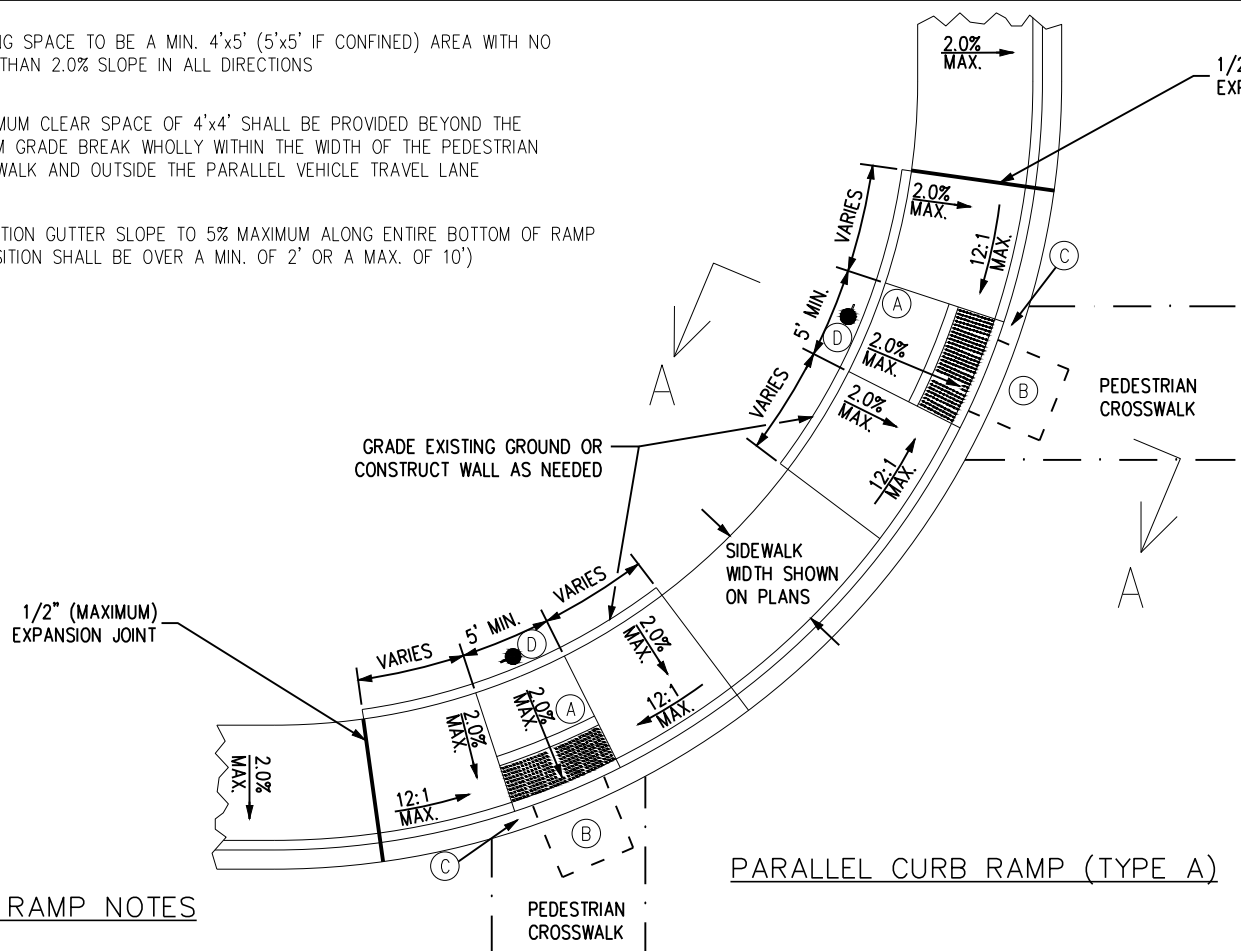
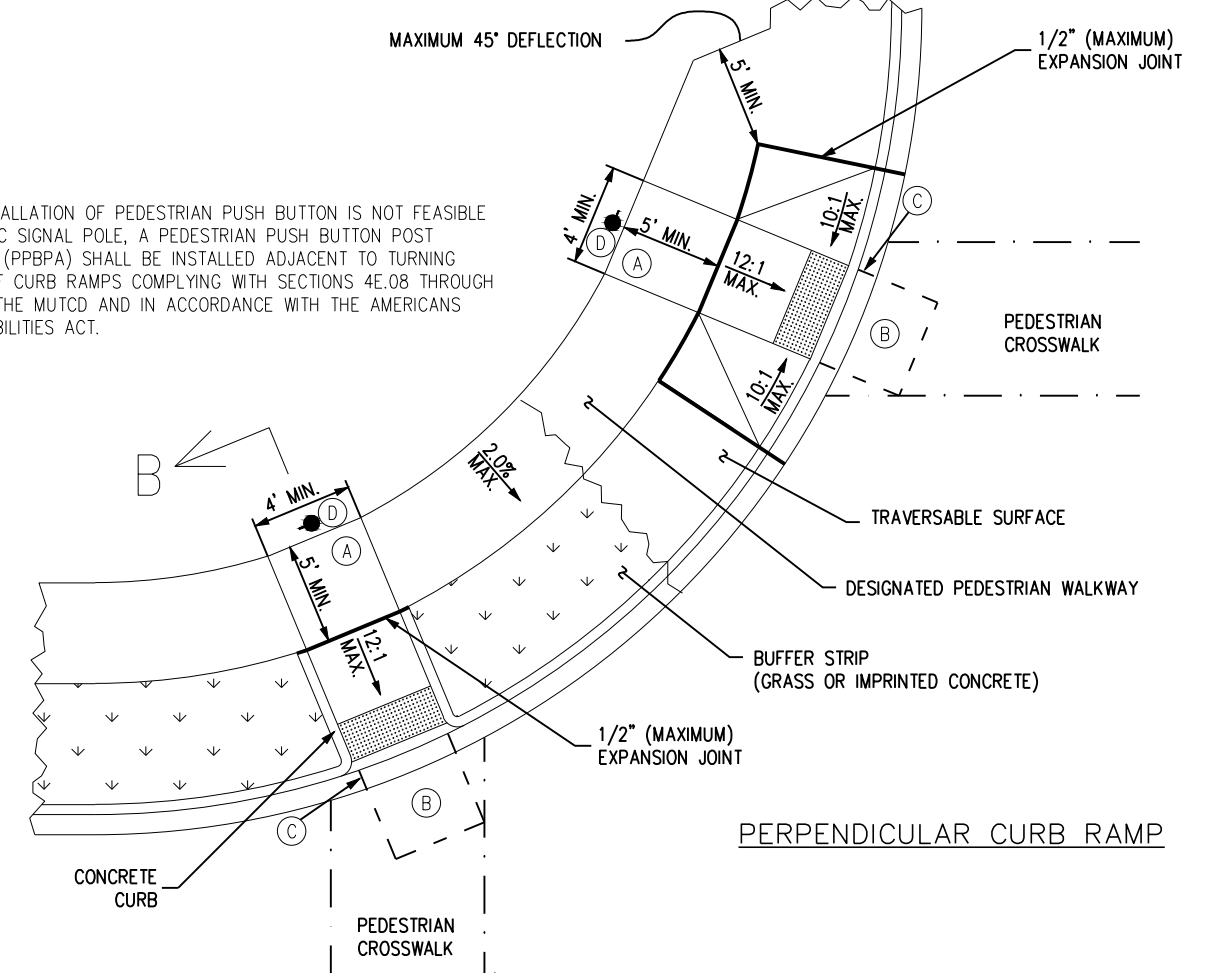


- (A) TURNING SPACE TO BE A MIN. 4'x5' (5'x5' IF CONFINED) AREA WITH NO MORE THAN 2.0% SLOPE IN ALL DIRECTIONS
- (B) A MINIMUM CLEAR SPACE OF 4'x4' SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK WHOLLY WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE
- (C) TRANSITION GUTTER SLOPE TO 5% MAXIMUM ALONG ENTIRE BOTTOM OF RAMP (TRANSITION SHALL BE OVER A MIN. OF 2' OR A MAX. OF 10')

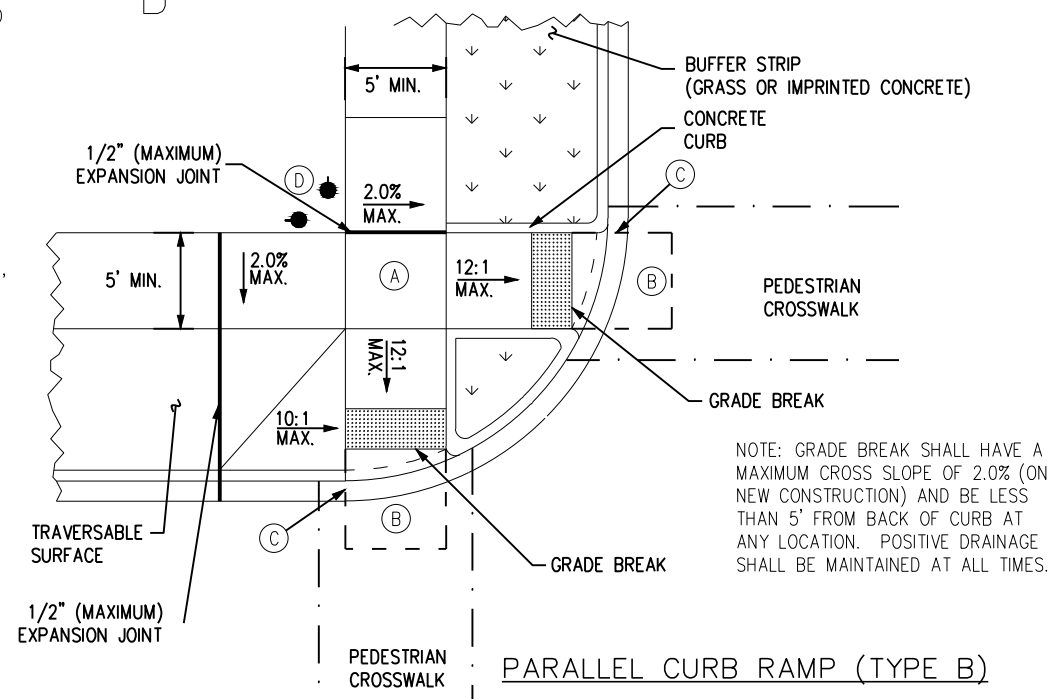


- (D) WHEN INSTALLATION OF PEDESTRIAN PUSH BUTTON IS NOT FEASIBLE ON TRAFFIC SIGNAL POLE, A PEDESTRIAN PUSH BUTTON POST ASSEMBLY (PPBPA) SHALL BE INSTALLED ADJACENT TO TURNING SPACES OF CURB RAMPS COMPLYING WITH SECTIONS 4E.08 THROUGH 4E.13 OF THE MUTCD AND IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT.



CURB RAMP NOTES

1. RAMP SLOPES SHALL BE 12:1 MAXIMUM AND 20:1 MINIMUM.
2. RAMP GRADE BREAKS MUST BE PERPENDICULAR TO THE RUNNING SLOPE.
3. CURB RAMPS SHALL BE A MINIMUM OF 6" THICK. CURB RAMPS ADJACENT TO A CROSS PAN SHALL BE POURED MONOLITHIC AND THE GUTTER THICKNESS SHALL MATCH THE THICKNESS OF THE CROSS PAN (8" MINIMUM).
4. CURB RAMP OPENINGS SHALL BE WHOLLY LOCATED WITHIN PEDESTRIAN CROSSWALK. GUTTERLINE PROFILE SHALL MATCH ROADWAY SLOPE. CROSSWALKS ON NEW CONSTRUCTION SHALL MEET ADA REQUIREMENTS AS DESCRIBED IN THE CODE OF FEDERAL REGULATIONS TITLE II.
5. ALL CONCRETE SIDEWALK AND CURB RAMP SURFACES SHALL HAVE A BROOM FINISH PERPENDICULAR TO THE DIRECTION OF TRAVEL.
6. EXPANSION JOINTS (SEE JOINT DETAIL) SHALL BE PROVIDED AT ALL LOCATIONS WHERE NEW CONCRETE MEETS EXISTING CONCRETE OR AT LOCATIONS SHOWN IN THESE DETAILS.
7. DETECTABLE WARNING SURFACES SHALL BE INSTALLED AT SIDEWALK TO STREET TRANSITIONS AND BE OF A CONTRASTING COLOR. THE U.S. ACCESS BOARD PROPOSED GUIDELINES FOR ACCESSIBLE RIGHTS-OF-WAY (PROWAG) CHAPTER R3 TECHNICAL REQUIREMENTS PROVIDES GUIDANCE ON DETECTABLE WARNING SURFACES. FOR REFERENCE, THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) HAS AN APPROVED PRODUCTS LIST (APL) OF TRUNCATED DOME PANELS. THE COUNTY RESERVES THE RIGHT TO ACCEPT OR REJECT ANY PROPOSED DETECTABLE WARNING SURFACES.
8. ALL DETECTABLE WARNING SURFACE AREAS SHALL BE WITHIN A MAXIMUM OF 2 IN. FROM THE BACK OF CURB (EXCEPT AS SHOWN IN TYPE B PARALLEL RAMP). ALL DETECTABLE WARNING SURFACES SHALL BE 2 FT. IN LENGTH, MEASURED ALONG THE DIRECTION OF PEDESTRIAN TRAVEL, AND COVER THE COMPLETE WIDTH OF THE RAMP AREA.
9. DO NOT INSTALL DRAINAGE STRUCTURES, TRAFFIC SIGNAL EQUIPMENT, JUNCTION BOXES, AND OTHER OBSTRUCTIONS IN FRONT OF OR WITHIN THE RAMP CLEAR SPACE.
10. PERPENDICULAR AND PARALLEL CURB RAMPS SHOWN ARE ACCEPTABLE FOR USE AT MID-BLOCK INSTALLATIONS.
11. THE CONTRACTOR SHOULD ACCOUNT FOR CONSTRUCTION TOLERANCES TO PREVENT EXCEEDING MAXIMUM SLOPES AND MINIMUM DIMENSIONS SHOWN. THE COUNTY MAY REQUIRE REMOVAL AND RECONSTRUCTION TO ACHIEVE SPECIFIED DIMENSIONS AND SLOPES.
12. WHEN ISSUES ARISE THAT REQUIRE ADJUSTMENTS TO PROVIDE AN ADA COMPLIANT CURB RAMP, THE CURB RAMP SHOWN ON THE PLANS MAY BE ADJUSTED IN THE FIELD BY THE CONTRACTOR TO ACCOMMODATE DIFFERING ELEVATIONS, VARIANCES IN SLOPE, OR TRANSITIONS TO EXISTING FEATURES AS LONG AS THE MINIMUM DIMENSIONS ARE ACHIEVED AND THE MAXIMUM SLOPES ARE NOT EXCEEDED.
13. WHEN ISSUES ARISE THAT REQUIRE MODIFICATIONS (I.E. CHANGE CURB RAMP TYPE, ADDITION OR DELETION OF CURB RAMP FEATURES, ETC.) THE ENGINEER OF RECORD SHALL SUBMIT REVISIONS TO THE COUNTY USING THE APPROVED CONSTRUCTION PLANS FOR REVIEW AND APPROVAL.
14. THE CONTRACTOR SHALL NOTIFY THE COUNTY AT LEAST TWO (2) BUSINESS DAYS PRIOR TO NEEDING A CONCRETE FORM INSPECTION.
15. FOR ADDITIONAL GUIDANCE ON CURB RAMP LAYOUT AND CONSTRUCTION, PROWAG SHALL BE REFERENCED.



NOTE: GRADE BREAK SHALL HAVE A MAXIMUM CROSS SLOPE OF 2.0% (ON NEW CONSTRUCTION) AND BE LESS THAN 5' FROM BACK OF CURB AT ANY LOCATION. POSITIVE DRAINAGE SHALL BE MAINTAINED AT ALL TIMES.

Computer File Information	
Creation Date:	12/03/15 Initials: BPM
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Acad Ver.	2013 Scale: N/A Units: ENGLISH

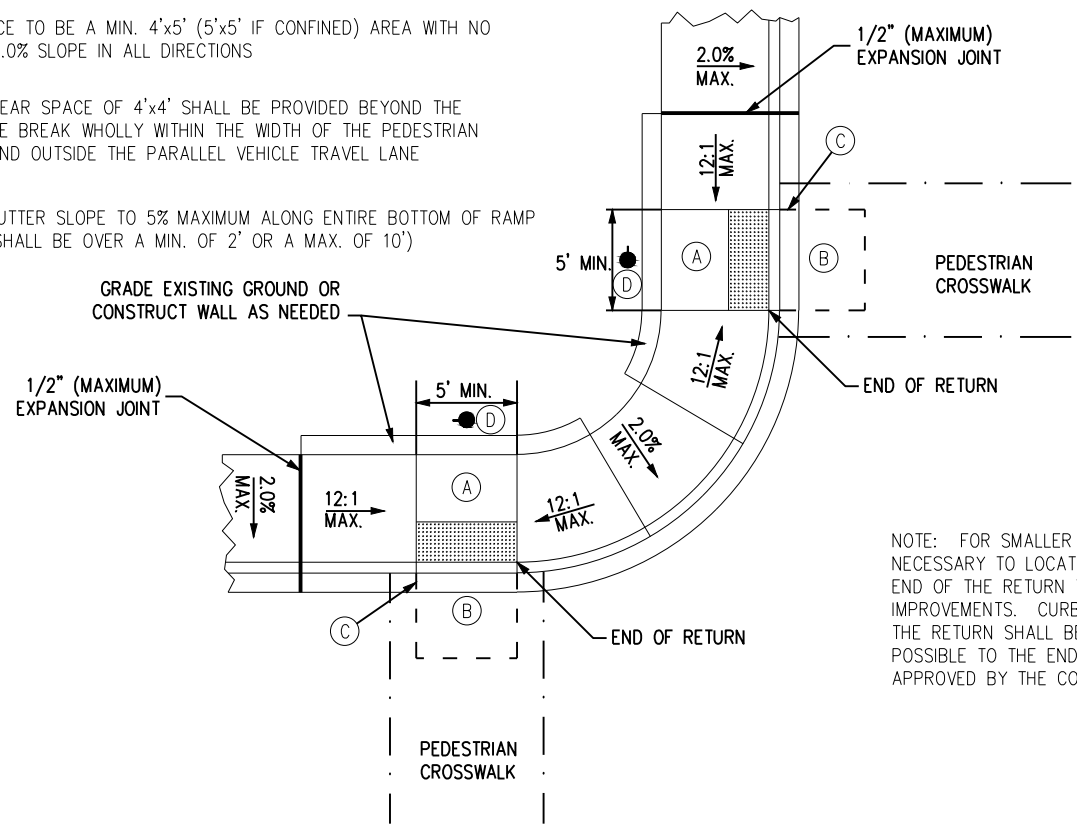
Sheet Revisions		
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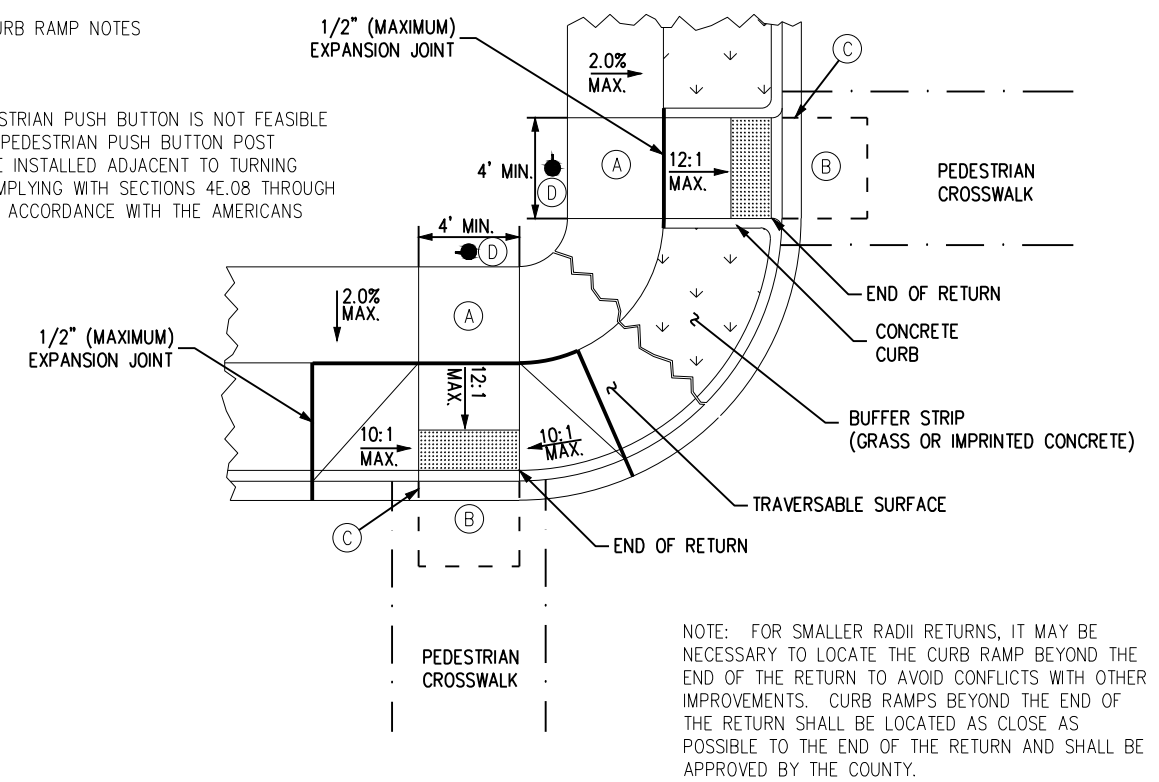
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- (B) A MINIMUM CLEAR SPACE OF 4'x4' SHALL BE PROVIDED BEYOND THE BOTTOM GRADE BREAK WHOLLY WITHIN THE WIDTH OF THE PEDESTRIAN CROSSWALK AND OUTSIDE THE PARALLEL VEHICLE TRAVEL LANE
- (C) TRANSITION GUTTER SLOPE TO 5% MAXIMUM ALONG ENTIRE BOTTOM OF RAMP (TRANSITION SHALL BE OVER A MIN. OF 2' OR A MAX. OF 10')



PARALLEL CURB RAMPS – SMALL RADIUS

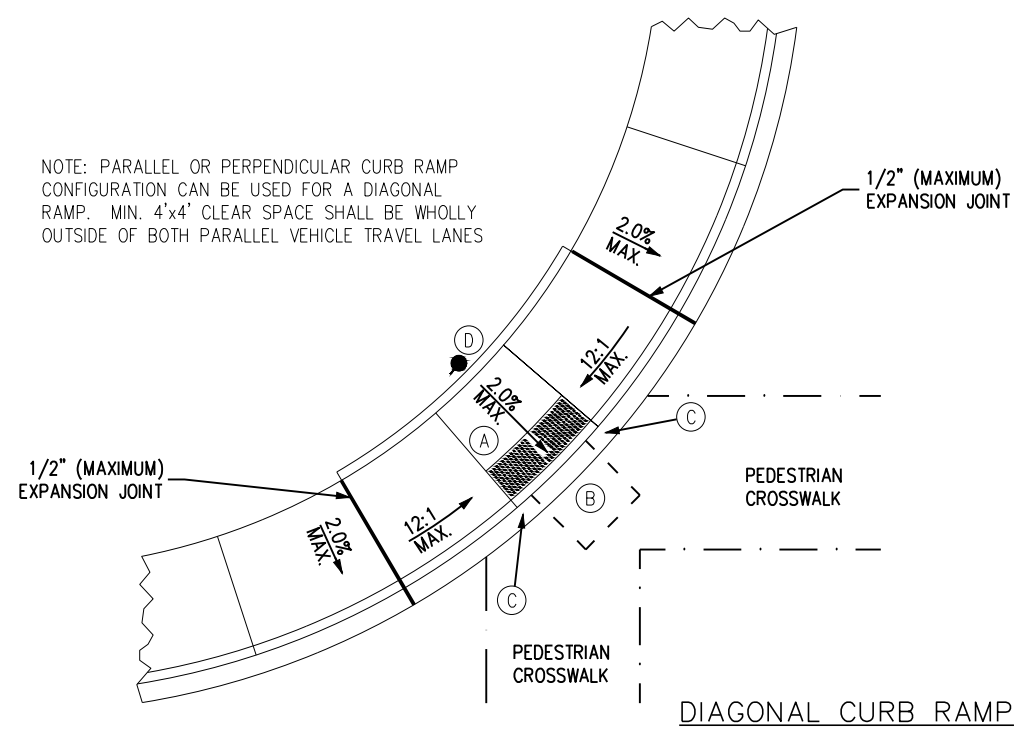
* SEE SHEET 1 FOR CURB RAMP NOTES

- (D) ● WHEN INSTALLATION OF PEDESTRIAN PUSH BUTTON IS NOT FEASIBLE ON TRAFFIC SIGNAL POLE, A PEDESTRIAN PUSH BUTTON POST ASSEMBLY (PPBPA) SHALL BE INSTALLED ADJACENT TO TURNING SPACES OF CURB RAMPS COMPLYING WITH SECTIONS 4E.08 THROUGH 4E.13 OF THE MUTCD AND IN ACCORDANCE WITH THE AMERICANS WITH DISABILITIES ACT.

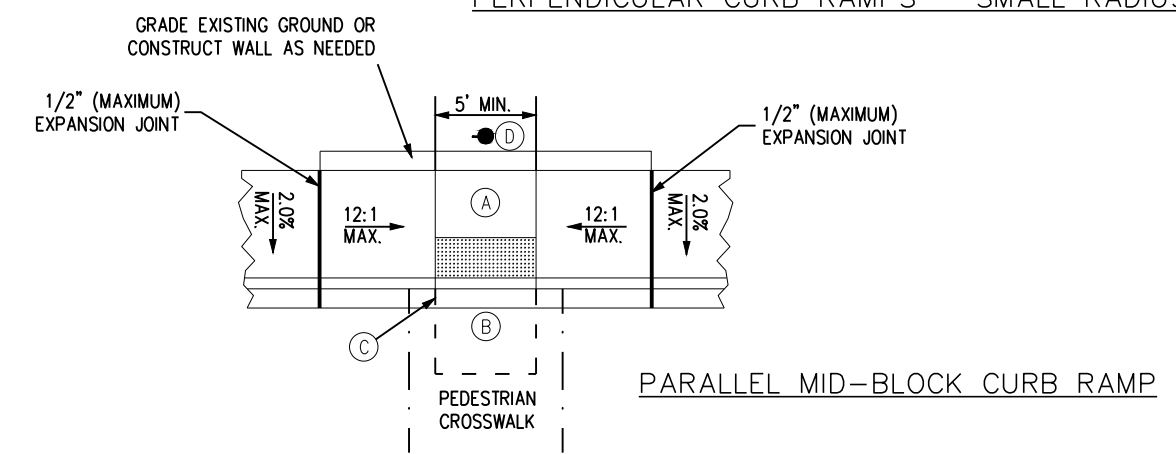


PERPENDICULAR CURB RAMPS – SMALL RADIUS

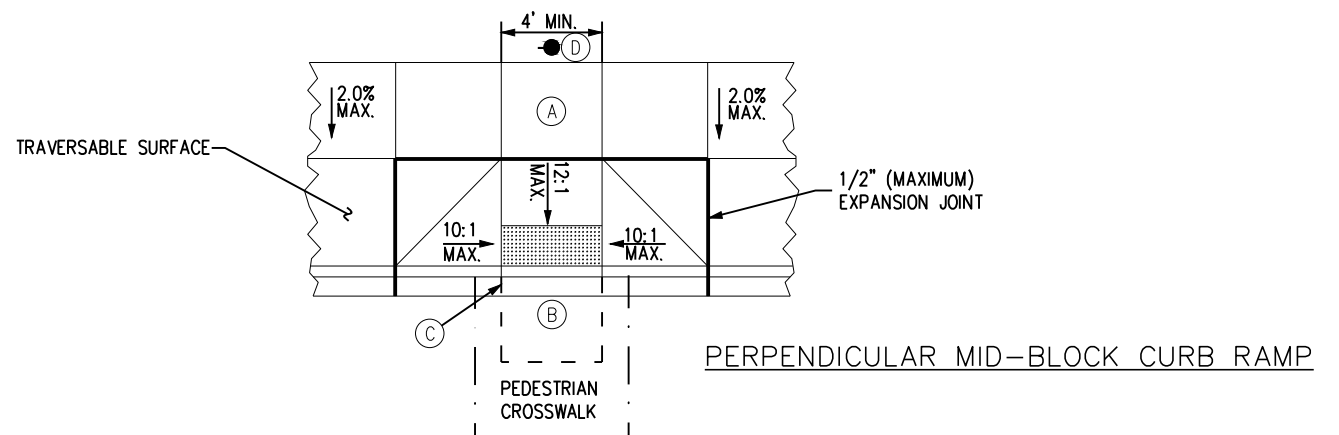
NOTE: PARALLEL OR PERPENDICULAR CURB RAMP CONFIGURATION CAN BE USED FOR A DIAGONAL RAMP. MIN. 4'x4' CLEAR SPACE SHALL BE WHOLLY OUTSIDE OF BOTH PARALLEL VEHICLE TRAVEL LANES



DIAGONAL CURB RAMP



PARALLEL MID-BLOCK CURB RAMP



PERPENDICULAR MID-BLOCK CURB RAMP

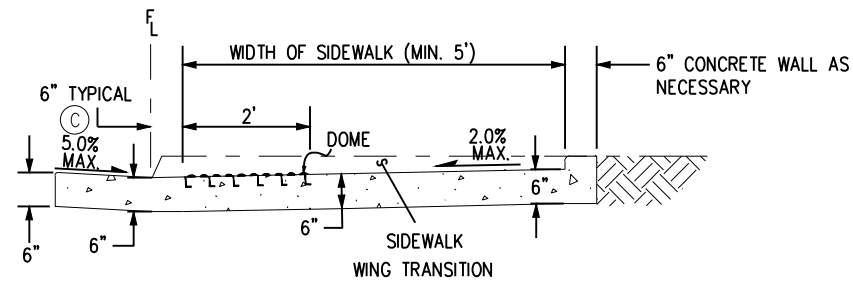
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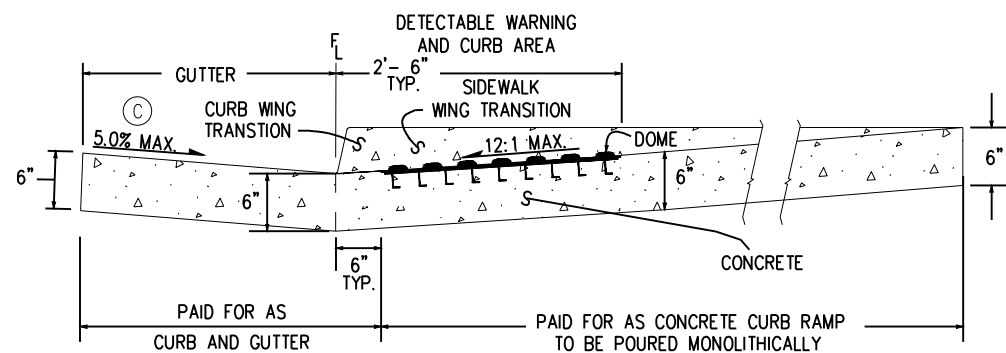


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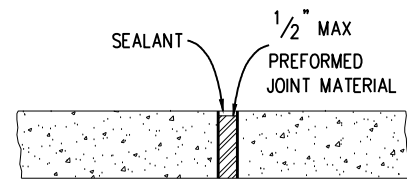
CURB RAMP DETAILS



SECTION A-A CURB RAMP PROFILE



SECTION B-B CURB RAMP PROFILE

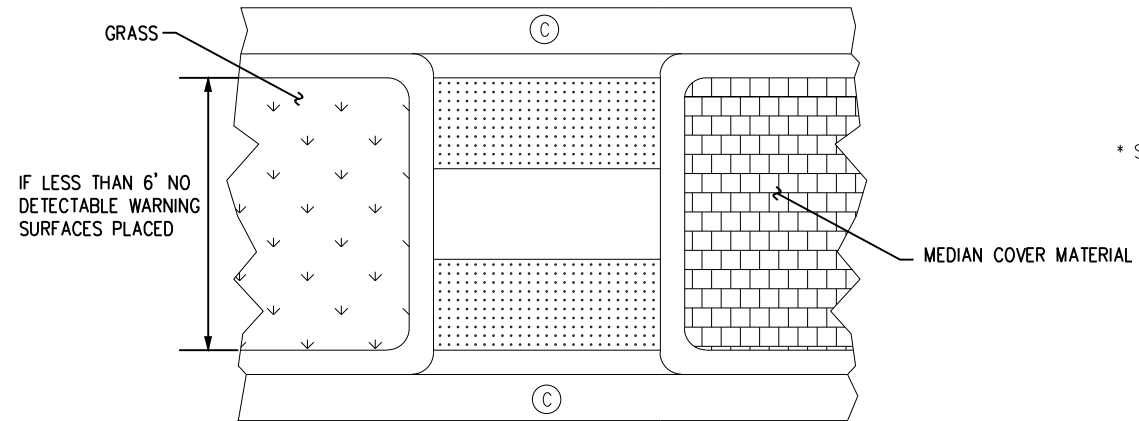


NOTE:
EXPANSION JOINTS SHALL NOT BE PLACED WITHIN THE RAMP AREA

EXPANSION JOINT

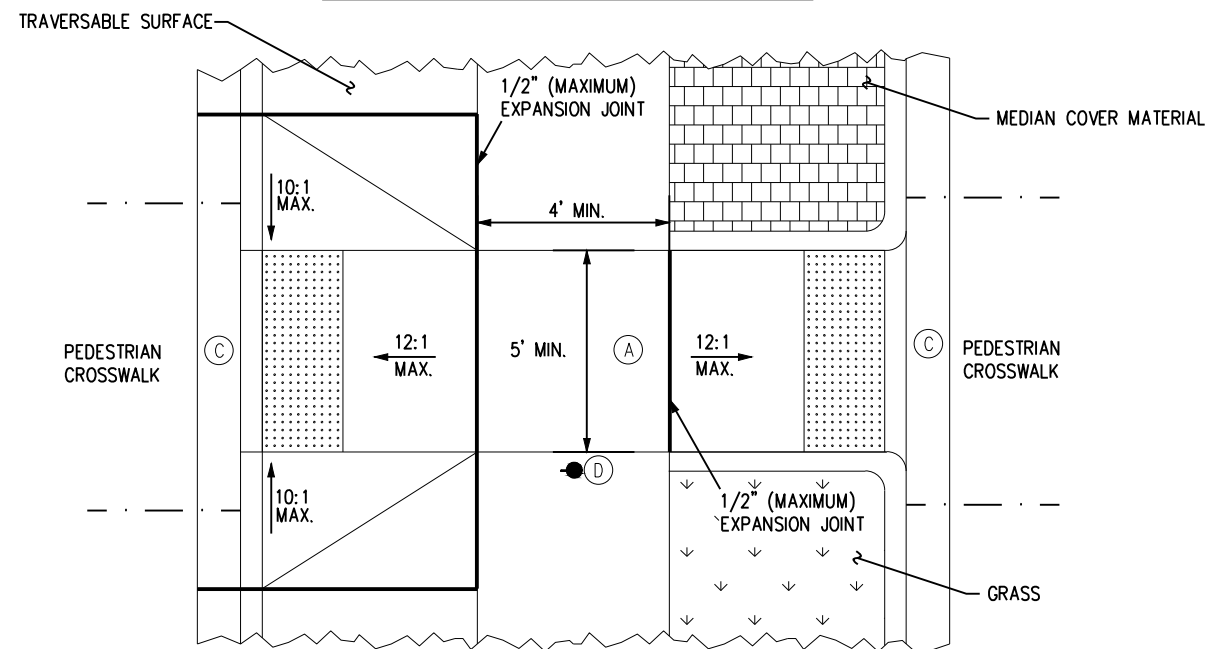
DOMe PANEL NOTES

1. DETECTABLE WARNING SURFACES SHALL BE OF A CONTRASTING COLOR AND MEET CHAPTER R3 TECHNICAL REQUIREMENTS IN THE U.S. ACCESS BOARD PROPOSED GUIDELINES FOR ACCESSIBLE RIGHTS-OF-WAY (PROWAG) AS APPROVED BY THE COUNTY. FOR REFERENCE, THE COLORADO DEPARTMENT OF TRANSPORTATION (CDOT) HAS AN APPROVED PRODUCTS LIST (APL) OF TRUNCATED DOME PANELS. THE COUNTY RESERVES THE RIGHT TO ACCEPT OR REJECT ANY PROPOSED DETECTABLE WARNING SURFACES.
2. DETECTABLE WARNINGS SHALL BE INSTALLED AT SIDEWALK TO STREET TRANSITIONS.
3. THE TRUNCATED DOME PANEL SHALL BE EMBEDDED IN THE CONCRETE CURB RAMP WHILE CONCRETE IS PLASTIC.
4. WHEN DETECTABLE WARNING SURFACE IS CUT, GRIND OFF REMAINING PORTION OF ANY CUT DOMES. SEAL ALL CUT PANEL EDGES TO PREVENT WATER DAMAGE.
5. IF BRICKS OR PAVERS ARE USED FOR DETECTABLE WARNING SURFACE, THE UNDERLYING CONCRETE SHALL BE A MINIMUM OF 4 INCHES THICK.
6. JOINT SEALANT SHALL BE A PRODUCT ON THE CDOT APL OR AN APPROVED EQUAL BY THE COUNTY.



* SEE SHEET 1 FOR CURB RAMP NOTES

MEDIAN CUT THROUGH CROSSING



PEDESTRIAN MEDIAN REFUGE

- (A) TURNING SPACE TO BE A MIN. 4'x5' (5'x5' IF CONFINED) AREA WITH NO MORE THAN 2.0% SLOPE IN ALL DIRECTIONS
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