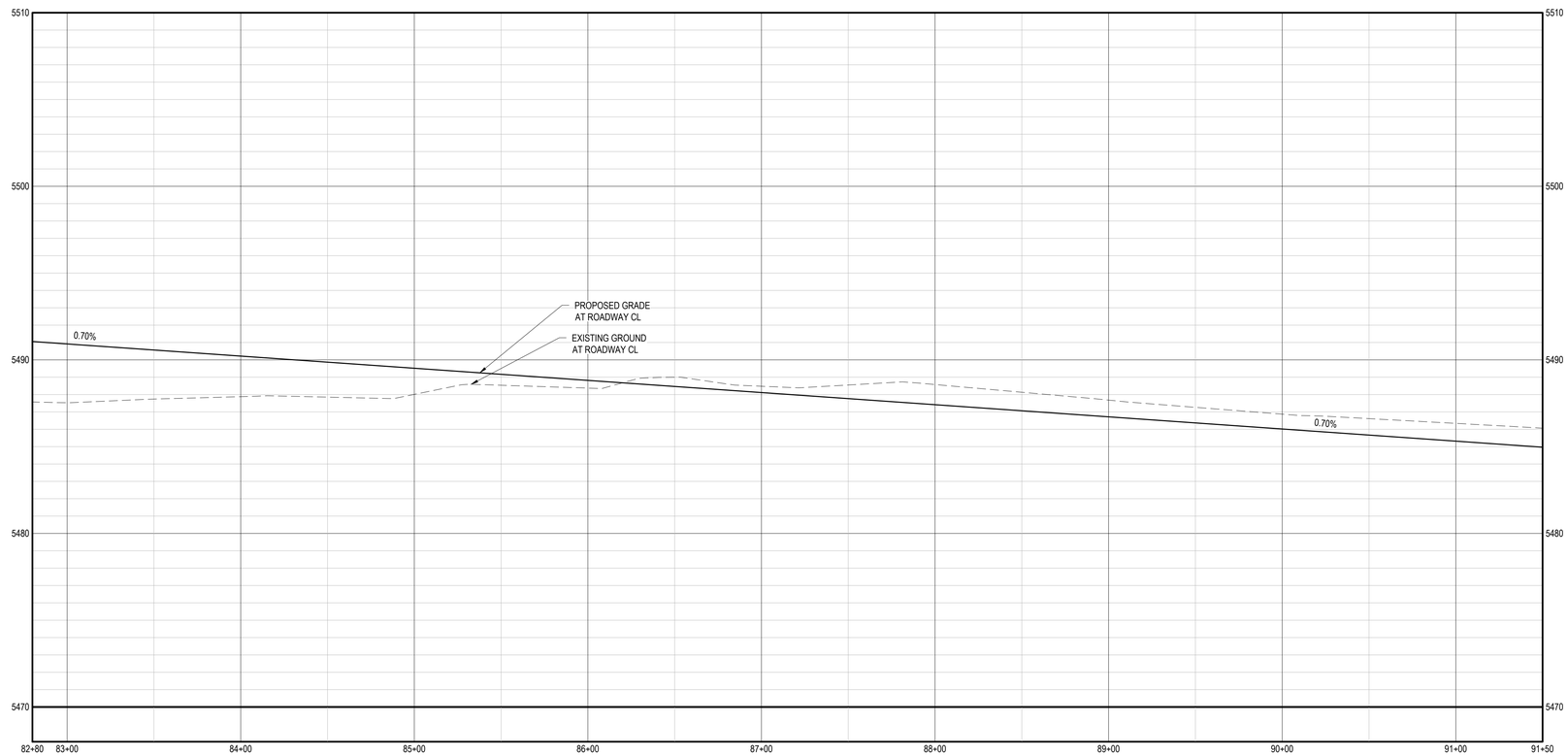


PLAN: PETTERSON ROAD STA: 82+80 TO 91+50

HORIZONTAL SCALE: 1" = 40'

0' 20 40 80

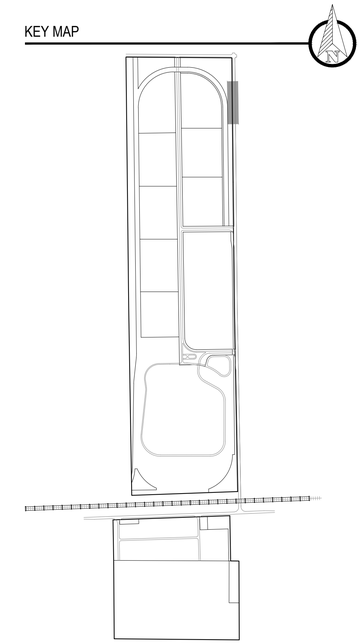
SCALE: 1"=40'



PROFILE: PETTERSON ROAD STA: 82+80 TO 91+50

HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
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38175
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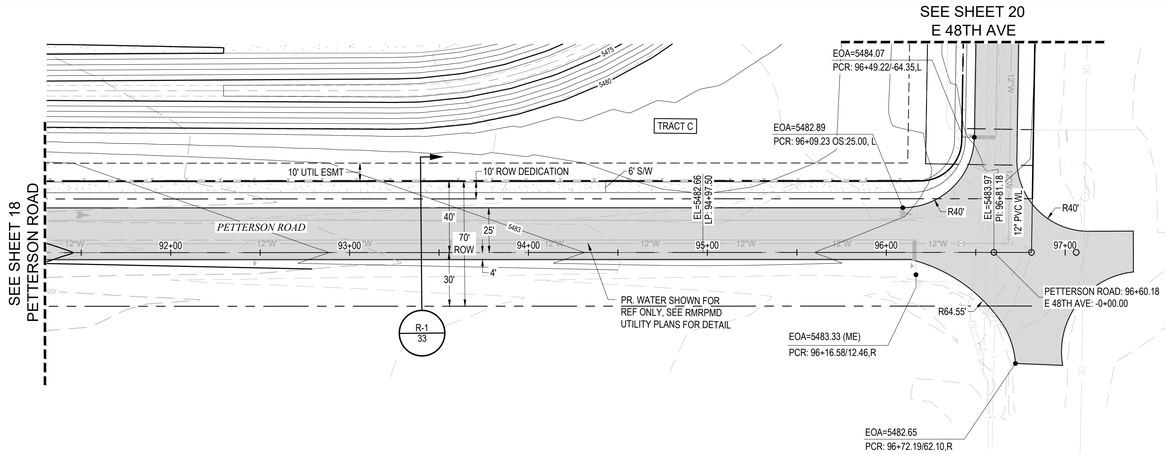
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON ROAD STA: 82+80 TO 91+50
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.10
SHEET	18 OF 33

CASE #: PRC2018-00006

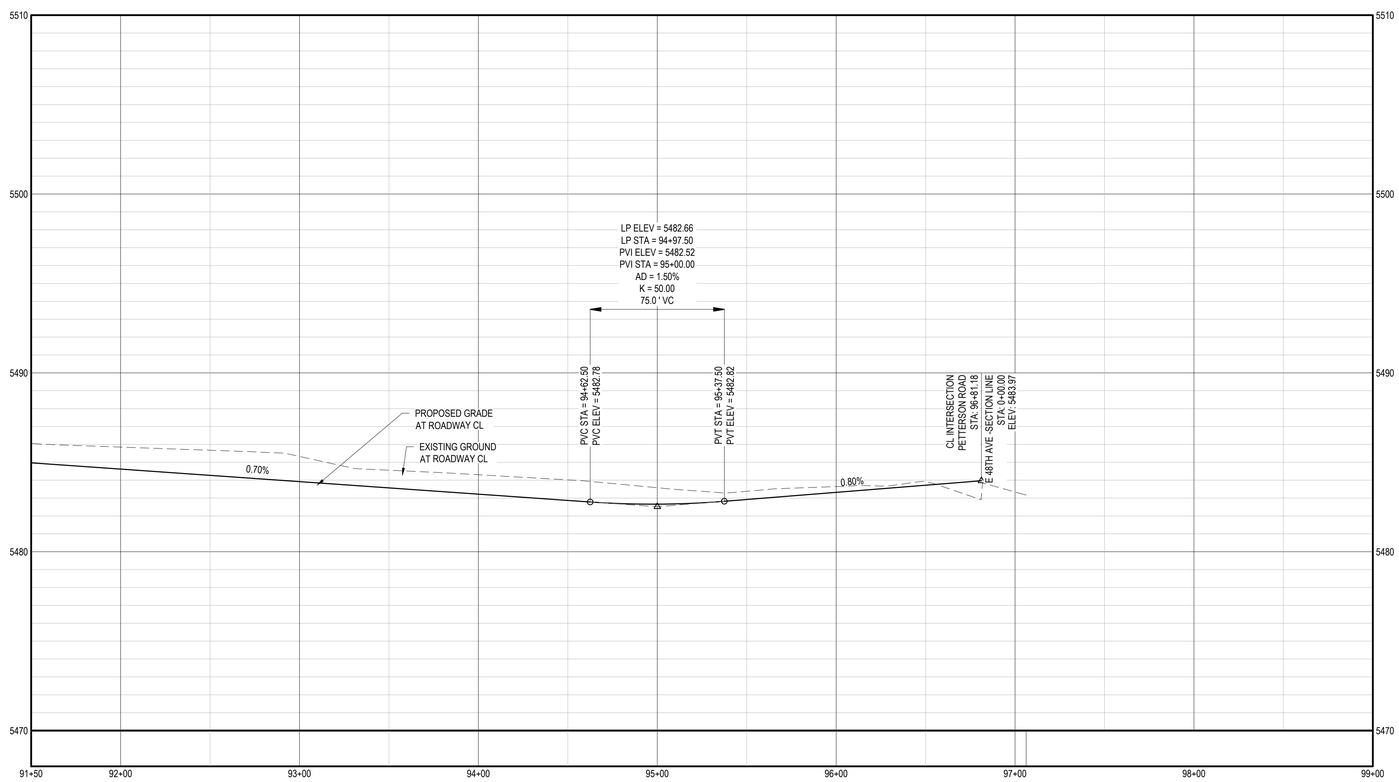


PLAN: PETERSON RD STA: 91+50 TO 99+00

HORIZONTAL SCALE: 1" = 40'

0' 20 40 80

SCALE: 1"=40'



PROFILE: PETTERSON ROAD STA: 91+50 TO 99+00

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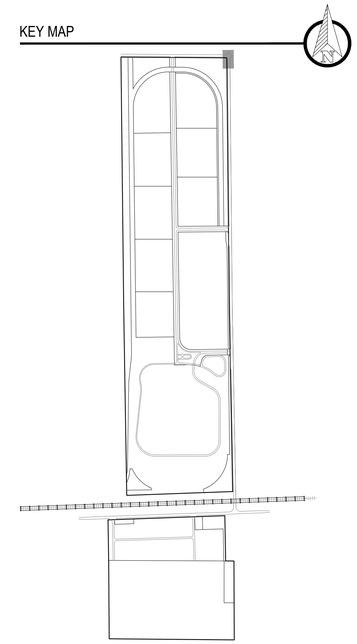

LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLGREN
38176
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN RAIL PARK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

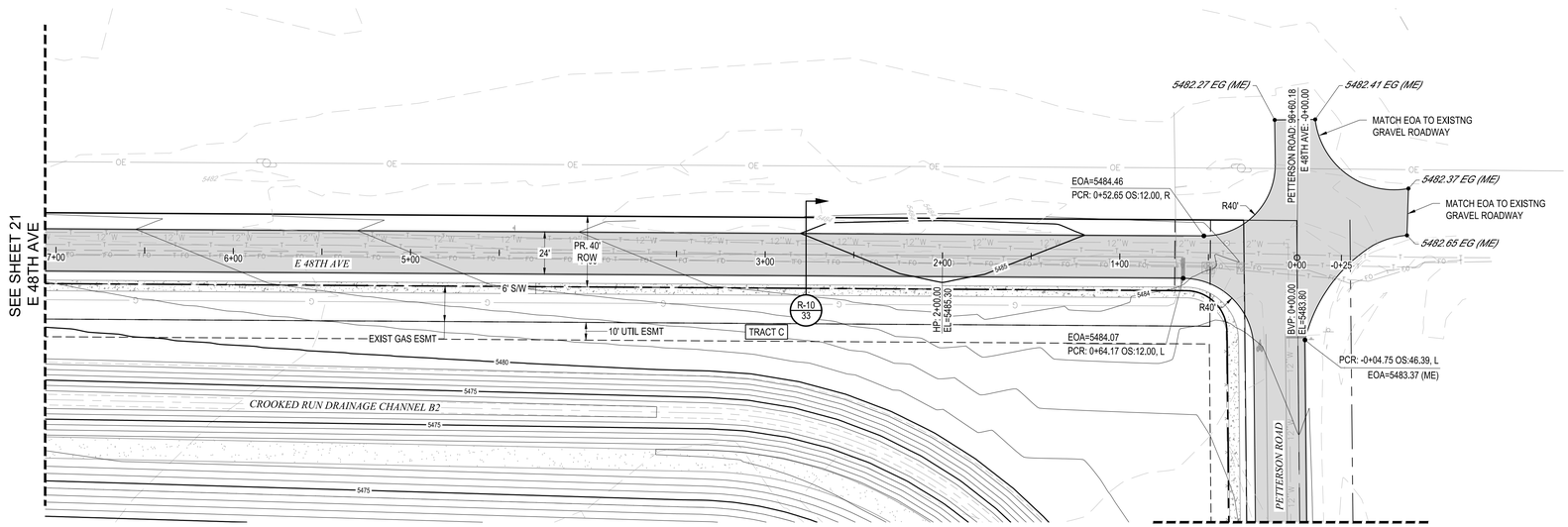
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETERSON RD STA: 91+50 TO 99+00
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

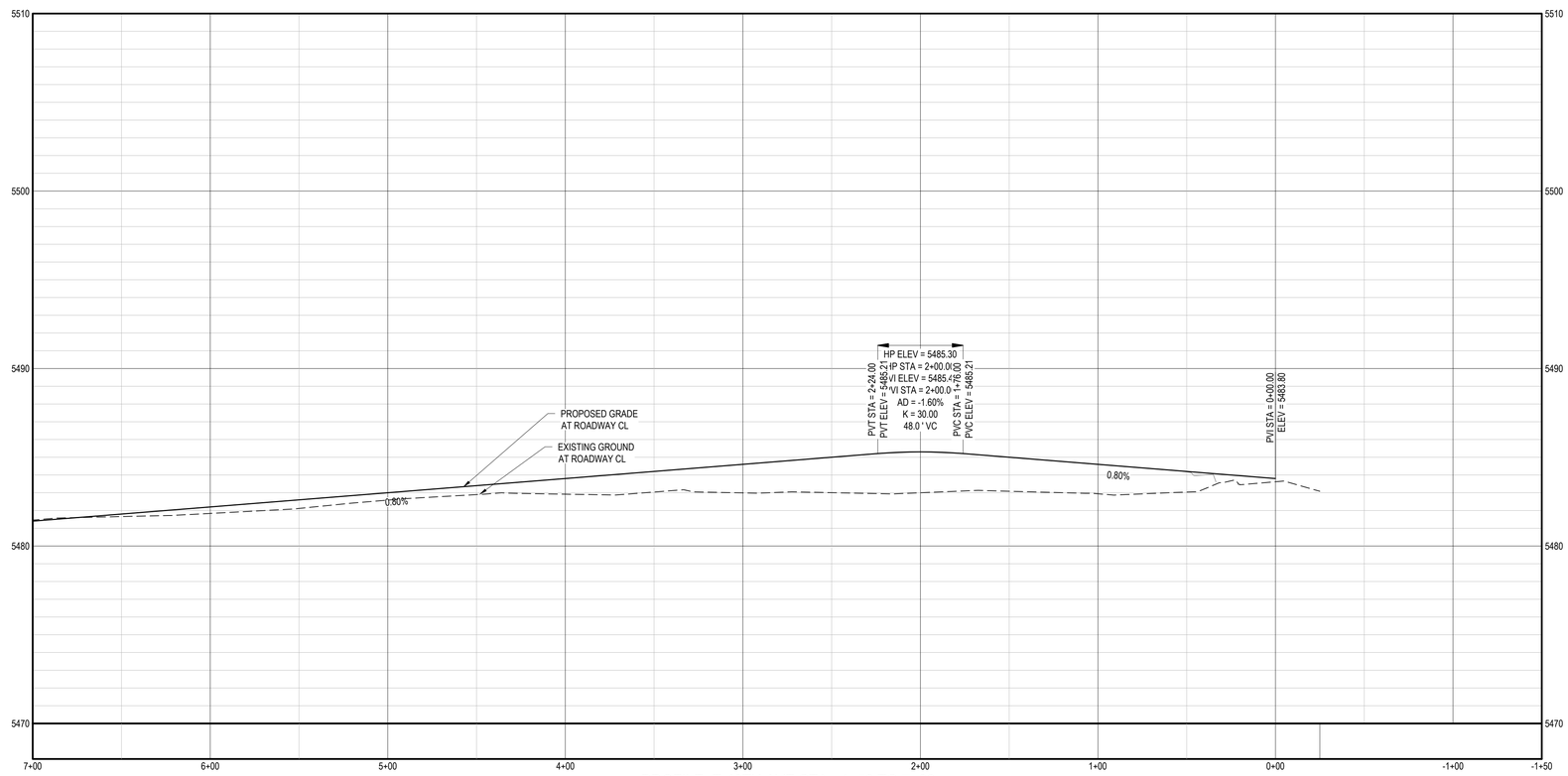
DATE	5/15/2020
FILE	
DWG	RDW-2.11
SHEET	19 OF 33

CASE #: PRC2018-00006



PLAN: E 48TH AVE STA: -1+50 TO 7+00

HORIZONTAL SCALE: 1" = 40'



PROFILE: E 48TH AVE STA: -1+50 TO 7+00

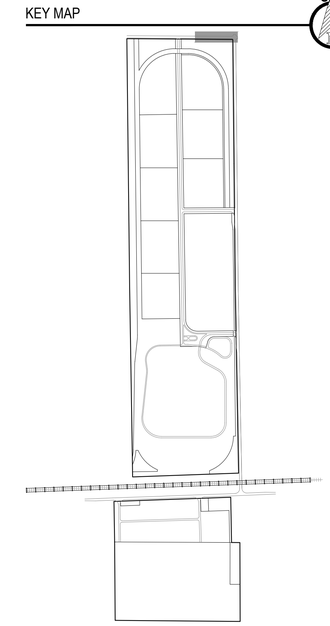
HORIZONTAL SCALE: 1" = 40'
VERTICAL SCALE: 1" = 4

FOR BURIED UTILITY INFORMATION
THREE (3) BUSINESS DAYS BEFORE YOU DIG
CALL 811
(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

ABBREVIATION LIST

BW/TW	BOTTOM OF WALL / TOP OF WALL	INV	INVERT
CBC	CONCRETE BOX CULVERT	KB/TB	KICK BLOCK / THRUST BLOCK
DIP	DUCTILE IRON PIPE	LP	LOW POINT
ESMT	EASEMENT	ME	MATCH EXISTING
EL	ELEVATION	MH	MANHOLE
EOA	EDGE OF ASPHALT	OHE	OVERHEAD ELECTRIC
EX	EXISTING	PR	PROPOSED
FES	FLARED END SECTION	PVC	POLYVINYL CHLORIDE PIPE
FG	FINISHED GRADE	RCP	REINFORCED CONCRETE PIPE
FL	FLOWLINE	REF	REFERENCE
FU	FUTURE	TC	TOP OF CURB
GB	GRADE BREAK	TP	TOP OF PIPE
GV	GATE VALVE	UE	UTILITY EASEMENT
HP	HIGH POINT	VC	VERTICAL CURB
		W/	WITH

ROADWAY NOTES

1. PROPOSED DEVELOPMENT OF THE ROCKY MOUNTAIN RAIL PARK INCLUDE PUBLIC AND PRIVATE STREET, GRADING & DRAINAGE, WATER & SEWER IMPROVEMENTS AND GRADING AND EROSION SEDIMENT CONTROL (GESC). THESE PLANS ONLY INCLUDE THE PROPOSED CITY OF AURORA PUBLIC ROADWAY IMPROVEMENTS. THE PLANS FOR THE OTHER IMPROVEMENTS ARE ALL PREPARED BY ENERTIA CONSULTING GROUP, LLC. AND ARE UNDER SEPARATE COVER AS FOLLOWS:
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COLORADO REGISTERED PROFESSIONAL ENGINEER
TODD D. NELLOGAN
38176
5/15/20
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DSGN	DR	CHK	APVD	

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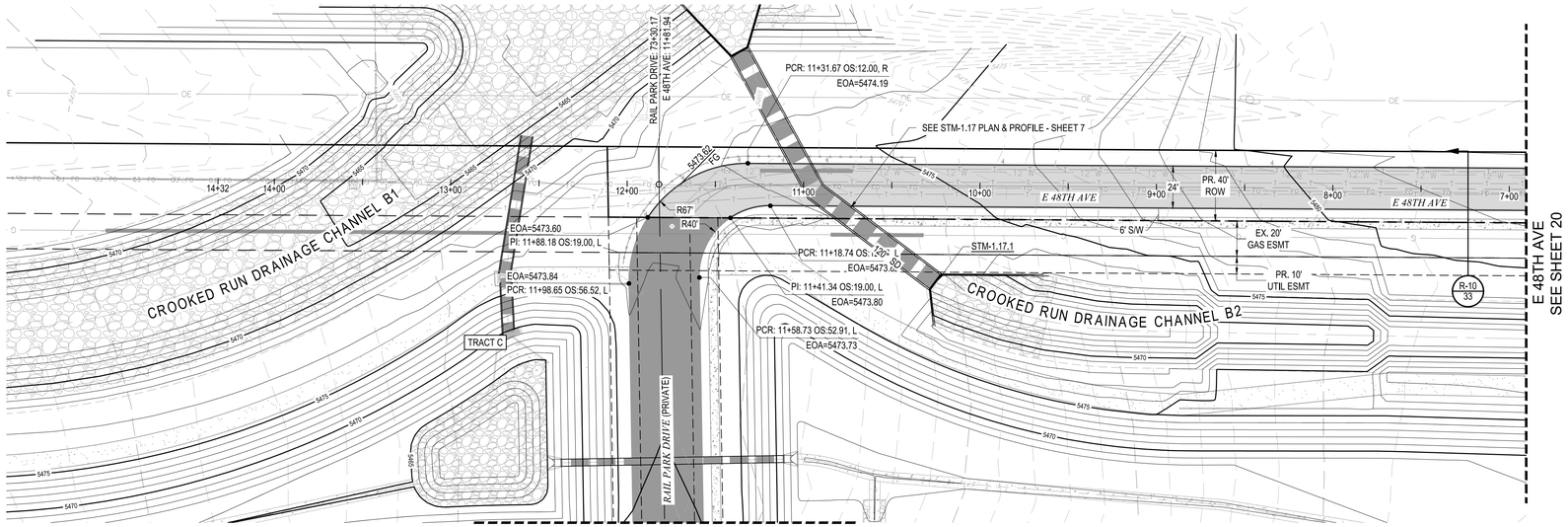
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

E 48TH AVE STA: -1+50 TO 7+00
PLAN & PROFILE

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.12
SHEET	20 OF 33

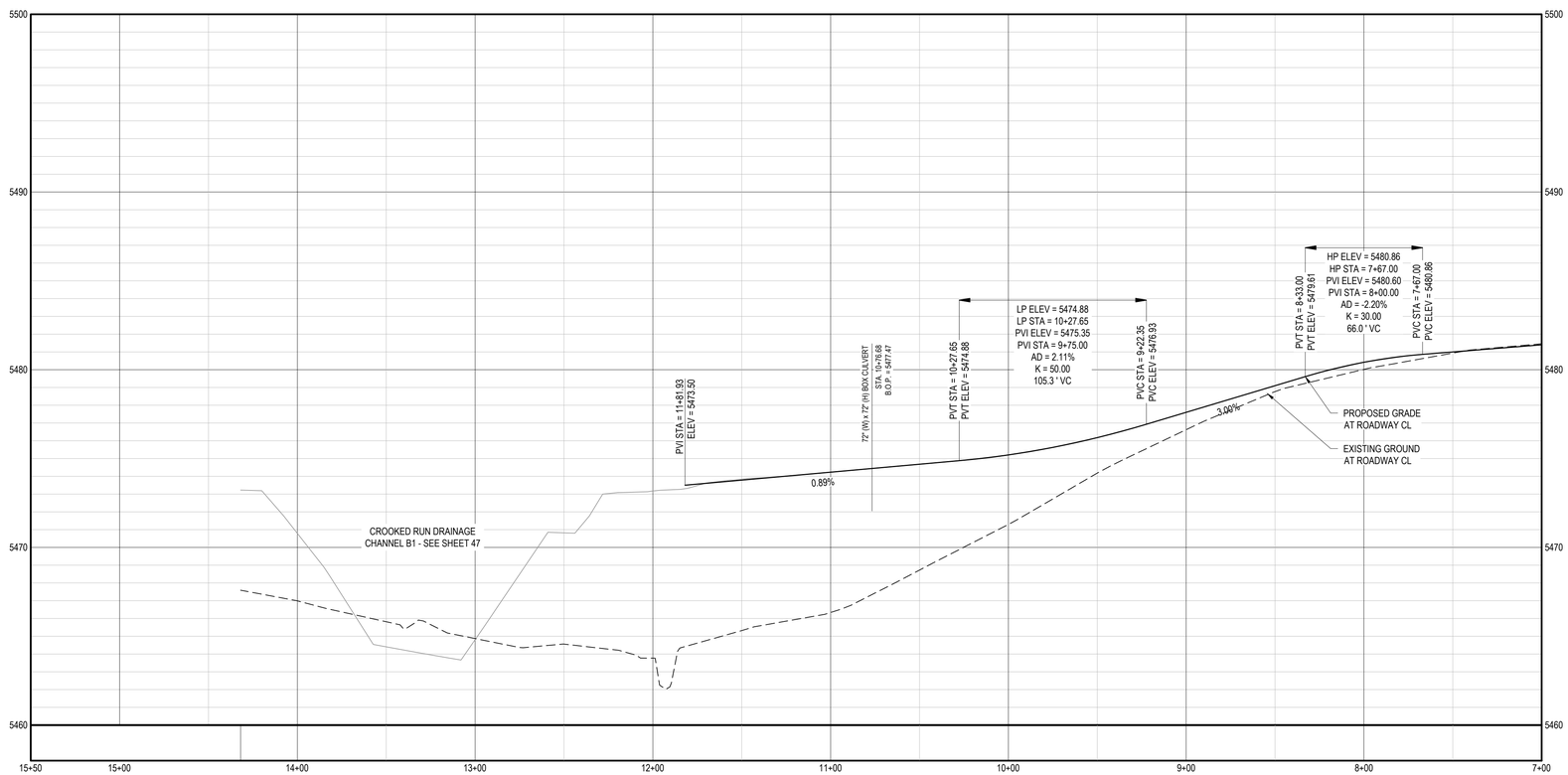
CASE #: PRC2018-00006



RAIL PARK DRIVE
SEE CIVIL CONSTRUCTION PLANS

PLAN: E48TH AVE STA: 7+00 TO 15+50

HORIZONTAL SCALE: 1" = 40'



PROFILE: E 48TH AVE STA: 7+00 TO 15+50

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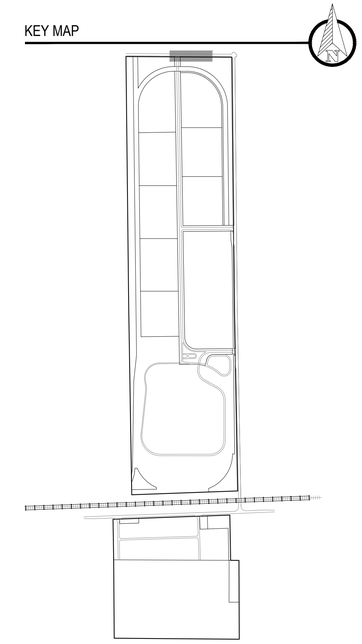

LEGEND

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	STRIPING		STORM DRAIN
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PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

E48TH AVE STA: 7+00 TO 15+50
PLAN & PROFILE

VERIFY SCALE
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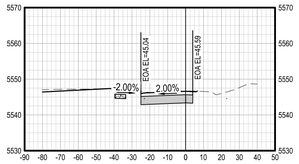
0 1"

DATE	5/15/2020
FILE	
DWG	RDW-2.13
SHEET	21 OF 33

CASE #: PRC2018-00006

STA: 4+00



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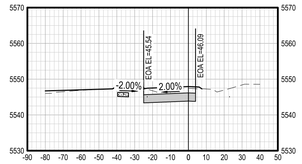

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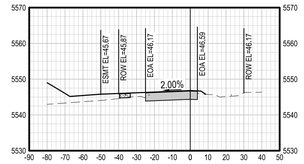

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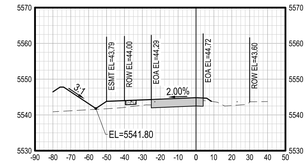

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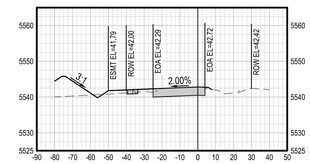

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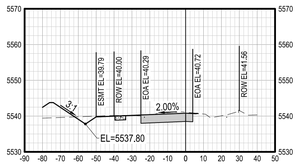

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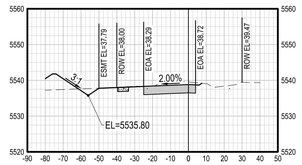

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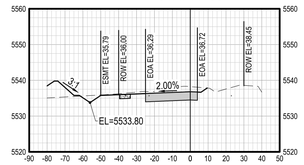

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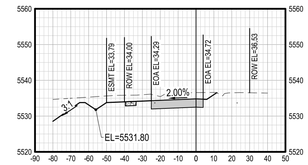

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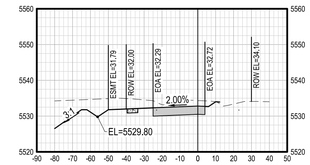

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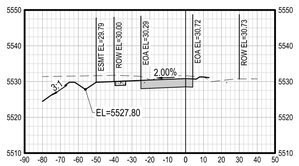

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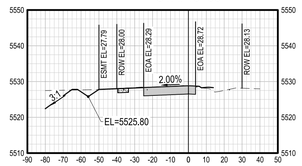

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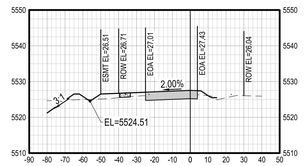

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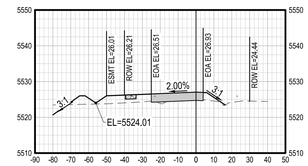

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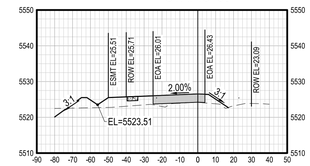

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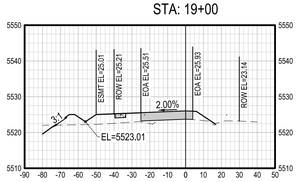

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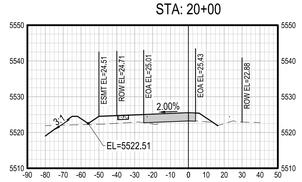

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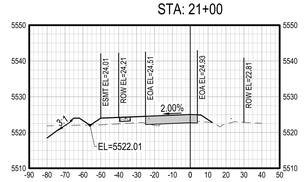

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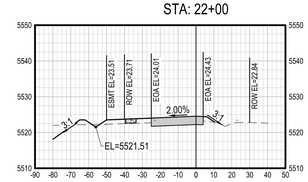

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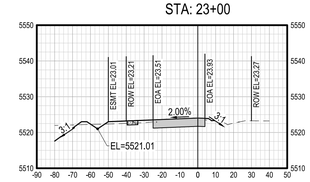

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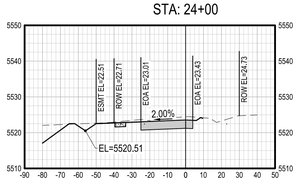
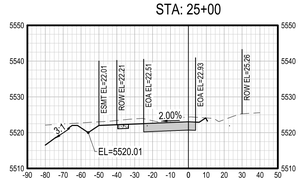
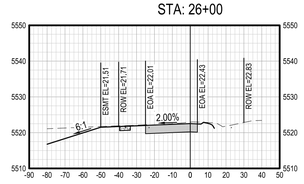


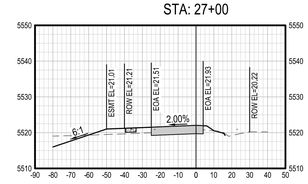
STA: 25+00



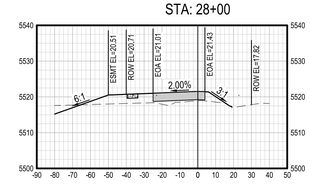
STA: 26+00



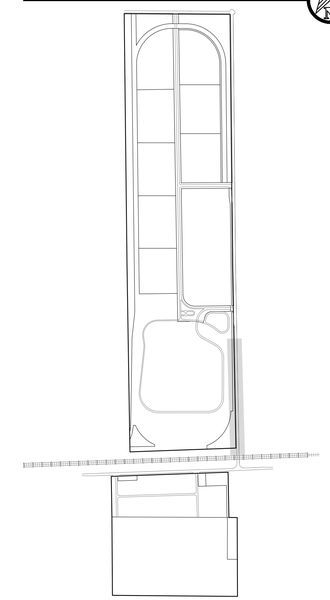
STA: 27+00



STA: 28+00



KEY MAP



ROADWAY NOTES

1. PETERSON ROAD STATIONING IS ON PGL OF ROADWAY, PGL = ROW CENTERLINE AND ULTIMATE CL OF PETERSON ROAD.
2. EAST 48TH AVENUE STATIONING IS ON PGL OF ROADWAY, PGL = PAVEMENT CENTERLINE .
3. PETERSON ROAD AND EAST 48TH AVENUE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF AURORA STANDARDS AND SPECIFICATIONS.
6. REFERENCE THE APPROVED GEOTECHINAL STUDY FOR THE PROJECT FOR PAVEMENT DESIGN AND RECOMMENDATIONS.
7. ELEVATIONS ARE THE FINISHED GRADED ELEVATION ALONG THE PROPOSED ROADWAY SURFACE AND DO NOT ACCOUNT FOR PAVEMENT OR MATERIAL DEPTHS.

CROSS SECTION LEGEND

- EXISTING GROUND ALONG SECTION
- PROPOSED GRADE ALONG SECTION
- PROPOSED ROADWAY PAVEMENT SECTION (SEE NOTE 6)
- PROPOSED 6' SIDEWALK

0' 20 40 80

SCALE: HORZ 1"=40'
VERT 1"=4'

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(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELSON
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN BANK
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

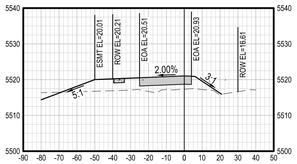
PETERSON ROAD STA: 4+00 TO 28+00 CROSS SECTIONS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SEC-3.0
SHEET	22 OF 33

CASE #: PRC2018-00006

STA: 29+00



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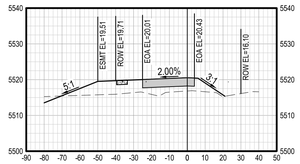
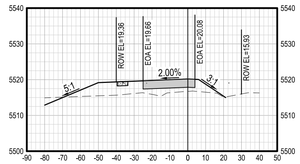


STA: 30+70



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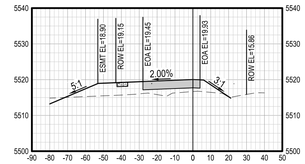
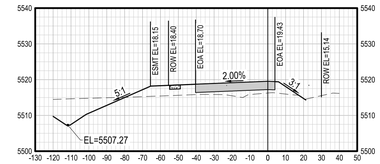


STA: 32+00



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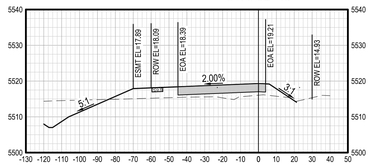

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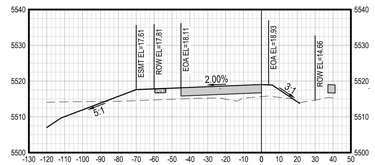
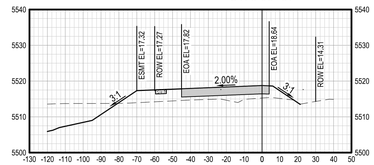


STA: 33+57



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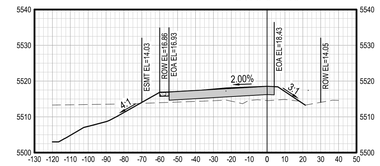

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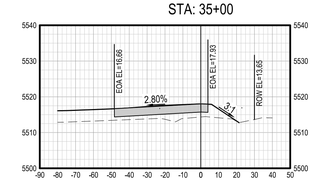

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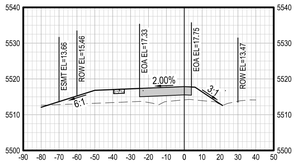

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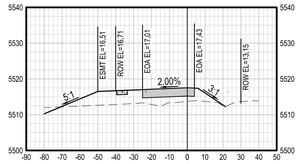

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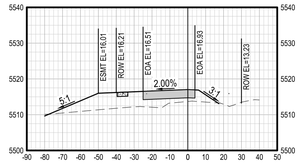
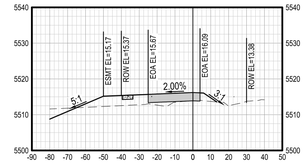
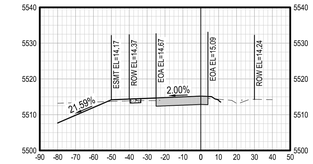


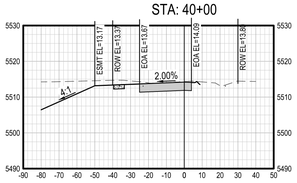
STA: 38+00



STA: 39+00



STA: 40+00



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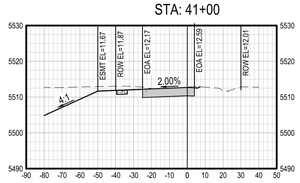

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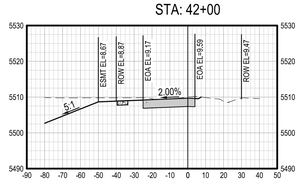
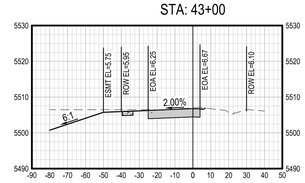


STA: 43+00



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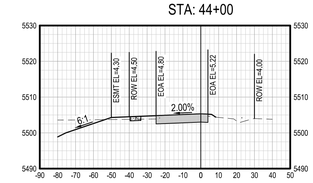

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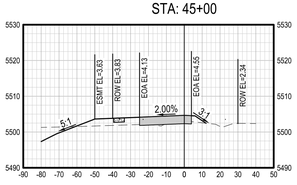

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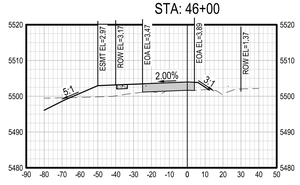

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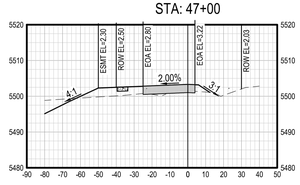
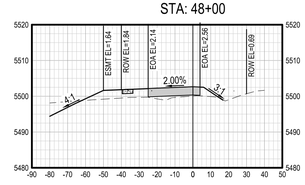
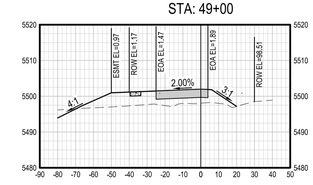


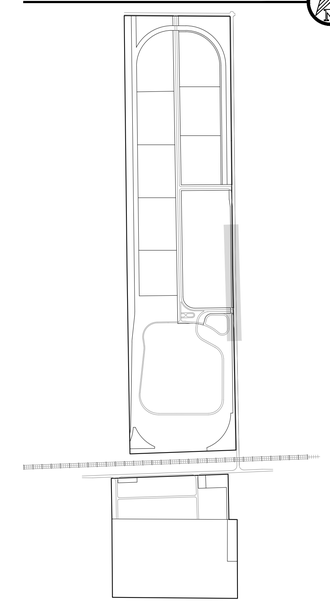
STA: 48+00



STA: 49+00



KEY MAP



ROADWAY NOTES

1. PETERSON ROAD STATIONING IS ON PGL OF ROADWAY. PGL = ROW CENTERLINE AND ULTIMATE CL OF PETERSON ROAD.
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CROSS SECTION LEGEND

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- PROPOSED ROADWAY PAVEMENT SECTION (SEE NOTE 6)
- PROPOSED 6' SIDEWALK

0' 20 40 80

SCALE: HORZ 1"=40'
VERT 1"=4'

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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLOMS
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

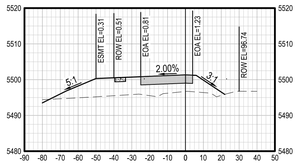
PETERSON ROAD STA: 29+00 TO 49+00
CROSS SECTIONS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SEC-3.1
SHEET	23 OF 33

CASE #: PRC2018-00006

STA: 50+00



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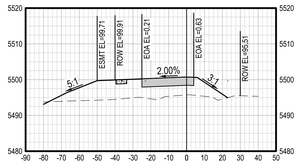
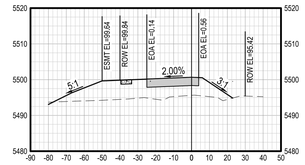
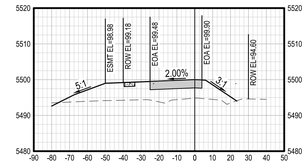


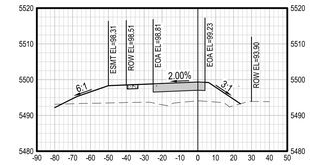
STA: 51+00



STA: 52+00



STA: 53+00



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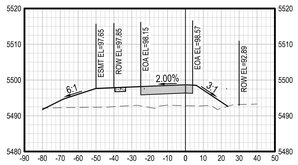

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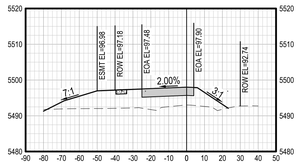

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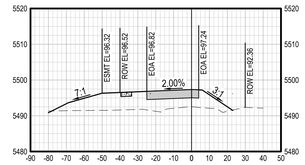
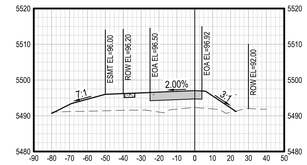
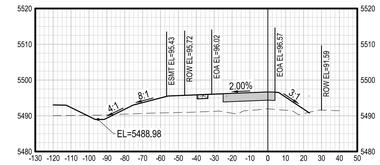


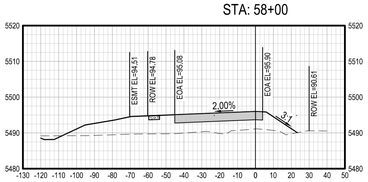
STA: 56+47



STA: 57+00



STA: 58+00



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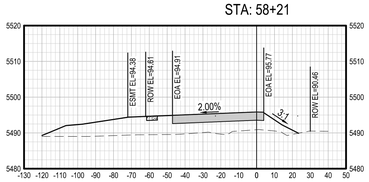

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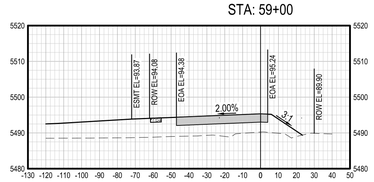
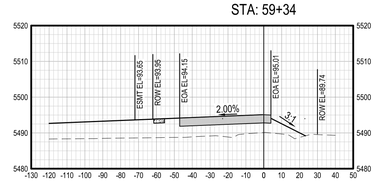


STA: 59+34



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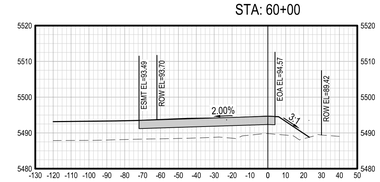

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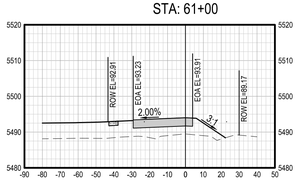

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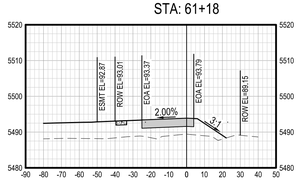
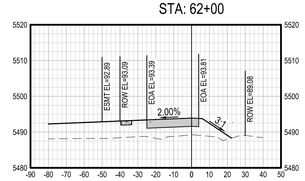


STA: 62+00



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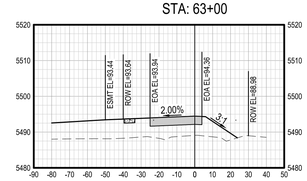

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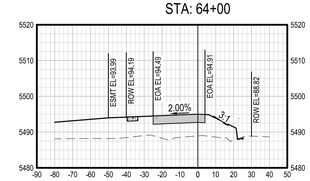

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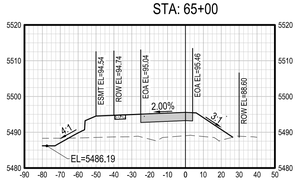
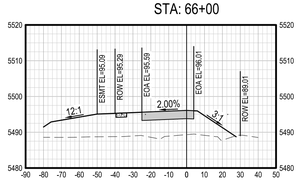


STA: 66+00



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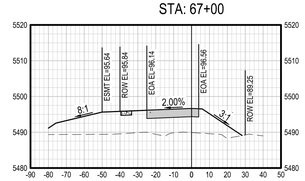

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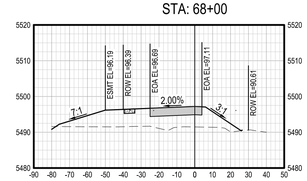
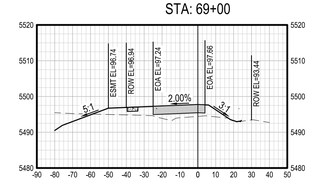
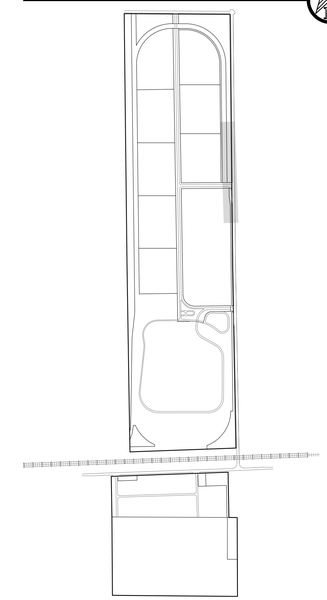


STA: 69+00



KEY MAP



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2. EAST 48TH AVENUE STATIONING IS ON PGL OF ROADWAY, PGL = PAVEMENT CENTERLINE .
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CROSS SECTION LEGEND

- EXISTING GROUND ALONG SECTION
- PROPOSED GRADE ALONG SECTION
- PROPOSED ROADWAY PAVEMENT SECTION (SEE NOTE 6)
- PROPOSED 6' SIDEWALK

0' 20 40 80

SCALE: HORZ 1"=40'
VERT 1"=4'

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(OR 1-800-922-1987)
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A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

COLORADO REGISTERED MATTHEW D. NELSON 38176 PROFESSIONAL ENGINEER
FOR REVIEW ONLY
5/15/20

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN BROADBAND
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

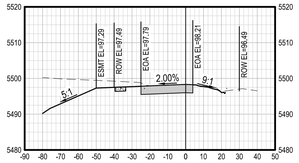
PETERSON ROAD STA: 50+00 TO 69+00 CROSS SECTIONS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SEC-3.2
SHEET	24 OF 33

CASE #: PRC2018-00006

STA: 70+00



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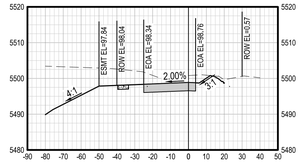

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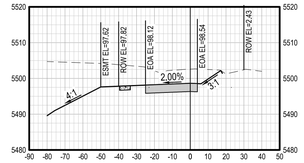

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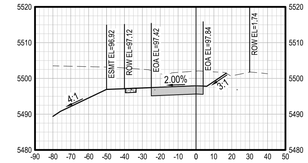

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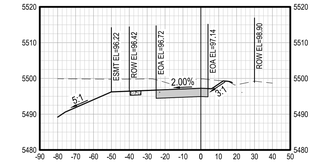

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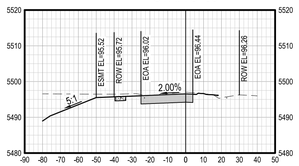

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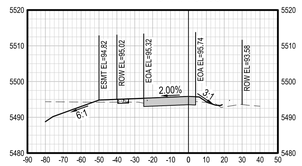

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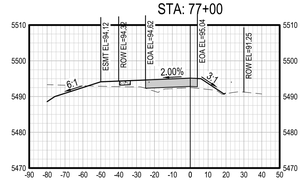

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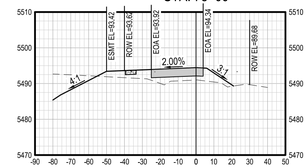

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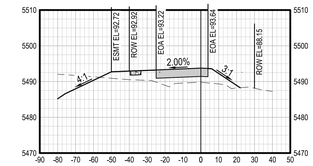

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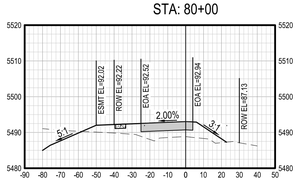

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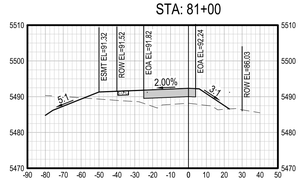

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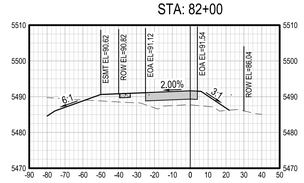
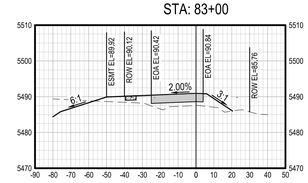
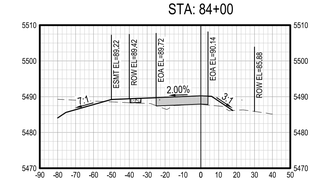


STA: 83+00



STA: 84+00



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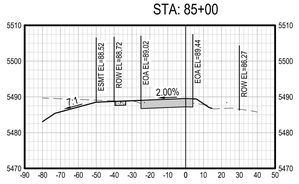

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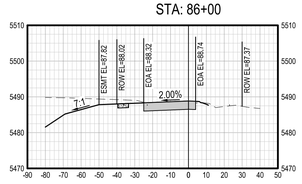

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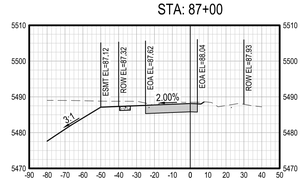
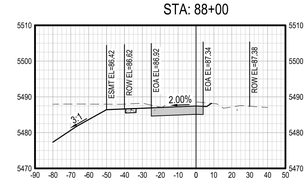
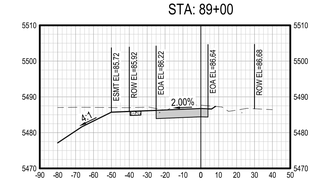


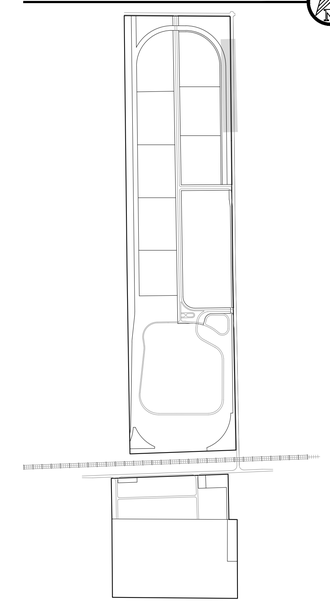
STA: 88+00



STA: 89+00



KEY MAP



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0' 20 40 80

SCALE: HORZ 1"=40'
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MATTHEW D. NELLOMS
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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DENVER, COLORADO 80202

ROCKY MOUNTAIN
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DENVER, CO 80237

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PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

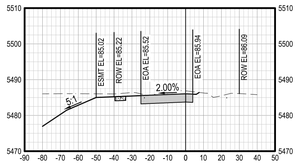
PETERSON ROAD STA: 70+00 TO 89+00 CROSS SECTIONS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

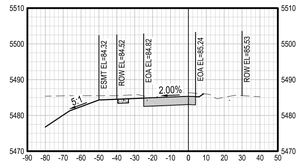
DATE	5/15/2020
FILE	
DWG	SEC-3.3
SHEET	25 OF 33

CASE #: PRC2018-00006

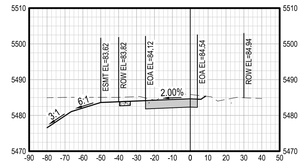
STA: 90+00



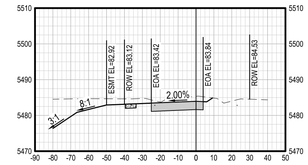
STA: 91+00



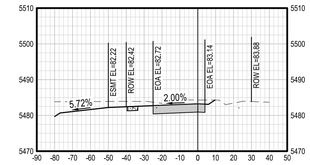
STA: 92+00



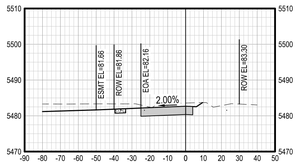
STA: 93+00



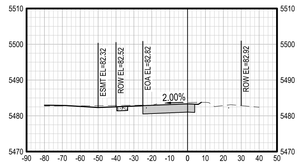
STA: 94+00



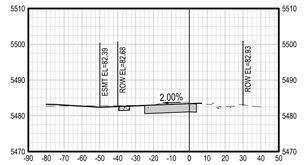
STA: 95+00



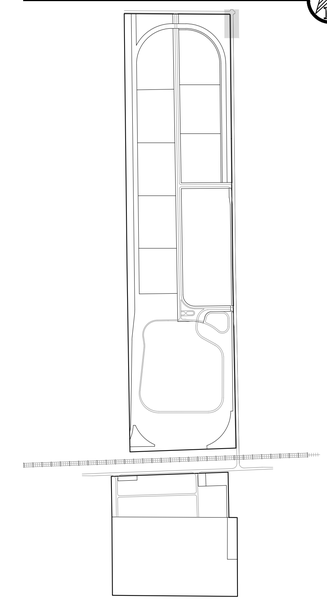
STA: 96+00



STA: 96+09



KEY MAP



ROADWAY NOTES

1. PETERSON ROAD STATIONING IS ON PGL OF ROADWAY, PGL = ROW CENTERLINE AND ULTIMATE CL OF PETERSON ROAD.
2. EAST 48TH AVENUE STATIONING IS ON PGL OF ROADWAY, PGL = PAVEMENT CENTERLINE .
3. PETERSON ROAD AND EAST 48TH AVENUE SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE CITY OF AURORA STANDARDS AND SPECIFICATIONS.
6. REFERENCE THE APPROVED GEOTECHINAL STUDY FOR THE PROJECT FOR PAVEMENT DESIGN AND RECOMMENDATIONS.
7. ELEVATIONS ARE THE FINISHED GRADED ELEVATION ALONG THE PROPOSED ROADWAY SURFACE AND DO NOT ACCOUNT FOR PAVEMENT OR MATERIAL DEPTHS.

CROSS SECTION LEGEND

- EXISTING GROUND ALONG SECTION
- PROPOSED GRADE ALONG SECTION
- PROPOSED ROADWAY PAVEMENT SECTION (SEE NOTE 6)
- PROPOSED 6' SIDEWALK

0' 20 40 80

SCALE: HORZ 1"=40'
VERT 1"=4'

FOR BURIED UTILITY INFORMATION
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(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLOGAN
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
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ROCKY MOUNTAIN BRANCH
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

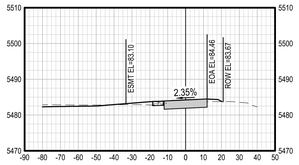
PETERSON ROAD STA: 90+00 TO 96+09 CROSS SECTIONS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

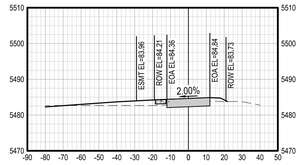
DATE	5/15/2020
FILE	
DWG	SEC-3.4
SHEET	26 OF 33

CASE #: PRC2018-00006

STA: 0+53



STA: 1+00



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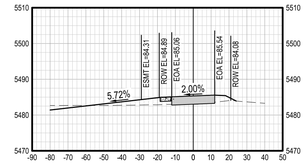

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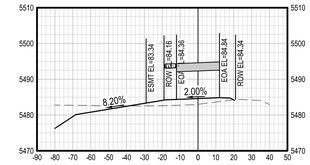
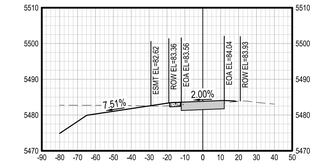


STA: 4+00



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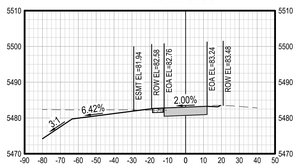
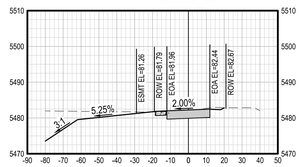
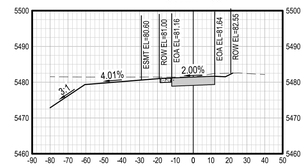


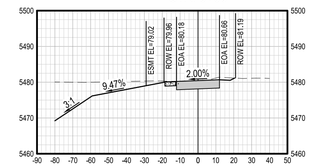
STA: 6+00



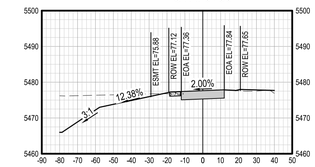
STA: 7+00



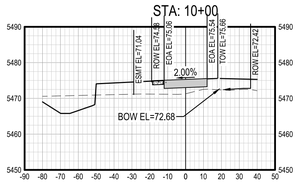
STA: 8+00



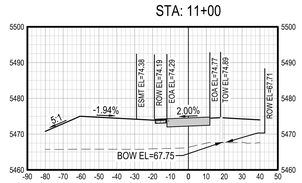
STA: 9+00



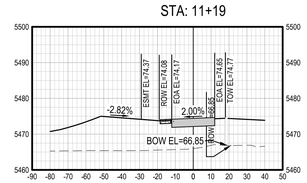
STA: 10+00



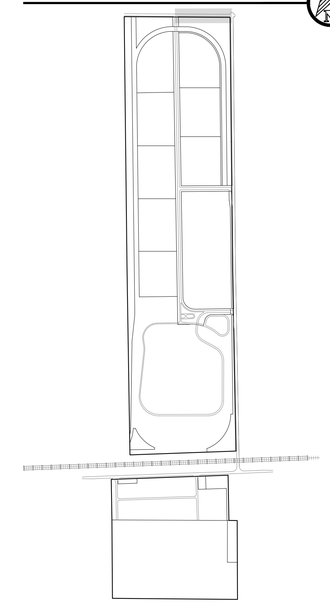
STA: 11+00



STA: 11+19



KEY MAP



ROADWAY NOTES

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2. EAST 48TH AVENUE STATIONING IS ON PGL OF ROADWAY, PGL = PAVEMENT CENTERLINE .
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6. REFERENCE THE APPROVED GEOTECHINAL STUDY FOR THE PROJECT FOR PAVEMENT DESIGN AND RECOMMENDATIONS.
7. ELEVATIONS ARE THE FINISHED GRADED ELEVATION ALONG THE PROPOSED ROADWAY SURFACE AND DO NOT ACCOUNT FOR PAVEMENT OR MATERIAL DEPTHS.

CROSS SECTION LEGEND

- EXISTING GROUND ALONG SECTION
- PROPOSED GRADE ALONG SECTION
- PROPOSED ROADWAY PAVEMENT SECTION (SEE NOTE 6)
- PROPOSED 6' SIDEWALK

0' 20 40 80

SCALE: HORZ 1"=40'
VERT 1"=4'

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BENCHMARK

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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLOMS
38176
5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
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DENVER, CO 80237

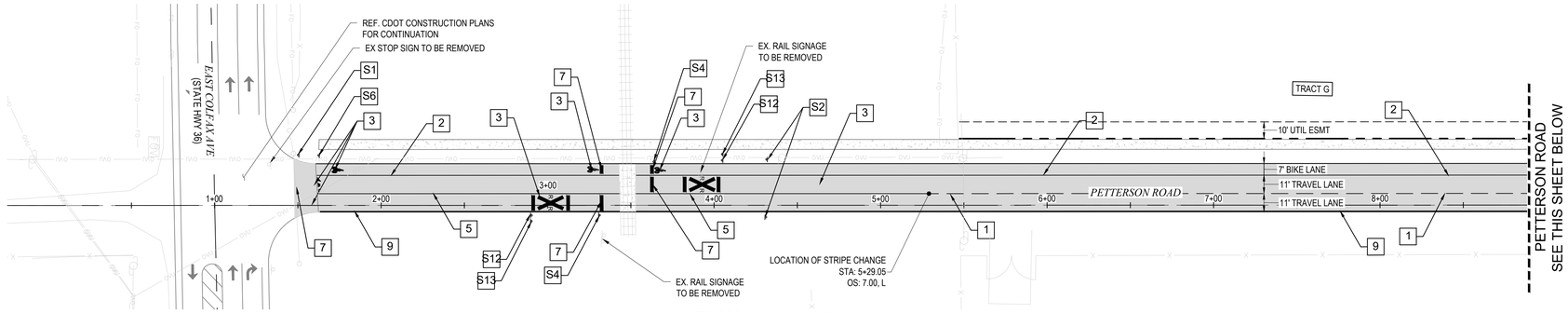
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

E. 48TH AVENUE STA: 0+53 TO 11+19 CROSS SECTIONS

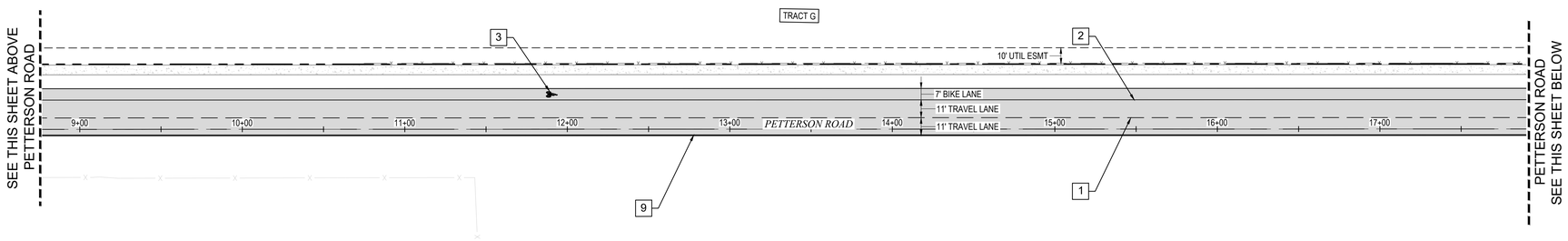
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SEC-3.5
SHEET	27 OF 33

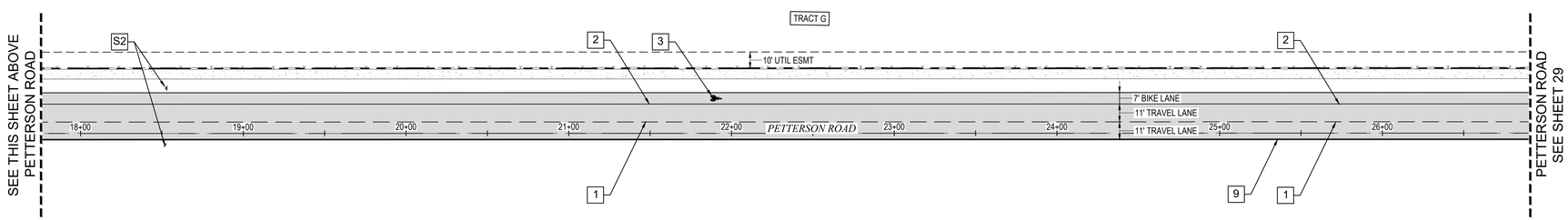
CASE #: 2020-00000



PLAN: PETTERSON RD STA: 1+00 TO 9+00
HORIZONTAL SCALE: 1" = 40'

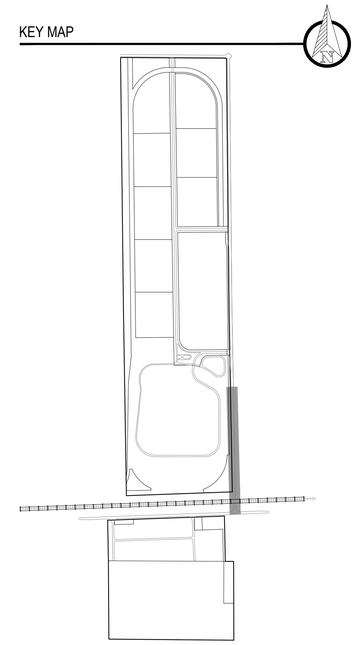


PLAN: PETTERSON RD STA: 9+00 TO 18+00
HORIZONTAL SCALE: 1" = 40'



PLAN: PETTERSON RD STA: 18+00 TO 27+00
HORIZONTAL SCALE: 1" = 40'

KEY MAP

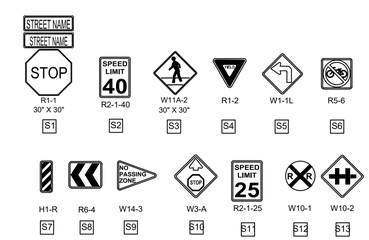


LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

CONSTRUCTION STRIPING NOTES & LEGEND

1. INSTALL SKIP LANE LINE 4" DASHED YELLOW STRIPING.
2. INSTALL BIKE LANE 6" SOLID WHITE STRIPING PER CITY OF AURORA STANDARDS & SPECIFICATIONS.
3. INSTALL PAVEMENT MARKING AS SHOWN, MARKING SHALL BE IN ACCORDANCE WITH CITY OF AURORA STANDARDS AND CDOT STANDARDS.
4. INSTALL DOTTED LANE LINE 4" SOLID WHITE STRIPING AT 2' LENGTH AND 4' INTERVALS. INSTALL IN ACCORDANCE WITH STRIPING STANDARDS.
5. INSTALL 4" SOLID DOUBLE YELLOW STRIPING LINE PER CITY OF AURORA STANDARDS & SPECIFICATIONS.
6. INSTALL CHANNELIZING LANE LINE 8" SOLID WHITE STRIPING.
7. INSTALL 24" STOP BAR WHITE
8. INSTALL 2' X 8' CROSS WALK WHITE 4' OC
9. INSTALL 4" SOLID YELLOW LANE LINE ALONG SHOULD ER OF ROADWAY



0' 20 40 80
SCALE: 1"=40'

STRIPING NOTES

1. PROPOSED STRIPING SHALL BE IN ACCORDANCE WITH THE CITY OF AURORAS STANDARDS AND SPECIFICATION AND IN ADDITION TO THE MUTCD.
2. RAIL CROSSING SIGNAGE SHALL FOLLOW UNION PACIFIC SIGNAGE, LIGHTING AND STRIPING STANDARDS.
3. LIGHTED RAIL CROSSING DESIGN SHALL BE PROVIDED BY A REGISTERED PROFESSIONAL ENGINEER WITH A STANDARD OF CARE FOR RAIL CROSSING. THIS PLAN IS NOT INTENDED TO PROVIDE SIGNALIZED, PROTECTED OR LIGHTED CROSSING DESIGN.

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BENCHMARK

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FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN
4601 DTC BLVD., SUITE 130
DENVER, CO 80237

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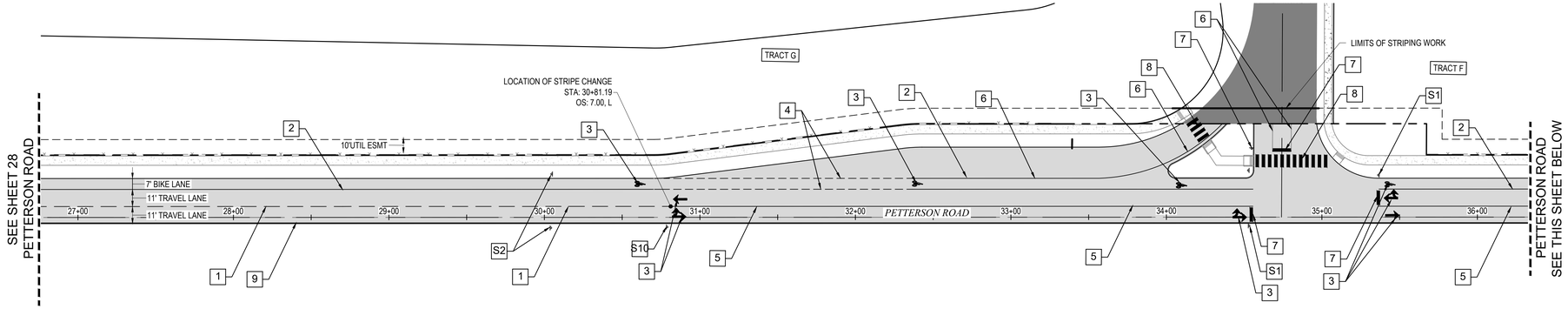
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 1+00 TO 27+00
SIGNAGE & STRIPING PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

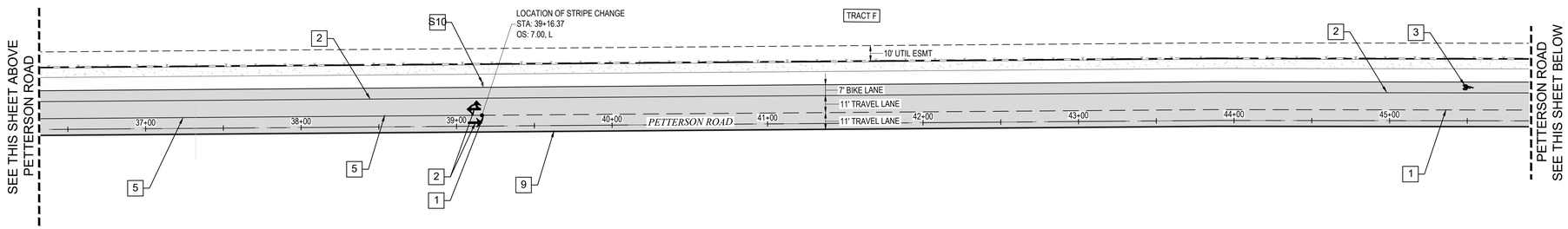
DATE	5/15/2020
FILE	
DWG	SSP-4.0
SHEET	28 OF 33

CASE #: 2020-00000



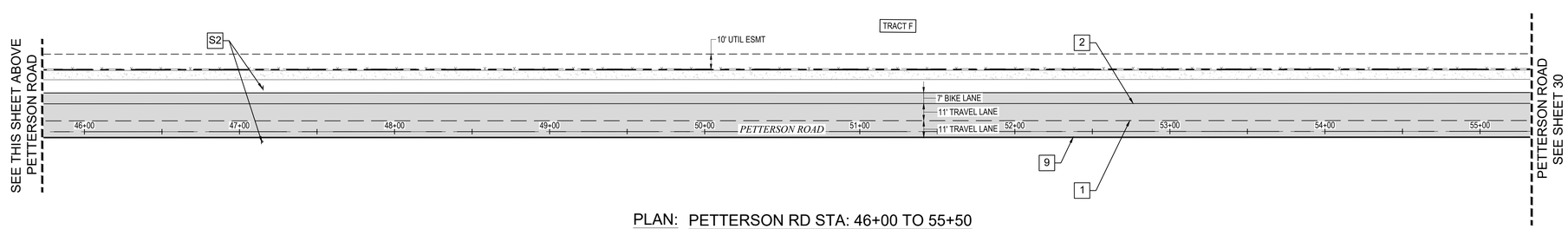
PLAN: PETTERSON RD STA: 27+00 TO 36+50

HORIZONTAL SCALE: 1" = 40'



PLAN: PETTERSON RD STA: 36+50 TO 46+00

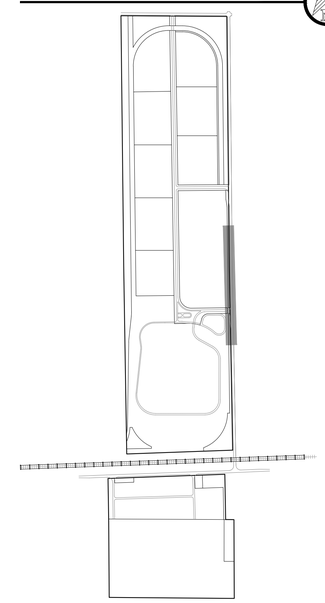
HORIZONTAL SCALE: 1" = 40'



PLAN: PETTERSON RD STA: 46+00 TO 55+50

HORIZONTAL SCALE: 1" = 40'

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

CONSTRUCTION STRIPING NOTES & LEGEND

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2. INSTALL BIKE LANE 6" SOLID WHITE STRIPING PER CITY OF AURORA STANDARDS & SPECIFICATIONS.
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4. INSTALL DOTTED LANE LINE 4" SOLID WHITE STRIPING AT 2' LENGTH AND 4' INTERVALS. INSTALL IN ACCORDANCE WITH STRIPING STANDARDS.
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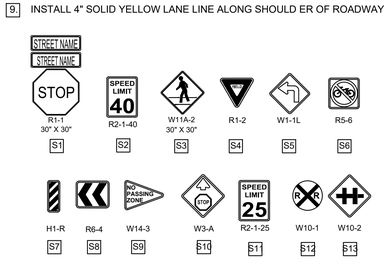
9. INSTALL 4" SOLID YELLOW LANE LINE ALONG SHOULD ER OF ROADWAY



STRIPING NOTES

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3. LIGHTED RAIL CROSSING DESIGN SHALL BE PROVIDED BY A REGISTERED PROFESSIONAL ENGINEER WITH A STANDARD OF CARE FOR RAIL CROSSING. THIS PLAN IS NOT INTENDED TO PROVIDE SIGNALIZED, PROTECTED OR LIGHTED CROSSING DESIGN.

0' 20 40 80

SCALE: 1"=40'

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COLORADO REGISTERED PROFESSIONAL ENGINEER MATTHEW D. NELLGREN 38176 PROFESSIONAL ENGINEER

FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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ROCKY MOUNTAIN
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PETTERSON ROAD AND EAST 48TH AVENUE
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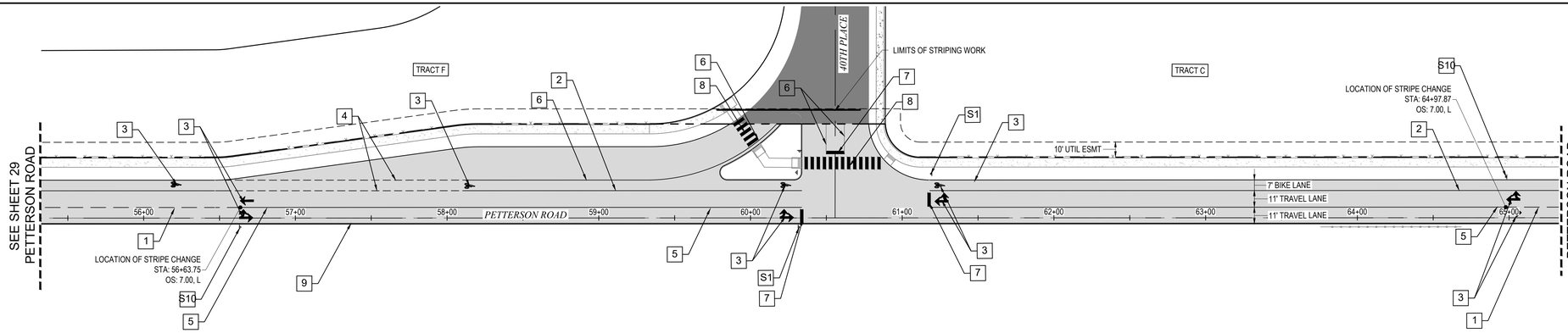
AURORA, COLORADO

PETTERSON RD STA: 27+00 TO 55+50
SIGNAGE & STRIPING PLAN

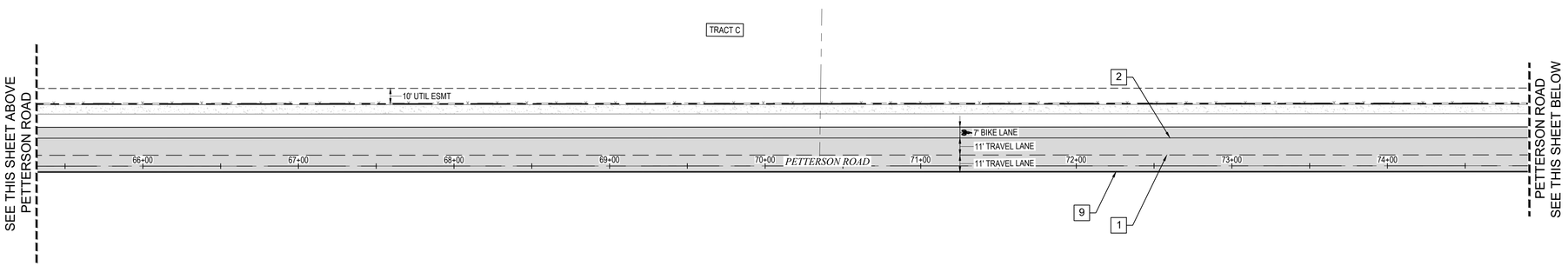
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SSP-4.1
SHEET	29 OF 33

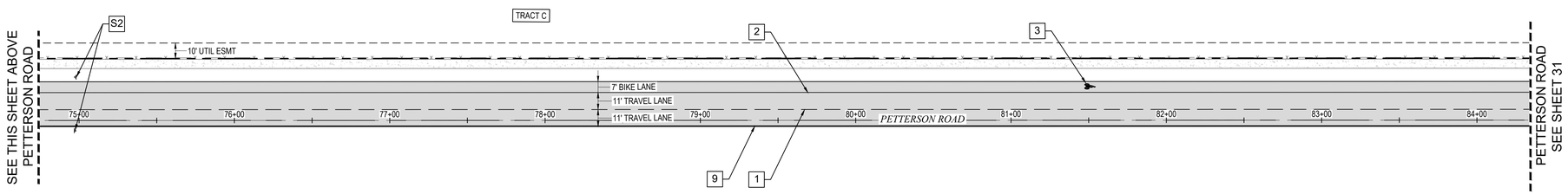
CASE #: 2020-00000



PLAN: PETTERSON RD STA: 55+50 TO 65+50
HORIZONTAL SCALE: 1" = 40'

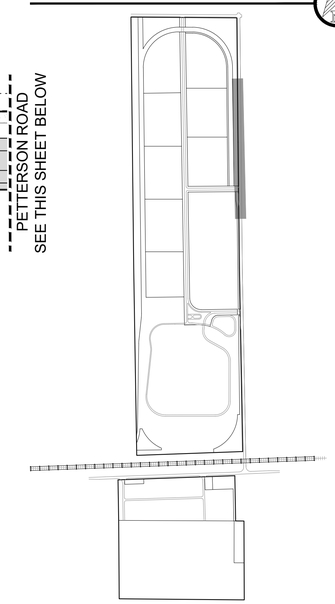


PLAN: PETTERSON RD STA: 65+50 TO 75+00
HORIZONTAL SCALE: 1" = 40'



PLAN: PETTERSON RD STA: 75+00 TO 84+50
HORIZONTAL SCALE: 1" = 40'

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

CONSTRUCTION STRIPING NOTES & LEGEND

1. INSTALL SKIP LANE LINE 4" DASHED YELLOW STRIPING.
2. INSTALL BIKE LANE 6" SOLID WHITE STRIPING PER CITY OF AURORA STANDARDS & SPECIFICATIONS.
3. INSTALL PAVEMENT MARKING AS SHOWN, MARKING SHALL BE IN ACCORDANCE WITH CITY OF AURORA STANDARDS AND CDOT STANDARDS.
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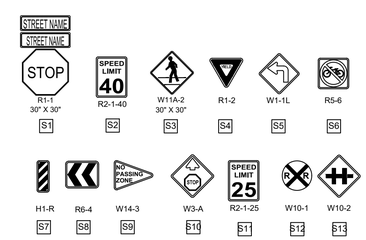
6. INSTALL CHANNELIZING LANE LINE 8" SOLID WHITE STRIPING.
7. INSTALL 24" STOP BAR WHITE
8. INSTALL 2' X 8' CROSS WALK WHITE 4' OC
9. INSTALL 4" SOLID YELLOW LANE LINE ALONG SHOULD ER OF ROADWAY



STRIPING NOTES

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2. RAIL CROSSING SIGNAGE SHALL FOLLOW UNION PACIFIC SIGNAGE, LIGHTING AND STRIPING STANDARDS.
3. LIGHTED RAIL CROSSING DESIGN SHALL BE PROVIDED BY A REGISTERED PROFESSIONAL ENGINEER WITH A STANDARD OF CARE FOR RAIL CROSSING. THIS PLAN IS NOT INTENDED TO PROVIDE SIGNALIZED, PROTECTED OR LIGHTED CROSSING DESIGN.

0' 20 40 80
SCALE: 1"=40'

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(OR 1-800-922-1987)
UTILITY NOTIFICATION CENTER OF COLORADO (UNCC)
WWW.UNCC.ORG

BENCHMARK

A GPS DERIVED ELEVATION WAS ESTABLISHED AT AN ONSITE BENCHMARK, CITY OF AURORA BENCHMARK E-200 41-20, APPROXIMATELY 30' WEST OF THE NORTHEAST CORNER OF SECTION 35, BEING A FOUND "3" BRASS CAP ON A 30" LONG STEEL. PIPE IN CONCRETE" WITH AN ELEVATION OF 5,548.25 FEET. (NAVD88). NO DIFFERENTIAL LEVELING WAS PERFORMED TO ESTABLISH THIS ELEVATION.

COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLGREN
FOR REVIEW ONLY
NOT FOR CONSTRUCTION

NO.	DATE	REVISION	BY	APVD

DSGN	DR	CHK	APVD

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

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4601 DTC BLVD., SUITE 130
DENVER, CO 80237

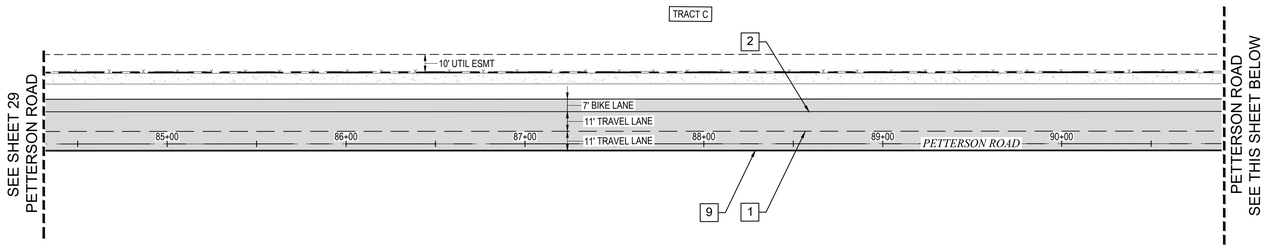
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

PETTERSON RD STA: 55+50 TO 84+50
SIGNAGE & STRIPING PLAN

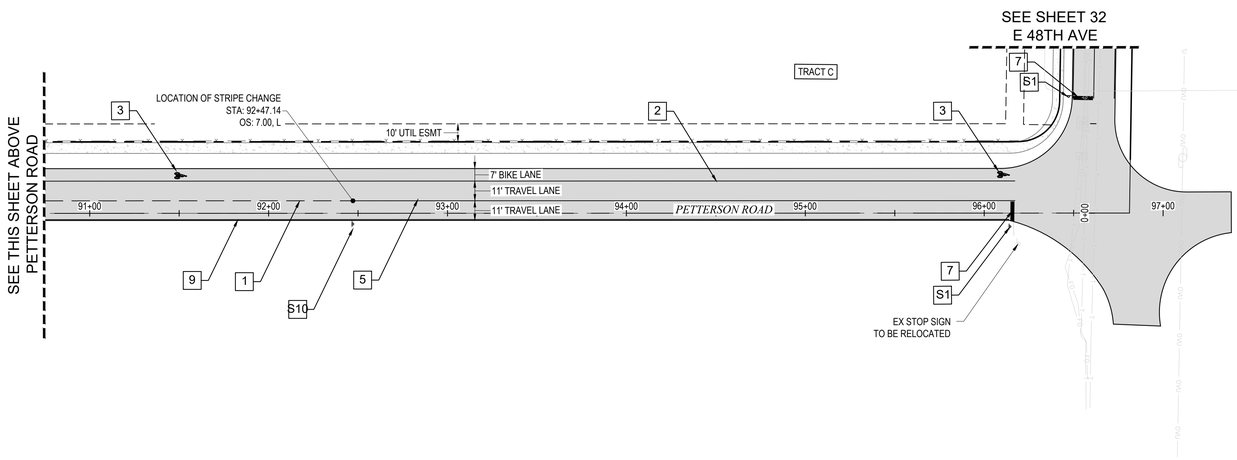
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SSP-4.2
SHEET	30 OF 33

CASE #: 2020-00000

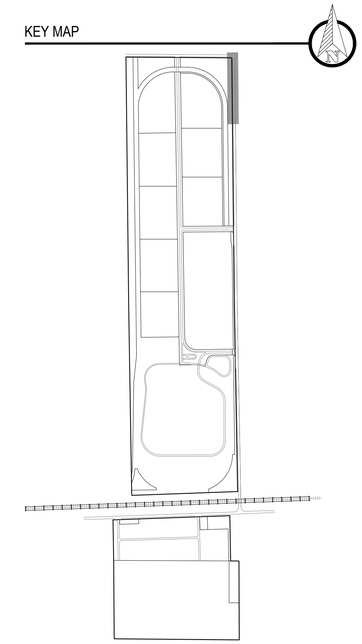


PLAN: PETERSON RD STA: 84+50 TO 91+00
HORIZONTAL SCALE: 1" = 40'



PLAN: PETTERSON RD STA: 91+00 TO 97+00
HORIZONTAL SCALE: 1" = 40'

KEY MAP



LEGEND

	EDGE OF ASPHALT	SS	SANITARY SEWER
	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
5500.00	EXISTING ELEVATION		

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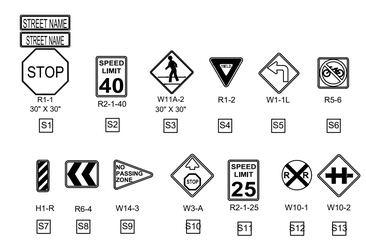

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FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

ENERTIA CONSULTING GROUP
1529 MARKET STREET, SUITE 200
DENVER, COLORADO 80202

ROCKY MOUNTAIN
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PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

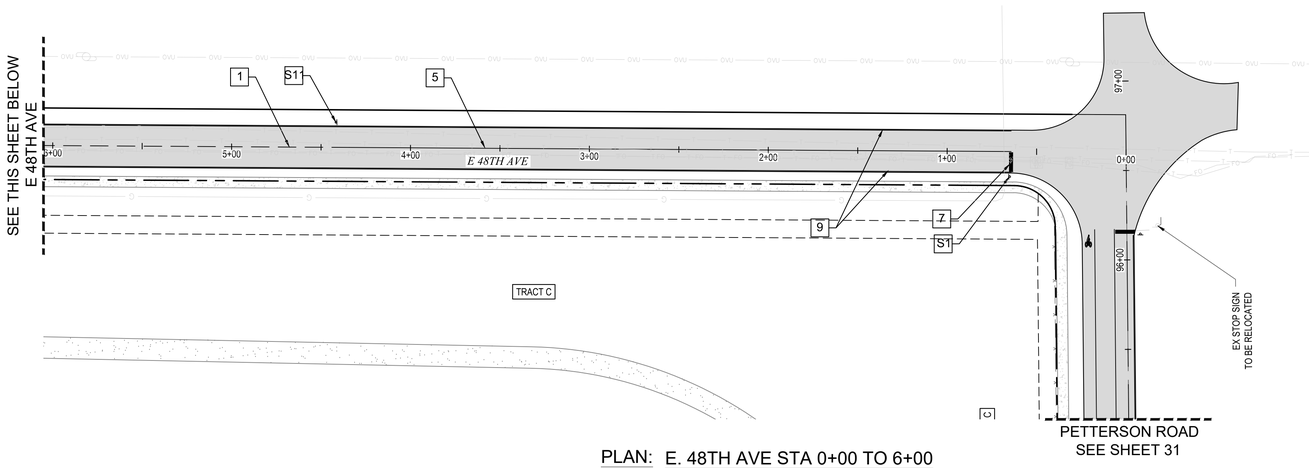
PETTERSON RD STA: 91+00 TO 97+00 SIGNAGE & STRIPING PLAN

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

0' 20 40 80
SCALE: 1"=40'

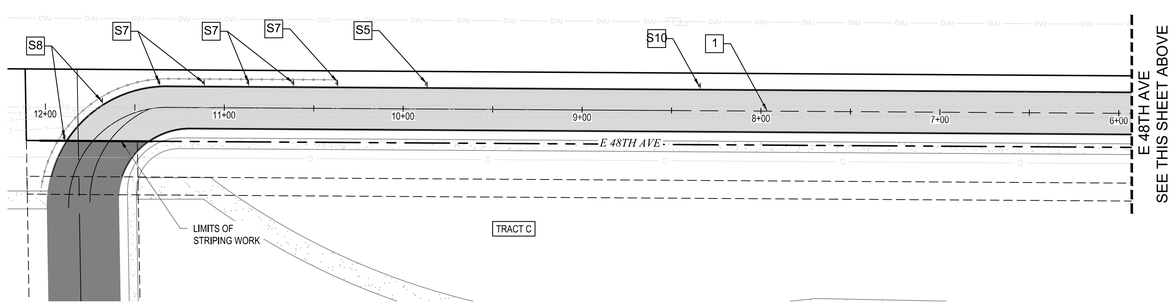
DATE	5/15/2020
FILE	
DWG	SSP-4.3
SHEET	31 OF 33

CASE #: 2020-00000



PLAN: E. 48TH AVE STA 0+00 TO 6+00

HORIZONTAL SCALE: 1" = 40'



PLAN: E. 48TH AVE STA 6+00 TO 12+00

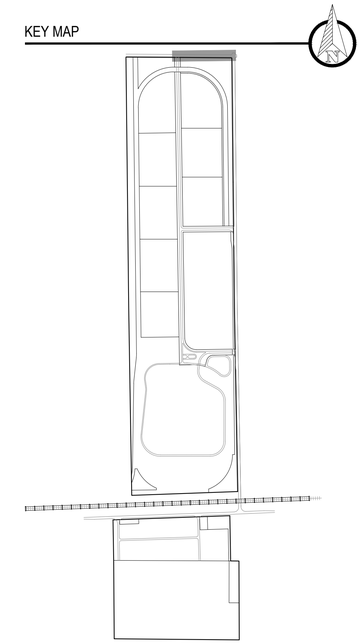
HORIZONTAL SCALE: 1" = 40'

N

0' 20 40 80

SCALE: 1"=40'

KEY MAP



LEGEND

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	CENTERLINE	W	WATER LINE
	STRIPING		STORM DRAIN
	PROPOSED 5' CONTOUR		GRAVEL SURFACE
	PROPOSED 1' CONTOUR		ASPHALT SURFACE
	EXISTING 5' CONTOUR		RIP RAP
	EXISTING 1' CONTOUR		
5500.00	PROPOSED ELEVATION		
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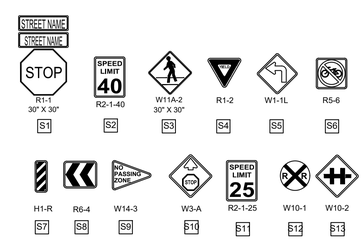

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COLORADO REGISTERED PROFESSIONAL ENGINEER
MATTHEW D. NELLOGAN
38175
7/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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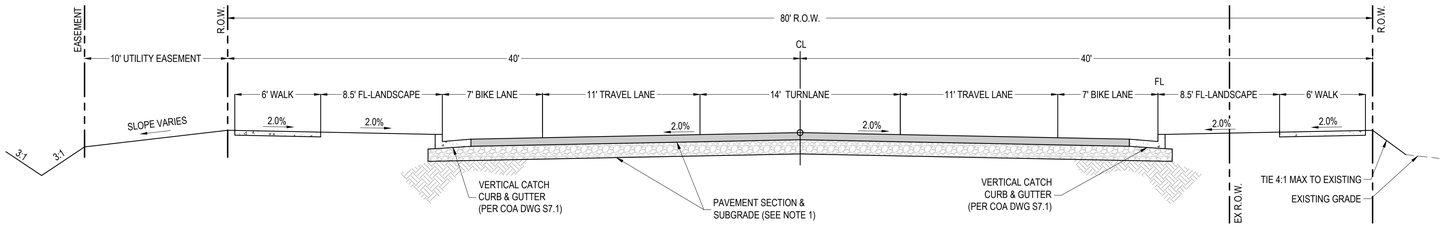
PETTERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

E 48TH AVE STA: 1+00 TO 12+00
SIGNAGE & STRIPING PLAN

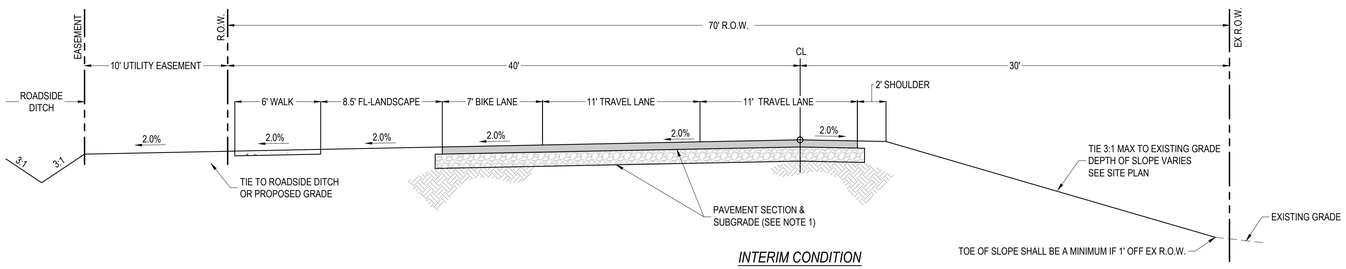
VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	SSP-4.4
SHEET	32 OF 33

CASE #: PRC2018-00006



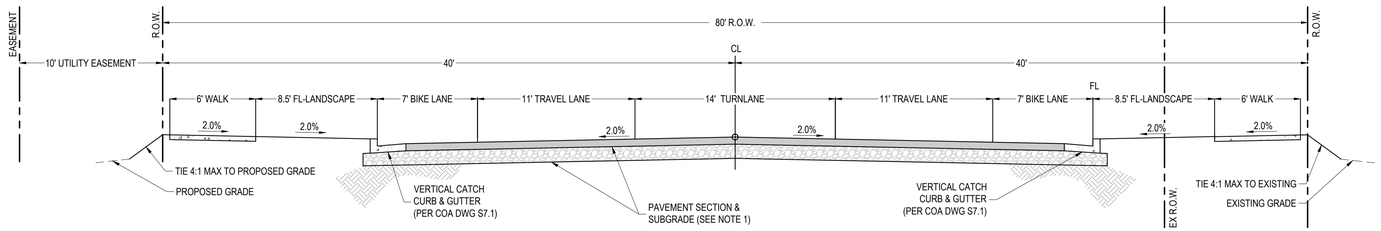
ULTIMATE BUILD OUT CONDITION



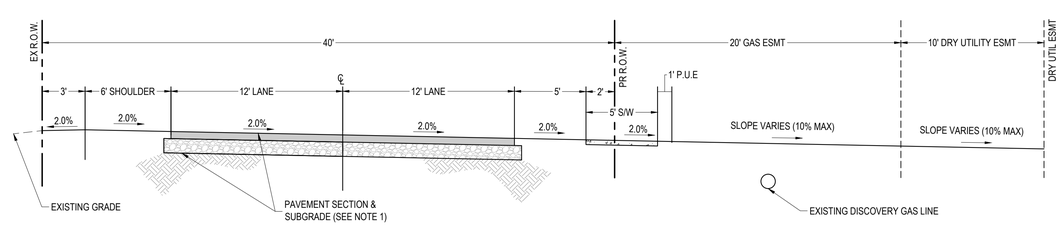
INTERIM CONDITION

R-1 PETERSON ROAD 3-LANE COLLECTOR DETAIL
70' ROW
NTS

NOTE: PAVEMENT SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED GEOTECHNICAL REPORT SPECIFICATIONS



ULTIMATE BUILD OUT CONDITION



R-10 E48TH AVE
NTS

NOTE: PAVEMENT SECTION SHALL BE CONSTRUCTED IN ACCORDANCE WITH THE APPROVED GEOTECHNICAL REPORT SPECIFICATIONS

ROADWAY DETAILS

ITEM	MUNICIPALITY	DETAIL DRAWING NO.
STANDARD MEDIAN CURBS	CITY OF AURORA	S7.2
SIDEWALK CONSTRUCTION JOINT	CITY OF AURORA	S7.3
CURB RAMPS	CITY OF AURORA	S9.5
TYPE 4 CURB RAMP W/ CROSS WALK	CITY OF AURORA	S9.12
CITY OF AURORA RANGE BOX SPECS	CITY OF AURORA	S21.1

NOTE: CITY OF AURORA ROADWAY DETAILS PROVIDED IN THE CITY OF AURORA ROADWAY DESIGN STANDARDS AND SPECIFICATIONS.

STORM SEWER DETAILS

ITEM	MUNICIPALITY	DETAIL DRAWING NO.
SINGLE CONCRETE BOX CULVERT	CDOT	M-601-1
WINGWALLS FOR PIPE OR BOX CULVERT	CDOT	M-601-20
REINFORCED CONCRETE PIPE	CDOT	M-603-2
CONCRETE AND METAL END SECTIONS	CDOT	M-603-10
INLET TYPE D	CDOT	M-604-11

NOTE: CDOT DETAILS PROVIDED IN THE CDOT STANDARD M AND S STANDARDS 20012.

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COLORADO REGISTERED PROFESSIONAL ENGINEER TIMOTHY D. NELLGREN 38176 5/15/20
FOR REVIEW ONLY

NO.	DATE	REVISION	BY	APVD
DSGN	DR	CHK	APVD	

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DENVER, CO 80237

PETERSON ROAD AND EAST 48TH AVENUE
PUBLIC ROADWAY CONSTRUCTION DOCUMENTS
AURORA, COLORADO

TYPICAL SECTIONS & ROADWAY DETAILS

VERIFY SCALE
BAR IS ONE INCH ON ORIGINAL DRAWING.
0 1"

DATE	5/15/2020
FILE	
DWG	DTL-5.0
SHEET	33 OF 33

After Recording Return To:

MASTER DEVELOPMENT AGREEMENT
FOR ROCKY MOUNTAIN RAIL PARK

THIS MASTER DEVELOPMENT AGREEMENT FOR ROCKY MOUNTAIN RAIL PARK ("MASTER AGREEMENT") is made and entered into by and between the BOARD OF COUNTY COMMISSIONERS OF THE COUNTY OF ADAMS, State of Colorado, hereinafter "COUNTY," and RAIL LAND COMPANY, LLC d/b/a ROCKY MOUNTAIN RAIL PARK, a Colorado limited liability company, hereinafter "DEVELOPER" (each a "PARTY," or collectively, the "PARTIES").

RECITALS

WHEREAS, Developer is the owner of approximately 619.45 acres real property in the County of Adams, State of Colorado, as described in **Exhibit A** attached hereto, and by this reference made a part hereof ("PROPERTY"), commonly known as the Rocky Mountain Rail Park ("SUBDIVISION"):.

WHEREAS, the Subdivision will facilitate the development of a rail park adjacent to the Colorado Air and Space Port ("CASP"), and is designed to allow rail delivery to individual end users and further the economic development of the County;

WHEREAS, the subdivision statutes of the State of Colorado, C.R.S. § 30-28-137, and the subdivision resolution of the County authorize the execution of an agreement between the County and Developer whereby Developer agrees to construct any required public improvements for the Subdivision, and to provide security for completion of the public improvements that are within the County's jurisdiction;

WHEREAS, due to the size and complexity of the Subdivision, as well as economic cycles and market conditions that will impact the term of absorption, this Agreement provides for the completion of certain public improvements ("FUTURE IMPROVEMENTS") within the Subdivision and outside of the Subdivision when warranted by development activities within the Subdivision;

WHEREAS this Master Agreement will serve to ensure that the Future Improvements are completed concurrently with, or before, the completion of development within the Subdivision that creates the impacts that create the demand for such improvements, and that security and warranties will be provided to the County according to the County's adopted requirements, which are included in the form Development Agreement ("DA") attached as **Exhibit B**;

WHEREAS, this Master Agreement is consistent with the purpose of Planned Unit Development ("PUD") zoning for the Subdivision, which is to permit detailed development planning as development occurs to allow flexibility in adapting to market demands while protecting and promoting the public health, safety, and welfare of existing and future residents of Adams County, and relieving the County from the cost of completing the Future Improvements; and

WHEREAS, the Parties agree that the impacts of the Subdivision as approved will be fully addressed by this Master Agreement and the subsequent Development Agreements between the County, and the Parties acknowledge that surety may be required for Future Improvements as detailed in those subsequent Development Agreements.

NOW, THEREFORE, in consideration of the foregoing and upon approval and recordation of the final plat of the Subdivision, the parties hereto promise, covenant, and agree as follows:

1. **Incorporation of Recitals**. The Recitals above are fully incorporated herein and made a part hereof.

2. **Future Improvements**.

2.1 *Generally*. The Parties agree that the Future Improvements are not yet warranted and are likely to be influenced by specific development plans proposed by ~~other land~~subsequent purchasers of individual lots within the Subdivision ("LOTS"). Future Improvements will be required if and when certain thresholds ("THRESHOLDS") identified in Master Traffic Impact Study, as defined in Section 2.2, below, are met. If and when a Threshold is met, Developer shall design~~,~~ and construct the Future Improvements as set forth herein.

2.2 *Master Traffic Impact Study*.~~-~~ Prior to issuance of any building or construction permits by the County, ~~the~~ Developer must submit for review and approval a Master Traffic Impact Study ("MASTER STUDY"). The Master ~~Traffic Impact Study~~Study ~~is required to~~shall analyze the entire ~~development~~ Subdivision at full build-out of all ~~lots~~Lots. The ~~study~~ Master Study must identify what improvements are necessary to mitigate for impacts of the fully developed ~~site~~ Subdivision and identify traffic count Thresholds for when specific Future ~~improvements~~ Improvements will be required. The Master ~~Traffic Impact Study~~Study ~~is to~~shall be prepared at ~~the~~ Developer's sole cost and expense.

2.3 *Development of Lots*. ~~Prior to each lot developing t~~Prior to or simultaneously with the application for construction, building, or change-in-use permits for any Lot (each, a "LOT DEVELOPMENT PERMIT"), the Developer shall prepare a Traffic Impact Study ("TIS"), at the Developer's sole cost and expense, and submit the TIS to the County for review and approval. ~~These individual lot Traffic Impact Studies~~The TIS shall be used to determine whether trip generation ~~thresholds~~ Thresholds have been reached.~~–~~ The ~~individual lot Traffic Impact Studies~~TIS must include a Trip Generation analysis for the subject ~~lot~~ Lot and all previously developed ~~lots~~ Lots, and a comparison to the thresholds identified in the Master Traffic Impact Study. Actual traffic counts shall be used (in lieu of Trip Generation calculations) for previously developed Lots, if Certificates of Occupancy have been issued for those Lots and a use is established such that their traffic impacts may be measured.~~–~~ If a ~~threshold~~ Threshold is reached, the ~~individual lot Traffic Impact Study~~TIS must include recommendations for improvements that are identified in the Master ~~Traffic Impact~~ Study. ~~For purposes of this section "developing" means the application of construction, building or change-in-use permits.~~

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Upon approval of the TIS and the application for the Lot Development Permit, the County shall issue the Lot Development Permit. Thereafter:

2.3.1 If Future Improvements are not recommended by the TIS, the Lot Development Permit shall be closed (*e.g.*, by final inspection and issuance of Certificate of Occupancy) according to the County's adopted standards and procedures; or

~~2.3~~2.3.2 If Future Improvements are recommended by the TIS, the Lot Development Permit shall be closed according to the County's adopted standards and procedures only after the Future Improvements that are recommended by the TIS are constructed, in place, and inspected and approved by the governmental entity with jurisdiction (*i.e.*, Adams County, the City of Aurora, the Mile High Flood District, or the Colorado Department of Transportation) ("REGULATING JURISDICTION").

2.4 *Thresholds for Road Improvements*. When a Threshold is met (as identified in the Master ~~Traffic Impact~~ Study) that warrants Future Improvements (as identified in the Master Study)[1], Developer shall provide an executed DA and post appropriate surety with the County as provided in Section 5.4, below. To the extent that Future Improvements are outside of County jurisdiction, nothing in this Master Agreement affects the right of other jurisdictions to apply their adopted approval procedures, including review fees, permit fees, and administrative fees during the review process. ~~For the purposes of this Section 2.3 and its subsections, "complete" shall mean "final acceptance" by Adams County or such entity with jurisdiction to accept the improvement (however such comparable acceptance is titled).~~

~~2.4.1 PETTERSON ROAD AND 48TH AVENUE. Prior to the issuance of a building permit for any building on any lot in the Subdivision, Developer shall satisfy all requirements of the City of Aurora ("AURORA") for Petterson Road and furnish to the County evidence of approval from Aurora.~~

~~2.4.2 RAIL CROSSING MODIFICATIONS. Prior to the issuance for a certificate of occupancy on any lot, Developer shall, in coordination with Union Pacific Railroad, the Public Utilities Commission, and the City of Aurora, design and construct (or cause to be constructed) the Rail Crossing Modifications that are described and detailed in **Exhibit C**.~~

~~2.4.3 COLFAX AVENUE AND PETTERSON ROAD INTERSECTION TURN LANES. Developer shall complete the Colfax Avenue and Petterson Road Intersection Turn Lanes that are described and detailed in **Exhibit C** when such lanes are warranted pursuant to traffic counts and applicable standards as outlined by Colorado Department of Transportation ("CDOT") standards and Traffic Counts provided by Developer.~~

[1] The table of Future Improvements and Thresholds from the Master Study is attached as **Exhibit C**.

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Commented [TM1]: This edit eliminates the need for 2.4.1 to 2.4.4, as the thresholds will be set out in the Master Study and the table will be attached to this Agreement. This structure will prevent potential inconsistencies from arising between the Agreement and the Master Study, ensuring clarity moving forward.

Commented [CF2]: Rail Park Road and 40th Place are private road, should not be in the agreement.

Commented [TM3R2]: Can we assume that the timing of construction of these roads can be timed as the Developer sees fit, provided that any developing lot has access to them?

Commented [CF4]: From County conversations with Aurora, it is likely they will require all the improvements to Petterson up front.

Commented [TM5R4]: Thank you for this information.

Commented [CF6]: Add reference to North and South changes….

Commented [CF7]: Private improvements.

Commented [CF8]: During the PDP hearing, this crossing was something the Commissioners wanted to see up front. It would be in the best interest of RMRP to discuss doing this earlier.

Commented [ME9]: See my comments on the Exhibits.

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2.4.4 COLFAX AVENUE AND MANILLA ROAD ACCELERATION AND DECELERATION LANES. Developer shall complete Colfax Avenue and Manilla Road Acceleration and Deceleration lanes described and detailed in **Exhibit C** when such improvements are warranted pursuant to traffic counts and applicable standards as outlined by Colorado Department of Transportation ("CDOT") and Adams County standards and Traffic Counts provided by Developer.

2.4.52.5 *Drainage Improvements*.

2.5.1 SUB-AREAS FOR DRAINAGE PURPOSES. This The development Subdivision is separated into two distinct development areas, divided by Colfax Avenue. For the purposes of describing the drainage improvement, the "North Area" is defined as the area north of Colfax Avenue, and, the "South Area" is defined as the area south of Colfax Avenue.

2.5.2 Prior to or simultaneously with the development of the first lot Lot in the North Area, Developer shall complete all required drainage improvements in the North Area and the swales to the east and west of the development (*i.e.*, Crooked Run and Crooked Run East).

2.5.3 Prior to or simultaneously with the development of the first lot Lot in the South Area, Developer shall complete all required drainage improvements in the South Area.

3. **Engineering Services**. Developer shall furnish or cause to have furnished, without cost to the County, all engineering services in connection with the design and construction of the Future Iimprovements that are then warrantedrequired pursuant to this Master Agreement when such Future Improvements are warranted. Developer shall obtain, or cause to have obtained the approval from the Regulating Jurisdiction the governmental entity with jurisdiction (*i.e.*, Adams County, the City of Aurora, the Mile High Flood District, or the Colorado Department of Transportation) ("REGULATING JURISDICTION") for all construction plans and engineering reports in connection with the design and construction of said Future Improvements. If the Regulating Jurisdiction is not the County, Developer shall provide confirmation to the County of such approvals within a reasonable time after they are obtained.

4. **Drawings and Estimates**. At or before the time that a TIS associated with the development of a Lot identifies that a Threshold is met or exceeded by such development by the development of the Subdivision, Developer shall furnish, or cause to have furnished, drawings and cost estimates for the Future Improvements that are then warranted, to (and for approval by) the Regulating Jurisdiction. If the Regulating Jurisdiction is not the County, Developer shall provide confirmation to the County of the submittal of such drawings and cost estimates within a reasonable time after they are submitted. Nothing in this Section 4 shall be interpreted to require Developer to furnish more or less information to the Regulating Jurisdiction than it would otherwise require.

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Commented [CF10]: This must go through planner review. This is a requirement in the development standards for an amendment. An agreement is not the proper way to solve this issue.

Commented [CF11]: Master Traffic Impact Study should establish the required thresholds.

5. **Development Agreement.**

5.1 *Generally*. At or before the time that a development application is filed for any Lot~~ within the development,~~ that requires ~~public improvements~~the installation of Future Improvements~~,~~ for which the County is the Regulating Jurisdiction, Developer shall submit a proposed DA in the form attached hereto as **Exhibit B**, in order to provide for construction and (as applicable) warranty of the Future Improvements then required, according to the drawings and materials approved by the ~~Regulating Jurisdiction~~County.

5.2 *Cost of Construction Exhibit.* Developer shall attach to the DA the design and a corresponding Engineer's Opinion of Probable Cost ("EOPC") (together, the "COST OF CONSTRUCTION EXHIBIT") for the applicable Future Improvement, to detail the costs used in determining the Surety requirement as defined in the DA. Construction costs shall be estimated by a registered Colorado Engineer, who must sign and stamp the EOPC exhibit.

5.3 *Public Improvement Construction Schedule Exhibit.* The Cost of Construction Exhibit shall include a completion deadline for all of the improvements included therein. Developer shall not commence construction prior to approval of the DA for that ~~lot~~ Lot and receipt of the appropriate Grading Permit, Access Permit, and/or Right-of-Way ("ROW") Permit from the Regulating Jurisdiction(s), as applicable.

5.4 *Provisions for Surety*. Surety to the County shall be in an amount equal to estimated cost to construct the Future Improvement plus twenty percent (20%) to cover administration and five percent (5%) per year of the term of the DA to cover inflation. Draws under any such Surety shall follow the procedures set out in the Surety, which shall be consistent with the DA. Release of Surety shall be in accordance with the requirements of the Adams County Development Standards and Regulations.

5.5 *Other Regulating Jurisdictions*. Developer shall follow such protocols as adopted by other Regulating Jurisdictions with regarding to permitting and surety for Future Improvements within their jurisdictions. The County will not require the Developer to provide Surety to the County for Future Improvements to be constructed in or for other Regulating Jurisdictions.

6. **Public Land Dedication.** Pursuant to Section 5-05-05-02-01, Adams County Development Standards and Regulations ("ACDS&R"), Developer shall pay cash-in-lieu for 30.9725 acres of regional parks (representing five percent of the land area of the Subdivision), totaling one million five hundred thirty-nine thousand seven hundred ninety-eight and no / 100 dollars **($1,539,798.00)**. Pursuant to Section 5-05-05-03(3), ~~Adams County Development Standards and Regulations~~ACDS&R, the payment of cash-in-lieu for regional parks shall be split into four payments. The first payment is due prior to recording the final plat, the second payment is due prior to the issuance of a Building Permit which would constitute greater than 25 percent of the development, the third payment is due prior to the issuance of a Building Permit which would constitute greater than 50 percent of the development, and the fourth payment is due prior to the issuance of a Building Permit which would constitute greater

Commented [CF12]: In accordance with Section 5-02-04 of the ACDS&R.

Commented [TM13R12]: Our mistake. Can this be reduced for a Metro District that has street and drainage system construction and maintenance in its service plan and the bonding and taxing authority to carry it out?

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than ~~(~~75~~)~~ percent of the development. The cash-in-lieu set out in this Section ~~9~~ 6 shall represent the maximum cash-in-lieu required from the Subdivision for regional parks; however, if the County amends the ~~Development Standards and Regulations~~ACDS&R, at any time prior to the fourth payment, to provide for: (a) a lower cash-in-lieu payment for ~~comparably zoned~~ property that is comparable to the Subdivision due to either industrial or PUD zoning that allows comparable uses; or (b) an appeal or individual calculation methodology to reduce amounts due based on the individual characteristics of a development, then ~~the~~ Developer may apply for a reduction of its total payment according to such updated or amended County regulations and apply that reduction to the balance due to the County.

7. **Reimbursement.** Pursuant to Section 5-02-04, ACDS&R ~~of the Adams County Development Standards and Regulations~~, a subdivider is entitled to fair-share reimbursement of the cost of any streets and related facilities the County requires the subdivider to construct adjacent to or outside of the Subdivision. The reimbursement for the Subdivision will be in the form of credits granted to the assessed Traffic Impact Fee. The reimbursement amount shall be limited to the amount of the Traffic Impact Fee Assessed.

   7.1 [Things to add by RMRP: (1) cost breakdown of the off-site improvements for Manilla Rd; (2) cost estimates are subject to PW review]

8. **Default by Developer.** A default by Developer under this Master Agreement shall exist after notice and hearing and an opportunity to cure of not less than 30 days, as hereinafter provided, if: (a) Developer fails to cure any noncompliance specified in any written notice of noncompliance from the County within a reasonable time after receipt of the notice of noncompliance; (b) Developer otherwise breaches or fails to comply with any obligation of Developer under this Agreement; or (c) Developer becomes insolvent, files a voluntary petition in bankruptcy, is adjudicated a bankrupt pursuant to an involuntary petition in bankruptcy, or a receiver is appointed for Developer.

~~8.~~9. **Protection of Innocent Purchasers.** This Agreement is entered into pursuant to the requirements of the ACDS&R and C.R.S. § 30-28-137(1)(b). It is therefore subject to the provisions of C.R.S. § 30-28-137.

~~9.~~10. **Successors and Assigns.** This Agreement shall be binding upon the heirs, executors, personal representatives, successors, and assigns of Developer, and shall be deemed a covenant running with the real property as described in **Exhibit A** attached hereto.

~~10.~~11. **Assignability**.

   ~~10.1~~11.1 *Assignment to District*. County acknowledges that the Rocky Mountain Rail Park Metro District ("DISTRICT") has been formed for the purpose of constructing and maintaining certain improvements within the Property. The County hereby consents to the performance by the District of any of the obligations of the Developer hereunder. Upon the assignment by the Developer of all or a portion of the Developer's obligations hereunder to the District and the

Commented [TM14]: Maybe the notice and opportunity to cure should be first, then a confirmation by the Director that the default has been cured, followed by a BOCC hearing only if the Director finds that the default has not been cured. Then the hearing would only be necessary if the default wasn't cured or there is a dispute between the Developer and the Director that the BOCC would be called upon to resolve.

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Commented [CF15]: County and subsequent purchasers should be afforded the recourse under C.R.S. 30-28-137(3) and (4)

Commented [TM16]: Sec. 11.1 numbering is the wrong font. This should ultimately be cleaned up, but numbered list font cleanup in redline mode is buggy.

assumption of such obligation by the District, the Developer shall be released from such assigned obligation, and the County will accept performance of such assigned obligation from the District.

~~10.2~~11.2 *Assignment to Others*. Developer may assign its rights and obligations under this Agreement to a party who is the successor or assignee of Developer in its capacity as developer of the Subdivision without the consent of the County; provided, however, that: (a) Developer notifies the County of the assignment and of the name and address of the successor developer; and (b) the successor Developer assumes the obligations of Developer under this Agreement. Unless otherwise agreed by County, Developer shall remain liable for performance of the obligations of Developer under this Agreement. The County shall release a surety furnished by Developer if the County accepts new security from any successor Developer of the Subdivision.

~~11.~~12. **Miscellaneous.**

~~11.1~~12.1 *Notices*. Any notice or communication required under this Agreement must be in writing, and may be given either personally or by registered or certified mail, return receipt requested. If given by registered or certified mail, the same will be deemed to have been given and received on the first to occur of (a) actual receipt by any of the addressees designated below as the party to whom notices are to be sent, or (b) five days after a registered or certified letter containing such notice, properly addressed, with postage prepaid, is deposited in the United States mail. If personally delivered, a notice will be deemed to have been given when delivered to the party to whom it is addressed. Any party hereto may at any time, by giving written notice to the other party hereto as provided in this Section, designate additional persons to whom notices or communications will be given and designate any other address in substitution of the address to which such notice or communication will be given. Such notices or communications will be given to the parties at their addresses set forth below:

If to the County:

Board of County Commissioners
County of Adams
4430 South Adams County Parkway
Brighton, CO 80601
Attention: Chair

With a required copy to:
Adams County Attorney
4430 South Adams County Parkway
Brighton, CO 80601
Attention: County Attorney

If to Owner:

Rail Land Company, LLC
d/b/a Rocky Mountain Rail Park
4601 DTC Boulevard, Suite 130
Denver, CO 80237

With a required copy to:
Fairfield and Woods, P.C.
1801 California Street, Suite 2600
Denver, Colorado 80203
Attention: Rita Connerly, Esq.

~~11.2~~12.2 *Further Assurances*. At any time, and from time to time, upon request of either Party, the other Party agrees to make, execute and deliver or cause to be made, executed and delivered to the requesting Party any and all further instruments, certificates, or documents consistent with the provisions of this Agreement as may, in the reasonable opinion of the requesting party, be necessary or desirable in order to effectuate, complete, or perfect the right of the Parties under this Agreement.

~~11.3~~12.3 *Binding Effect*. Subject to Section 9, above, this Agreement shall run with the land and be binding upon and inure to the benefit of the parties hereto and their respective successors and assigns.

~~11.4~~12.4 *Headings for Convenience*. All headings and captions used herein are for convenience only and are of no meaning in the interpretation or effect of this Agreement.

~~11.5~~12.5 *Severability*. If any provision of this Agreement is declared by a court of competent jurisdiction to be invalid, it shall not affect the validity of this Agreement as a whole or any part thereof other than the part declared to be invalid and there shall be substituted for the affected provision, a valid and enforceable provision as similar as possible to the affected provision.

~~11.6~~12.6 *No Waiver of Governmental Immunity*. Nothing contained in this Agreement shall constitute a waiver of the governmental immunity under applicable state law.

~~11.7~~12.7 *Consent to Jurisdiction and Venue*. Personal jurisdiction and venue for any civil action commenced by either party to this Agreement with respect to this Agreement or a surety shall be proper only if such action is commenced in the District Court for Adams County, Colorado.

~~11.8~~12.8 *Third-Party Beneficiaries*. This Master Agreement is intended to provide for the construction of infrastructure to support development within unincorporated Adams County, Colorado, and while non-parties are named in this Master Agreement, it is intended only for the benefit of the Parties hereto and their respective permitted successors and assigns, and is

not for the benefit of, nor may any provision hereof be enforced by, any other person, organization, entity, or political subdivision.

~~11.9~~12.9 *Force Majeure*. Neither party shall be liable for failure to perform hereunder if such failure is the result of Force Majeure and any time limit expressed in this Agreement shall be extended for the period of any delay resulting from any Force Majeure. "Force Majeure" shall mean causes beyond the reasonable control of a party such as, but not limited to, weather conditions, acts of God, acts of terrorism, strikes, work stoppages, unavailability of or delay in receiving labor or materials, faults by contractors, subcontractors, utility companies or third parties, fire or other casualty, pandemic disease, or action of government authorities.

~~11.10~~12.10 *Entire Agreement*. This Agreement, and any agreement or document referred to herein, constitutes the entire understanding between the parties with respect to the subject matter hereof and all other prior understandings or agreements shall be deemed merged in this Agreement.

IN WITNESS WHEREOF, the parties hereto have executed this Agreement as of the day and year first above written.

[Signature Pages Follow]

RAIL LAND COMPANY, LLC,
a Colorado limited liability company

By: _____
Authorized Signatory

STATE OF COLORADO }
} ss.
CITY AND COUNTY OF DENVER }

The foregoing instrument was acknowledged before me this _____ day of _____, 2020, by _____ as authorized signatory for Rail Land Company, LLC, a Colorado limited liability company.

My commission expires:_____

Address: _____ _____
Notary Public

_____

APPROVED BY resolution at the meeting of _____, 2020.

ATTEST:

BOARD OF COUNTY COMMISSIONERS
ADAMS COUNTY, COLORADO

_____
Clerk of the Board

_____
Chair

_____

EXHIBIT A

LEGAL DESCRIPTION
ROCKY MOUNTAIN RAIL PARK

EXHIBIT "A"

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

SHEET 1 OF 2

A PARCEL OF LAND LOCATED IN THE SOUTHEAST QUARTER OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 TO BEAR NORTH 89°24'01" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.

COMMENCING AT SAID CENTER QUARTER CORNER OF SECTION 35; THENCE ALONG THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, NORTH 00°24'15" WEST, A DISTANCE OF 986.62 FEET TO THE POINT OF BEGINNING; THENCE CONTINUING ALONG SAID WEST LINE, NORTH 00°24'15" WEST, A DISTANCE OF 1644.15 FEET TO THE SOUTH QUARTER CORNER OF SECTION 26; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°54'59" WEST, A DISTANCE OF 849.46 FEET TO POINT ON THE SOUTH RIGHT-OF-WAY LINE OF EAST COLFAX AVENUE; THENCE ALONG SAID SOUTH RIGHT-OF-WAY LINE, NORTH 87°56'25" EAST, A DISTANCE OF 2449.51 FEET; THENCE SOUTH 01°03'37" EAST, A DISTANCE OF 949.60 FEET TO THE SOUTH LINE OF THE SOUTHEAST QUARTER OF SECTION 26; THENCE ALONG SAID SECTION LINE, SOUTH 89°43'06" EAST, A DISTANCE OF 184.57 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF PETERSON ROAD; THENCE ALONG SAID WEST RIGHT-OF-WAY LINE, SOUTH 00°12'00" EAST, A DISTANCE OF 1653.32 FEET; THENCE DEPARTING SAID RIGHT-OF-WAY LINE, NORTH 89°31'02" WEST, A DISTANCE OF 2630.74 FEET TO A PONT ON THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, SAID POINT ALSO BEING THE POINT OF BEGINNING.

SAID PARCEL CONTAINING 6,545,869 SQ. FT. OR 150.27 ACRES MORE OR LESS.

I, JOHN B. GUYTON, A LAND SURVEYOR LICENSED IN THE STATE OF COLORADO, DO HEREBY STATE FOR AND ON BEHALF OF FLATIRONS, INC., THAT THIS PARCEL DESCRIPTION AND ATTACHED EXHIBIT, BEING MADE A PART THEREOF, WERE PREPARED BY ME OR UNDER MY RESPONSIBLE CHARGE AT THE REQUEST OF THE CLIENT AND IS NOT INTENDED TO REPRESENT A MONUMENTED LAND SURVEY OR SUBDIVIDE LAND IN VIOLATION OF STATE STATUTE.

16406
4/18/18
PROFESSIONAL LAND SURVEYOR

JOHN B. GUYTON
COLORADO P.L.S. #16406
CHAIRMAN/CEO, FLATIRONS, INC.

FSI JOB NO. 18-71,096

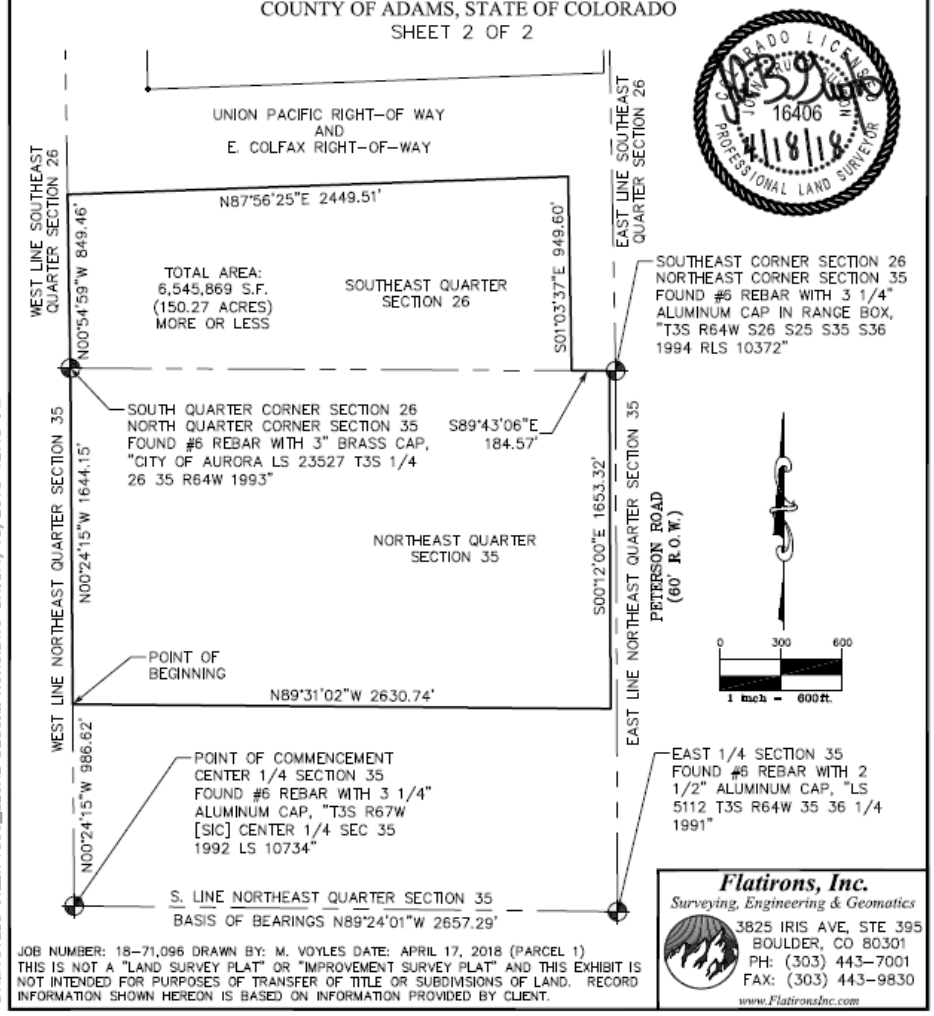
Flatirons, Inc.
Surveying, Engineering & Geomatics
3825 IRIS AVE, STE 395
BOULDER, CO 80301
PH: (303) 443-7001
FAX: (303) 443-9830
www.FlatironsInc.com

JOB NUMBER: 18-71,096 DRAWN BY: M. VOYLES DATE: APRIL 17, 2018 (PARCEL 1)
THIS IS NOT A "LAND SURVEY PLAT" OR "IMPROVEMENT SURVEY PLAT" AND THIS EXHIBIT IS NOT INTENDED FOR PURPOSES OF TRANSFER OF TITLE OR SUBDIVISIONS OF LAND. RECORD INFORMATION SHOWN HEREON IS BASED ON INFORMATION PROVIDED BY CLIENT.

EXHIBIT "A"

LOCATED IN THE SOUTHEAST QUARTER OF SECTION 26 AND THE NORTHEAST QUARTER OF SECTION 35, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN, COUNTY OF ADAMS, STATE OF COLORADO

SHEET 2 OF 2



JOB NUMBER: 18-71,096 DRAWN BY: M. VOYLES DATE: APRIL 17, 2018 (PARCEL 1)
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EXHIBIT "A"

LOCATED IN THE EAST HALF OF SECTIONS 26 AND 23, TOWNSHIP 3 SOUTH,
RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN,
COUNTY OF ADAMS, STATE OF COLORADO

SHEET 1 OF 2

A PARCEL OF LAND LOCATED IN THE EAST HALF OF SECTIONS 26 AND 23, TOWNSHIP 3 SOUTH, RANGE 64 WEST OF THE 6TH P.M., COUNTY OF ADAMS, STATE OF COLORADO, BEING MORE PARTICULARLY DESCRIBED AS FOLLOWS:

CONSIDERING THE SOUTH LINE OF THE NORTHEAST QUARTER OF SECTION 35 AS BEARING NORTH 89°24'01" WEST, A DISTANCE OF 2657.29 FEET BETWEEN THE EAST QUARTER CORNER OF SECTION 35, BEING A FOUND #6 REBAR WITH 2 1/2" ALUMINUM CAP, "LS 5112 T3S R64W 35 36 1/4 1991" AND THE CENTER QUARTER OF SECTION 35, BEING A FOUND #6 REBAR WITH 3 1/4" ALUMINUM CAP, "T3S R67W [SIC] CENTER 1/4 SEC 35 1992 LS 10734" WITH ALL BEARINGS SHOWN HEREON ARE RELATIVE THERETO.

COMMENCING AT SAID CENTER QUARTER CORNER OF SECTION 35; THENCE ALONG THE WEST LINE OF THE NORTHEAST QUARTER OF SECTION 35, NORTH 00°24'15" WEST, A DISTANCE OF 2630.77 FEET TO THE SOUTH QUARTER CORNER OF SECTION 26; THENCE ALONG THE WEST LINE OF THE SOUTHEAST QUARTER OF SECTION 26, NORTH 00°54'59"W, A DISTANCE OF 1349.52 FEET TO POINT ON THE NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID NORTH RIGHT-OF-WAY LINE, NORTH 87°56'25"E, A DISTANCE OF 400.05 FEET TO THE POINT OF BEGINNING; THENCE NORTH 00°54'59"W, A DISTANCE OF 1292.35 FEET; THENCE NORTH 00°54'42"W, A DISTANCE OF 2647.11 FEET; THENCE NORTH 00°38'13"W, A DISTANCE OF 2646.37 FEET; THENCE NORTH 00°38'12"W, A DISTANCE OF 2643.27 FEET; TO A POINT ON THE NORTH LINE OF THE NORTHEAST QUARTER OF SECTION 23; THENCE ALONG SAID NORTH LINE, THENCE SOUTH 89°37'39"E, A DISTANCE OF 2229.51 FEET TO A POINT ON THE WEST RIGHT-OF-WAY LINE OF PETERSON ROAD; THENCE ALONG SAID WEST RIGHT-OF-WAY LINE THE FOLLOWING FOUR (4) COURSES:

1) SOUTH 00°33'11"E, A DISTANCE OF 2646.70 FEET;
2) THENCE SOUTH 00°33'13"E, A DISTANCE OF 2646.16 FEET;
3) THENCE SOUTH 01°04'21"E, A DISTANCE OF 2650.03 FEET;
4) THENCE SOUTH 01°04'25"E, A DISTANCE OF 1191.58 FEET TO A POINT ON SAID NORTH RIGHT-OF-WAY LINE OF THE UNION PACIFIC RAILROAD; THENCE ALONG SAID RIGHT-OF-WAY LINE, SOUTH 87°56'25" WEST, A DISTANCE OF 2232.37 FEET TO THE POINT OF BEGINNING.

SAID PARCEL CONTAINING 20,437,415 SQ. FT. OR 469.18 ACRES MORE OR LESS.

I, JOHN B. GUYTON, A LAND SURVEYOR LICENSED IN THE STATE OF COLORADO, DO HEREBY STATE FOR AND ON BEHALF OF FLATIRONS, INC., THAT THIS PARCEL DESCRIPTION AND ATTACHED EXHIBIT, BEING MADE A PART THEREOF, WERE PREPARED BY ME OR UNDER MY RESPONSIBLE CHARGE AT THE REQUEST OF THE CLIENT AND [illegible] INTENDED TO REPRESENT A MONUMENTED LAND SURVEY OR SUBDIVIDE LAND IN VIOLATION OF STATE STATUTE.

16406
4/18/18

JOHN B. GUYTON
COLORADO P.L.S. #164[illegible]
CHAIRMAN/CEO, FLATIRO[illegible]

FSI JOB NO. 18-71,096

JOB NUMBER: 18-71,096 DRAWN BY: M. VOYLES DATE: APRIL 17, 2018 (PARCEL 2)
THIS IS NOT A "LAND SURVEY PLAT" OR "IMPROVEMENT SURVEY PLAT" AND THIS EXHIBIT IS NOT INTENDED FOR PURPOSES OF TRANSFER OF TITLE OR SUBDIVISIONS OF LAND. RECORD INFORMATION SHOWN HEREON IS BASED ON INFORMATION PROVIDED BY CLIENT.

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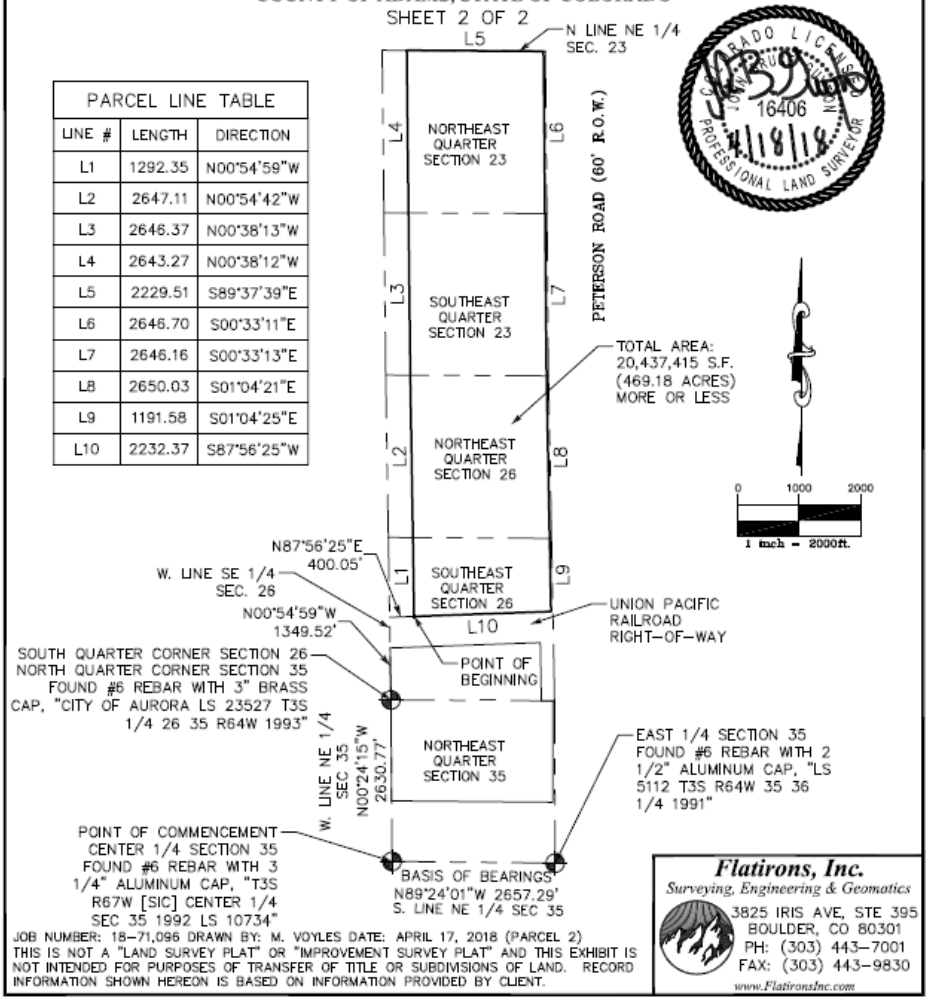
BY:MVOYLES FILE:71096_LEGAL DESCRIPTIONS.DWG DATE:4/18/2018 1:04 PM

EXHIBIT "A"

LOCATED IN THE EAST HALF OF SECTIONS 26 AND 23, TOWNSHIP 3 SOUTH,
RANGE 64 WEST OF THE 6TH PRINCIPAL MERIDIAN,
COUNTY OF ADAMS, STATE OF COLORADO

SHEET 2 OF 2

PARCEL LINE TABLE		
LINE #	LENGTH	DIRECTION
L1	1292.35	N00°54'59"W
L2	2647.11	N00°54'42"W
L3	2646.37	N00°38'13"W
L4	2643.27	N00°38'12"W
L5	2229.51	S89°37'39"E
L6	2646.70	S00°33'11"E
L7	2646.16	S00°33'13"E
L8	2650.03	S01°04'21"E
L9	1191.58	S01°04'25"E
L10	2232.37	S87°56'25"W



COLORADO LICENSED PROFESSIONAL LAND SURVEYOR 16406 4/18/18

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After Recording Return To:

EXHIBIT B

FORM OF DEVELOPMENT AGREEMENT

**(attach most recent version of
County's standard-form SIA, retitled "Development Agreement")**

Commented [ME17]: The Exhibit B should be a cost estimate of all the improvements RMRP needs to build prior to development of the first lot. This includes the improvements to Crooked Run (both channels), the detention ponds and any roadway improvements that will need to be built to support the first lot developed in the development.

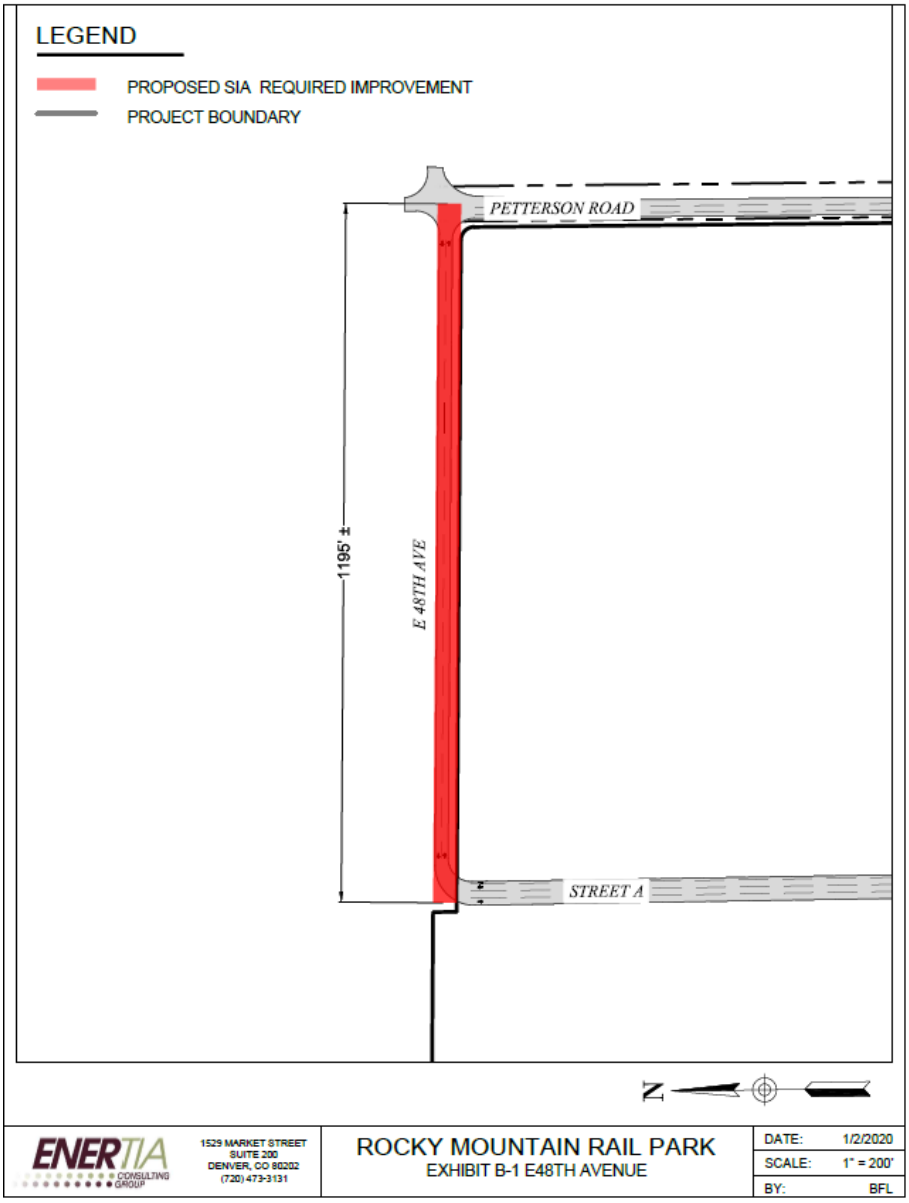
Commented [TM18R17]: The idea was to submit a DA with this agreement that would cover the first set of drainage improvements, and to attach a form here that would be used for Future Improvements. The current thinking is this Master Agreement would be executed concurrently with a DA for the south property that would collateralize the drainage improvements required for that property.

EXHIBIT C

FUTURE IMPROVEMENTS

EXHIBIT C
FUTURE IMPROVEMENTS

C-1A: 48th Avenue – ROW Improvements



Commented [ME19]: Aurora is going to want 48th built out at some point.

The Exhibit C should show all the necessary improvements that are required for the development of the first lot in the development.

Commented [TM20R19]: See prior comment regarding DAs. As we discussed on 6/16/2020, the thought is that the west part of 48th Avenue should be a private, secured access to CASP.

Work to Include:

- Dedication: Dedicate 40 feet south of the section line along 48th Ave. from Petterson Road to Street A.
- Grading: Grade approximately 1210 feet within the proposed ROW according to the construction documents submitted with this FDP. Grading is designed to accommodate the existing outfall channel and the future placement of a box culvert over that channel (if ever necessary). East of Street A, the north toe/top of slope (along the section line) shall not extend beyond the north section line so as not to infringe onto the adjacent landowner property. Plans currently show a retaining wall against the section line (where necessary), however RLC intends to request a grading easement north of the property line to eliminate the retaining wall during future discussions with the property owner.
- Drainage: Install 130-feet of 6x8 box culvert and headwall from detention pond B1 to the north section line. Construct a low-flow crossing on 48th west of Street A (on private property).
- Road Surfacing: Pave approximately 1,182' of Petterson from Station 0+00 to 11+82. Station 1+00 (Colfax) to Station 4+00 (just east of Rail) shall include gravel until such time as required by Exhibit B-2 improvements. Pavement width shall be 25' south of the section line.
- Sidewalk 6' concrete walk (detached 8.5' from EOA)
  Lighting: (none proposed)
- Sewer: None proposed in ROW
- Water: Construct approximately 8365' 12" waterline (single): Construct 1200' 12" waterline (dual) south of split at Petterson Station 13+00 (approx.)

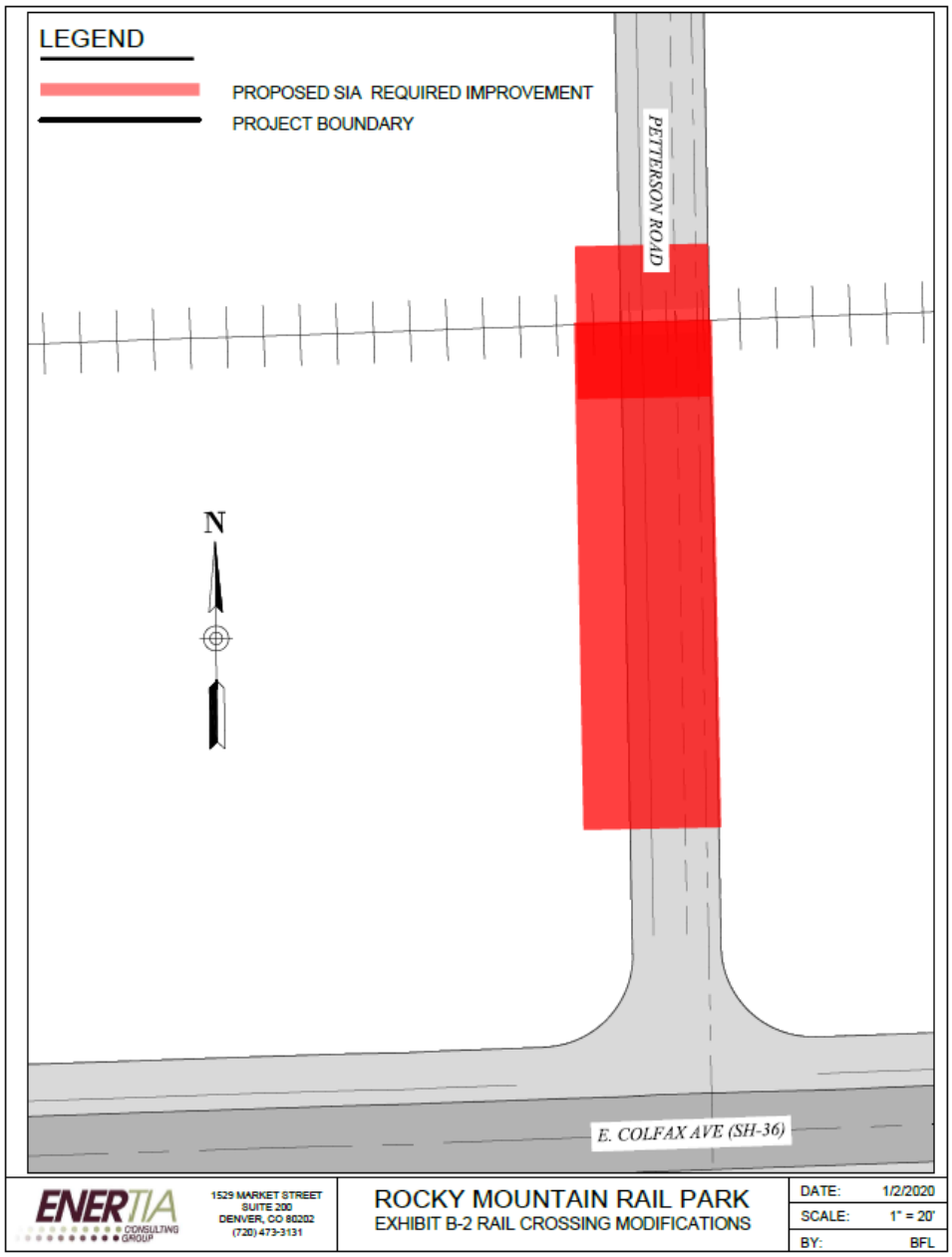
	Description	Quantity	Unit	Unit Price	Item Cost

Construction Completion Date: 3 Years from Commencement of Construction

EXHIBIT C

FUTURE IMPROVEMENTS

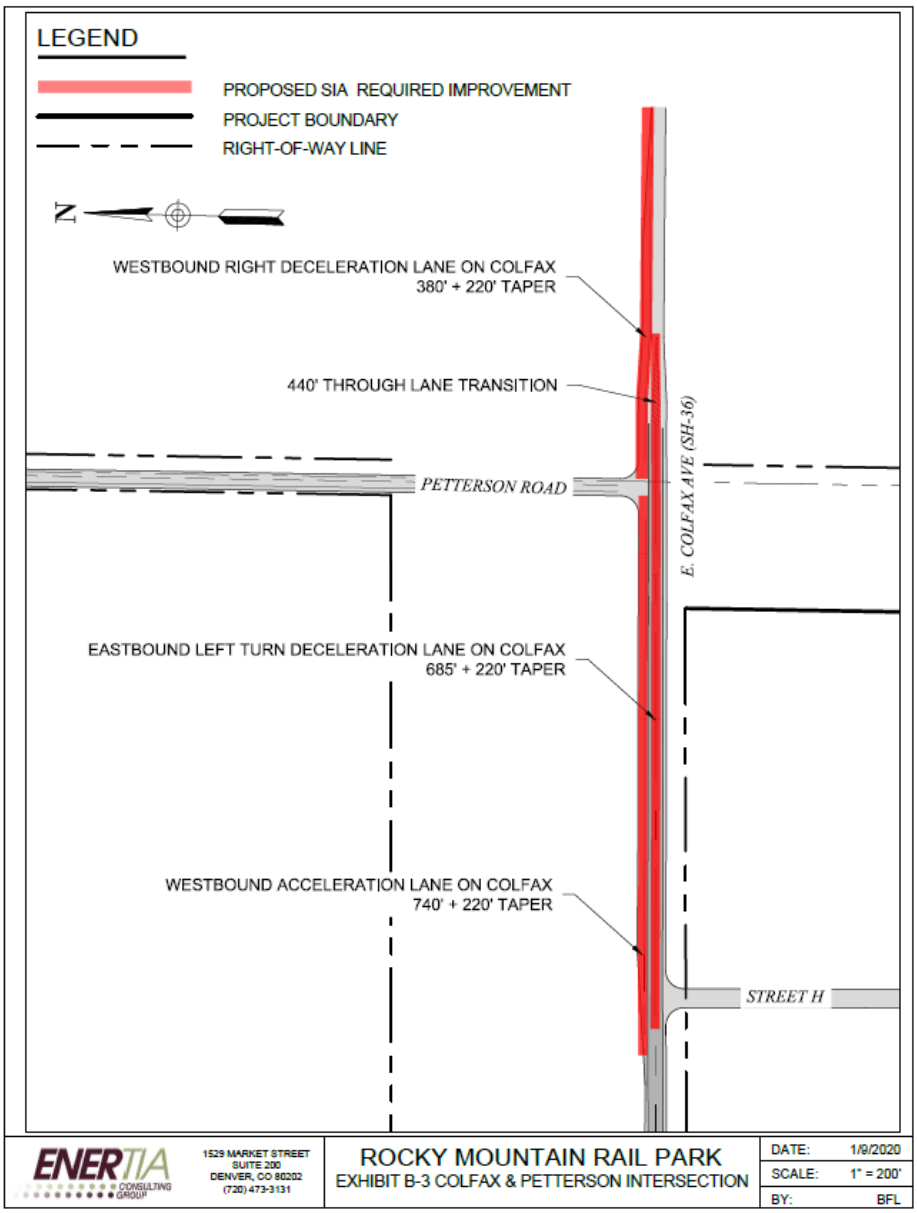
Rail Crossing Modifications (Crossing Petterson Road) (See Sec. 2.3.2)



Work to Include:
The rail crossing includes an at-grade crossing designed to meet the requirements of the Union Pacific RR and the Public Utilities Commission. This area will be graded according to the Peterson roadway construction plans and include a gravel surface until the trigger (described previously) is met. No drainage crossings are required as both sides of Petterson drain away from the road.

	Description	Quantity	Unit	Unit Price	Item Cost

Colfax Avenue and Petterson Road Intersection Turn Lanes (See Sec. 2.3.3)



Commented [ME21]: What gets built on Colfax and when it gets built is up to CDOT. CDOT will not allow for improvements until they are warranted. This is why the County will need to see a Master Traffic Impact Study.

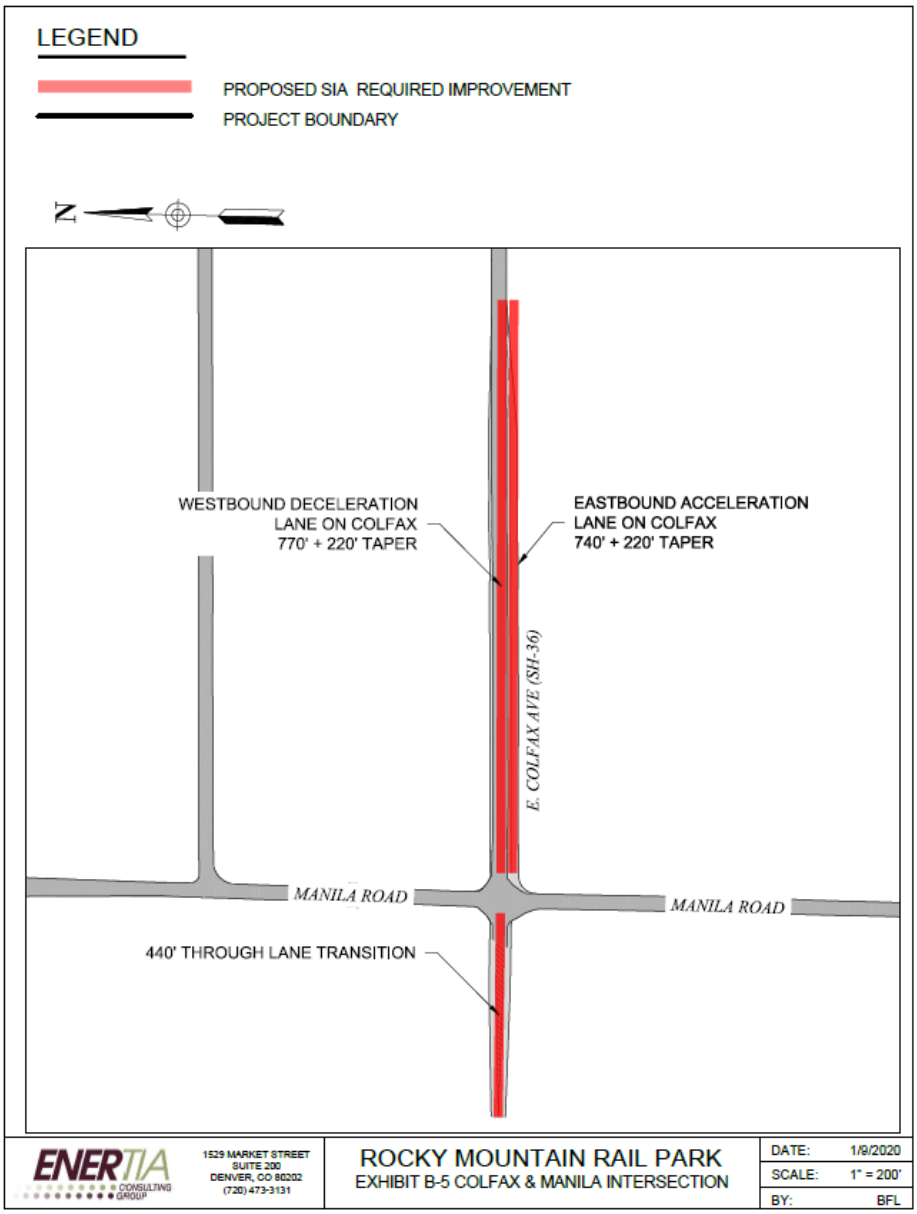
Commented [TM22R21]: Acknowledged.

Work to Include:

- An eastbound left turn deceleration lane on Colfax to turn north onto Petterson
- A westbound acceleration lane for southbound Petterson traffic
- A westbound right deceleration lane on Colfax for traffic turning north onto Petterson.

	Description	Quantity	Unit	Unit Price	Item Cost

Colfax Avenue and Manilla Road Acceleration and Deceleration Lanes (See Sec. 2.3.4)



Work to Include:

- Westbound deceleration lane on Colfax turning south onto Manilla
- Eastbound acceleration lane on Colfax for northbound traffic on Manilla

	Description	Quantity	Unit	Unit Price	Item Cost